

AGENDA
CITY COUNCIL STUDY SESSION
JANUARY 8, 2024 - 5:30PM
CITY OF FARMINGTON HILLS
CITY HALL – COMMUNITY ROOM
31555 W ELEVEN MILE ROAD
FARMINGTON HILLS, MICHIGAN
Telephone: 248-871-2410 Website: www.fhgov.com

1. Call Study Session to Order
2. Roll Call
3. Updates from [Winbourne](#)

Respectfully submitted,

Carly Lindahl, Interim City Clerk

Reviewed by:

Gary Mekjian, City Manager

NOTE: Anyone planning to attend the meeting who has need of special assistance under the Americans with Disabilities Act (ADA) is asked to contact the City Clerk's Office at 248-871-2410 at least two (2) business days prior to the meeting, wherein necessary arrangements/accommodations will be made. Thank you.

FARMINGTON HILLS POLICE DEPARTMENT ANALYSIS 2022

Phase 2 – Traffic Stops and Citations Assessment & Analysis

Section 1 of 2
Draft v13.1
Primary Report

January 5, 2024



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PROJECT SCOPE AND OBJECTIVES

The objective of this project is to complete an assessment of the Police Department's policies, procedures, training, customs, and culture regarding traffic stops and the issuance of traffic citations. The assessment will focus on identifying any bias regarding traffic stops and citations.

Winbourne Consulting Contract/Statement of Work Tasks

Assess the Police Department traffic stop policies, procedures, and training to compare law enforcement industry model policies, standards, guidelines, and best practices.

Information required to complete the assessment may include:

1. CAD breakdown
 - a. Traffic stops
 - i. All traffic stops for the entire City by type of stop (event code) and disposition code
 - ii. Traffic stops by District, Beat, Zone
 - iii. Traffic stops by shift – Day, evening, and midnight
 - iv. Does the department differentiate a vehicle stop for a traffic offense versus a crime-related one (e.g., BOLO, suspicious activity, wanted person, suspect vehicle, etc.)?
 - v. Can the CAD system map the above data (e.g., multiple map views)?
 - b. Traffic Stop/Anti-Bias
 - i. Does the Department collect driver/passenger demographic data for each stop?
 - c. Traffic stop Warnings (no further action)
 - d. Traffic stop resulting in arrests
 - i. Can the CAD/RMS provide a breakdown of arrests made from traffic stops?
 1. Type of crime – Traffic/DL, DUI, drugs, weapons, wanted person, etc.
 2. Demographics of persons arrested
 - e. Traffic stop Use of Force
 - i. Breakdown of Use of Force incidents for types of activity including traffic stops
 - f. Vehicle search data
 - i. Does the Department collect data regarding when searches of vehicles are made related to traffic stops
 1. Consent search
 2. Incident to arrest
 3. Criminal investigation/search warrant
2. Enforcement via automated systems [e.g., ALPR, speed cameras, either mobile or fixed (school zone) red-light cameras, etc.]

3. *Traffic citation data*
 - a. *Does the department enter traffic citation data into the RMS?*
 - b. *Does the local court or state enter traffic citations into a database? If yes, can this data be queried (e.g., generate various types of reports)*
4. *City demographic information*
 - a. *Entire City*
 - b. *District, Beat Zone*
 - c. *Roadway demographics (e.g., during different times of day, a manufacturing plant, office building, industry, etc., can change the demographic of a roadway with workers traveling to or from work)*
5. *Traffic crash data*
 - a. *Volume, locations – District, Beat, Zones*
 - b. *Crash patterns – Run red light, speeding*
 - c. *Most dangerous intersections*
6. *Revenue breakdown from traffic citations*
 - a. *Breakdown of revenue generated from traffic citations*
 - i. *State*
 - ii. *County*
 - iii. *City – Does the money go to a City general fund?*
7. *CALEA and/or State accreditation standards*
8. *BWC/In-Car camera*
 - a. *Does the department employ a BWC/In-Car camera system?*
 - b. *Does the department have a formal QA process regarding officer demeanor and professionalism at traffic stops*
9. *Critical incidents (officer-involved shooting, vehicle pursuits w/injuries, violent felony incidents, etc.) NOTE: The Winterbourne team may specify particular critical incidents based on interviews and personal research.*
10. *GIS Mapping (e.g., heat maps, with additional layers of real numbers of*
 - a. *Traffic accidents, layered by severity*
 - b. *Locations of traffic citations issued*
 - i. *If possible, Map 1 detailing citations issued to Farmington Hills residents*
 - ii. *Map 2, detailing citations issued to non-Farmington Hills addresses*
 - c. *Traffic patterns, particularly identifying principal routes of ingress, egress, and commuting patterns by rush hours, weekday, and weekend denominators*
 - d. *General (all) arrests*
 - e. *If possible, general (all) locations of documented use of force. (This may be delimited by Department-defined “serious” uses of force)*
 - f. *If possible, areas broken down by demographics and – separately – incidences of crime*

- g. *If possible, areas broken down by*
 - i. *Total 911 CFS, by precedence*
 - ii. *Total on-view (officer initiated) activity*
- h. *Other GIS data/mapping requirements identified as relevant by FHPD leadership*

The assessment will be completed via online interviews with relevant personnel and a review of department documentation.

City Contract/Statement of Work Responsibilities

1. *Provide a single point of contact that will manage the project for the City. Tasks will include*
 1. *Communication with the Winbourne Consulting Project Manager*
 2. *Communication with relevant City personnel*
 3. *Scheduling meetings with City and other relevant personnel as needed*
 4. *Communicating with personnel to obtain information/data*
2. *Provide access to relevant personnel, including but not limited to:*
 1. *Command Staff personnel*
 1. *Department personnel - Training, Patrol, Investigations, Traffic, Records Management, Communications, etc.*
 2. *Subject matter expert personnel who can generate the required data*
 3. *Other personnel to be determined*
3. *Provide requested relevant documentation and data.*

Winbourne Consulting Contract/Statement of Work Deliverables

1. *Project plan and status reports as project tasks and milestones are completed*
2. *Develop findings and recommendations relevant to project objectives*
3. *Obtain input from relevant City personnel regarding the findings and recommendations*
4. *Complete a draft report of findings and recommendations to be reviewed by the City for accuracy*
5. *Submit a final report*
6. *Complete an in-person presentation of the report if needed*

Overview of Methodology

The Phase 2 analysis started in earnest with the conclusion of the Phase 1 report and City Council presentation on January 23, 2023.

The same methodology used for the Phase 1/Training Assessment was used for Phase 2/Traffic Stop and Citation Analysis. Activities included:

- A comprehensive information request for various types of data was provided to the FHPD project team
- Numerous subject matter experts were engaged:
 - FHPD
 - CLEMIS
 - District Court Administrator's Office

The findings and recommendations in this report are based on:

- Law enforcement standards, guidelines, and best practices
- Decades of law enforcement leadership and experience,
- Professional relationships with law enforcement experts throughout the USA.
- Our experience as advisors and participants in policy development for a large number of police agencies – of all sizes – over the last 30+ years
- To arrive at the Findings and Recommendations, the Winbourne Team conducted a comprehensive review of data and documents provided by FHPD. This document production was supplemented by the assessment team utilization of an additional ~990 pages of outside studies and external source material.
- National research outcomes were used where relevant as additional benchmarks and criteria. Throughout this study, the findings, and recommendations we present were compared and contrasted to research initiatives from the National Police Institute (formerly the National Police Foundation), the International Association of Chiefs of Police (IACP), the Bureau of Justice Assistance, DOJ Office of Community Oriented Policing Services, and other prominent academic and professional institutions.

Overarching Statement of Methodology and Basis for Conclusions

The conclusions, analysis, findings, and recommendations presented in this study should be considered under the rubric of this all-encompassing predicate:

Winbourne’s conclusions are based upon neutrality and the standard of objective reasonableness. Opinions presented in this report are held by their authors to a reasonable degree of professional certainty. They are based on education, training, and experience in active policing and knowledge of policing standards and best practices.

The basis for all opinions and conclusions in this assessment is founded upon decades of continuous professional experience, national/international public safety networks and relationships, participation in academic and organizational research studies and reform initiatives, and both past and contemporaneous engagement, evaluation, information-sharing, and study of policies/practices in police agencies of all sizes throughout the US.

This is not an academic study intended to conform to control/treatment analysis standards or the other conventions that characterize formal academic (e.g., university) research formats.

It is important to remember that research conclusions about traffic enforcement are seldom 100% definitive. Causations are rarely asserted, at least not without qualifications. For example, the arguments both supportive of traffic enforcement – specifically that it saves lives – and the arguments that question the efficacy, social injustice, and creation of public hazard through traffic enforcement are largely *correlative versus causal*.

Correlation does not equal causality, although a strong correlation is a good starting point. In the studies we reviewed, it was very rare to see a study on racial bias and unjust outcomes of traffic that includes arguments supportive of the need for traffic enforcement to save lives; just as rare is the study that balances the efficacy of traffic enforcement to save lives alongside the impact of biased stops in human tragedy.

As stated in the seminal report entitled What Works in Policing [NIJ, Sherman et al.], *“scientific knowledge is provisional. The most important limitation of science is that the knowledge it produces is always becoming more refined, and therefore, no conclusion is permanent. As the U.S. Supreme Court noted in its analysis of scientific evidence in the case of Daubert vs. Merrell Dow (1993),³ no theory (or program) of cause and effect can ever be proved to be true. It can only be disproved.”*

Lastly, generalizations are inherently uncertain. The rules of science are relatively clear about how to test cause and effect in any given study—a concept known as “internal validity.” The rules about how to judge how widely the results of any study may be generalized are far less clear, especially in social sciences—a concept known as “external validity.” How much a conclusion can be generalized requires continued testing: replication. *“Until replications become far more common in crime prevention evaluations, the field will continue to suffer from the uncertain external validity of both positive and negative findings”* [NIJ-Sherman et al.]

SUMMARY OF FINDINGS AND RECOMMENDATIONS

Findings and Recommendation #1 – FHPD Strengths

- At the outset, we note that the Farmington Hills Police Department (FHPD) has many strengths, enabling its leaders to address the challenges we discuss in this report. Among these strengths are:
 - FHPD’s conscientious, experienced, and principled leadership
 - A commitment to situational awareness
 - Rigorous and comprehensive training programs
 - Comprehensive accountability systems and implicit bias training
 - An institutional commitment to procedural justice and police legitimacy
 - Several strategic traffic safety and enforcement strategies, including commercial vehicle inspections and regulatory enforcement
 - Professional traffic stop protocol
 - Recent introduction of Automated License Plate Reader (ALPR) technology
- We find that FHPD is a progressive learning institution

Findings and Recommendation #2 – Exceed CALEA and Michigan Accreditation Standards

- FHPD’s Traffic Stop and Citation policies meet CALEA and Michigan Accreditation Standards
- It is important to note that state and national accreditation standards represent more of an essential baseline than a leading-edge body of policies and practices
- The need to address deficiencies and implement model practices is the focus of this Phase 2 report
- Winbourne believes that FHPD is well-positioned to achieve these goals

Findings and Recommendation #3 – Create a Formal Traffic Safety and Enforcement Protocol

- FHPD should create and implement a formal traffic safety and enforcement strategic protocol that is data-informed and focused upon life safety as the ultimate goal of enforcement, warnings, education, and prevention
- The development of the protocol should be a top priority
- The elements of this strategic protocol include:
 - Mission statement such as:
 - *FHPD is committed to uniformity and consistency in enforcement to obtain and promote voluntary compliance. To this end, FHPD focuses on opportunities for qualitative enforcement – centered on prevention and education through warnings and non-punitive contacts – as preferable over quantitative enforcement whenever practical*
 - Clarity regarding FHPD traffic enforcement objectives. For example, there can be a perceived belief that traffic enforcement in Farmington Hills is founded upon revenue generation
 - Establishing objectives focused on accidents, injuries, and inherently dangerous driving behaviors

- Using timely, reliable data and evidence to implement strategies and programs
- Focused personnel education and enforcement initiatives. Ensure personnel understand department objectives and what is not permissible (e.g., pre-textual traffic stops)
- FHPD has started completing components of this initiative, including the implementation of the Traffic Enforcement Plan for 2024

Findings and Recommendation #4 – Improve the Quality of FHPD Data

- The inability to obtain the required data for this project was a significant constraint. FHPD does not have any personnel with expert knowledge regarding CLEMIS system data and “Advanced” certification to mine CLEMIS data
- To successfully achieve a formal Traffic Safety and Enforcement strategy, FHPD needs access to quality data (e.g., timely, accurate, relevant, objective, and comprehensive)
- many factors influence the quality of data, including:
 - Department strategic objectives
 - Leadership direction
 - Change management culture of the department
 - Clear policies and procedures
 - Training personnel to proficiency
 - Capabilities of the various systems (e.g., Computer Aided Dispatch, Mobile application, Records Management System)
 - Formal Quality Assurance/Quality Improvement (QA/QI) process
- Virtually all law enforcement initiatives are data-driven, including Crime Analysis, COMPSTAT, Community Policing, and Intelligence Led Policing
- FHPD requires dedicated subject matter experts to leverage data from all FHPD systems
- A formal plan is required that includes:
 - FHPD subject matter expertise regarding Business Intelligence/Analytics. Note: This position requires specific mathematical and statistical analysis capabilities. It is not a position for the average police officer or IT technician
 - Workflows and business processes that leverage existing systems to acquire and analyze data
 - Clear direction regarding the type of information that should be collected
 - The types of reports and dashboards that should be published daily, weekly, monthly, etc.

Findings and Recommendation #5 – Fully Leverage CLEMIS System Capabilities

- Related to the above #4, a focused effort is needed to ensure FHPD has personnel that fully understand CLEMIS systems, including:
 - Capabilities and functionality
 - Current configuration
 - Opportunities to change or enhance the current configuration
- FHPD has already made progress regarding this recommendation. FHPD did not collect relevant information regarding each traffic stop and citation when the project started. FHPD personnel learned there was new CLEMIS Mobile system functionality, implemented an Activity Report pilot project, and subsequently expanded it to all patrol operations. Refer to the Activity Report section

Findings and Recommendation #6 – Implement a Consistency of Operations Initiative

- Problematic enforcement priorities and practices examined at length in this report include:
 - A lack of uniformity in issuing traffic citations between residents and non-residents
 - Random and uncoordinated enforcement patterns
 - Inconsistent and insufficient traffic safety staffing and deployment, particularly related to DUI and aggressive driving enforcement
 - Ambiguity about traffic enforcement as principally being a revenue generator
 - Outdated and high liability policies, particularly vehicular pursuits
- All these problematic practices should be addressed and changed
- A formal Traffic Enforcement and Safety Program is required to obtain Consistency of Operations from all personnel

Findings and Recommendation #7 – Enhance the Current Anti-Bias Initiative

- While FHPD has implemented an anti-bias initiative, some areas can be enhanced, including:
 - Random assessment of Body Worn Camera (BWC) videos related to traffic stops and citations
 - Generate reports to identify trends and patterns of the department, shifts, squads, and individual personnel
 - Management observation in the field
 - Community input and surveys
- Note: At the end of 2023, the City Council approved a five-year \$2.9M project to purchase a suite of Axon BWC applications and equipment. The suite includes applications that will significantly enhance FHPD's ability to implement an automated process to assess BWC video and audio information regarding BWC utilization, policies, procedures and anti-bias factors. FHPD expects the suite to be operational by June 2024

Findings and Recommendation #8 – Discontinue Issuing Impeding Traffic Citations When Not Appropriate

- The analysis identified the highest volume of traffic citations for 2020, 2021, and 2022 was Impeding Traffic, which is a less common traffic offense
- Research revealed the FHPD instructed personnel to write this citation going back to 2003
- In November 2023, the FHPD updated its policy regarding civil citations and specifically provided instructions on when impeding traffic should not be used. Refer to the Impeding Traffic section

Findings and Recommendation #9 – Continue the Focus on Commercial Vehicle Enforcement Program

- FHPD provided excellent information regarding the Traffic Unit's Commercial Vehicle Enforcement Program, which has a direct nexus to the safety of citizens

Findings and Recommendation #10 – Enhance Automated License Plate Reader Policy and Procedures

- The implementation of a robust Automated License Plate Reader (ALPR) system in Farmington Hills is both an opportunity and a potential risk
- If ALPR is not deployed with scrupulous neutrality and strategic necessity, the potential for both public objection and civil legal action are real risks
- At an operational level, absent clear protocols, there will be the potential for an increase in high-risk apprehension scenarios, particularly high-speed pursuits.

Summary of Traffic Stop and Citation Data

Important Note:

- As stated throughout the report, numerous issues were encountered in obtaining the required data. Many of the problems were not resolved, causing the report to be completed with the data provided
- There were numerous instances when data from one report did not match the same type of data in another report. This can be due to many factors, such as:
 - How each report is designed and configured
 - The timing of data between the two reports is not the same (e.g., one database has access to information another database does not)
 - There is a misinterpretation of what the data is
 - System error
- We could never obtain a clear explanation regarding the differences encountered. One example is the CLEMIS “ad hoc” reports did not match FHPD Transparency Dashboard data
- Refer to Section 2 of this report for detailed information. The following is a high-level summary of the data.

2006 to 2021 City of Farmington Hills Civil Infractions Issued

Findings: From 2006 to 2020, there was a consistent decline in the number of traffic citations issued

- 76.8% decrease from the high in 2017 (17,316) to the low in 2020 (4,012)
- Numerous reasons can be attributed to the decline in traffic citations
 - Traffic citations not emphasized FHPD management
 - Reduced overall staffing
 - Reduced number of Traffic Unit personnel
 - Prioritize Patrol personnel on being available for Calls for Service
 - COVID
 - Other
- Over the 2006 to 2020 time period, the FHPD Traffic Safety Section (TSS) has had different staffing levels. In 2008-2010, TSS was reduced from nine members to five members. FHPD advised efforts to increase the number of TSS personnel has been unsuccessful due to budget constraints. One position has been restored. The current TSS staffing level is six
- Traffic citations increased from 38.4% from 2020 (4,012) to 2022 (5,553)

Year Issued	Total Issued
2006	16,202
2007	17,316
2008	15,681
2009	13,317
2010	10,123
2011	8,773
2012	8,592
2013	9,349
2014	8,036
2015	6,753
2016	6,407
2017	8,269
2018	9,337
2019	7,310
2020	4,012
2021	5,326
2022	5,553

Reduction in Civil Fines Collected

Finding: from 2020 to 2022, Civil Fines collected had a 4.6% decrease.

- 2019 – 2020 \$135,601.50
- 2020 – 2021 \$124,931.50 7.9% reduction
- 2021 – 2022 \$129,318.75 3.5% increase

City of Farmington Hills			
47th District Court			
	2019-2020	2020-2021	2021-2022
Civil Fines	\$ 135,601.50	\$ 124,931.30	\$ 129,318.75
Court Costs & SVR	\$ 636,106.71	\$ 699,100.02	\$ 512,901.42
Probation	\$ 156,113.95	\$ 141,492.75	\$ 146,765.49
Presentence Invest	\$ 13,506.08	\$ 11,991.56	\$ 13,590.01
Ordinance Fees	\$ 709,899.23	\$ 609,578.60	\$ 623,194.61
Motor Carrier Fines	\$ 54,405.25	\$ 53,477.60	\$ 54,556.16
Parking Fines	\$ 14,440.00	\$ 8,780.00	\$ 8,545.00
Bond Forfeitures	\$ 8,912.00	\$ 4,565.80	\$ 16,153.80
Trust Fund Work Release (Total of City + Hills)	\$ 46,001.26	\$ 23,834.32	\$ 18,826.00
District Court Building Fund	\$ 60,606.13	\$ 54,578.80	\$ 52,692.00
	\$ 1,835,592.11	\$ 1,732,330.75	\$ 1,576,543.24

2020 – 2022 Three-Year Total

Year	Citations Issued	Increase From Previous Year
2020	4,012	
2021	5,326	32.8% increase
2022	5,553	4.3% increase

2020-2022 Citation Data by Month

Month	2020 TSTOP	2020 TIME SPENT HH:MM:SS	2021 TSTOP	2021 TIME SPENT HH:MM:SS	2022 TSTOP	2022 TIME SPENT HH:MM:SS
January	1,134	214:56:36	879	188:27:54	590	88:23:43
February	960	182:53:07	809	174:27:26	448	65:12:54
March	520	96:24:45	875	188:26:46	631	94:31:08
April	39	8:59:08	692	135:52:33	720	115:42:49
May	135	30:20:42	837	169:36:52	792	121:46:22
June	194	37:25:01	1,003	195:06:12	782	135:05:14
July	416	77:10:28	1,148	222:32:13	903	169:24:26
August	466	101:05:40	788	153:56:19	726	141:50:34
September	503	112:37:01	658	120:26:04	771	138:24:20
October	715	146:31:26	662	112:27:05	769	128:00:22
November	681	113:25:31	595	97:47:14	1,059	177:41:03
December	731	156:41:27	569	96:03:54	1,266	201:33:54
	6,494	1278:30:52	9,515	1855:10:32	9,457	1577:36:49

Traffic Citations Issued to City Residents and Non-Residents

Finding: A disproportionate number of traffic citations are issued to non-residents versus City residents.

2020

- Total civil infractions issued 4,012
- Residents 1,003 25%
- Non-Residents 3,009 75%

2021

- Total civil infractions issued 5,326
- Residents 1,148 21.6%
- Non-Residents 4,178 78.4%

2022

- Total civil infractions issued 5,553
- Residents 1,392 25%
- Non-Residents 4,161 75%

Traffic Stop Use of Force Incidents

Finding: From 2020 to 2022, there has been an increase in Use of Force incidents related to Traffic Stops:

- 2020 6.5%
- 2021 11.1%
- 2022 19.2%

Data Source: Phase II - 1. CAD Breakdown e. Traffic Stops Use of Force data 2020
i. Breakdown of Use of Force incidents for types of activity including traffic stops.

Year	Use of Force Incidents	Traffic Stop Related
2020	62	4 (6.5%)
2021	63	7 (11.1%)
2022	73	14 (19.2%)

Traffic Stop Vehicle Pursuit Incidents

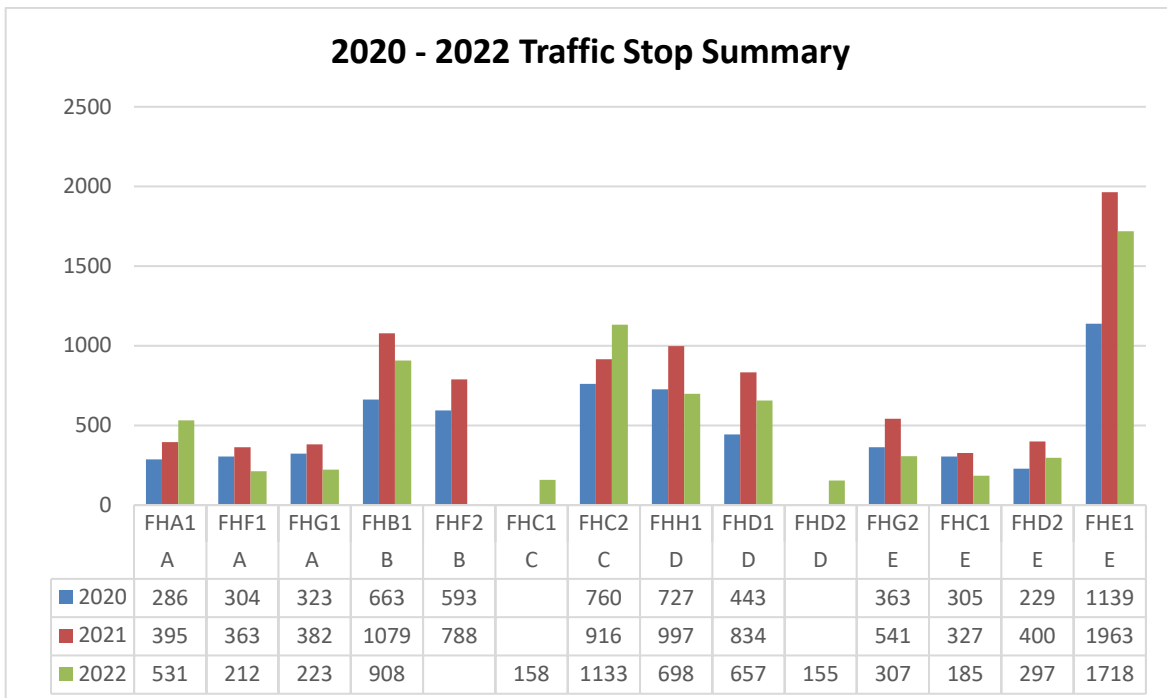
FHPD reported there were no vehicle pursuits as a result of a traffic stop in 2020, 2021, and 2022.

2020 – 2022 FHPD Traffic Stop Summary District, Beat and Zone

Finding: There are a significantly higher number of traffic stops in Area E than in other FHPD Beats.

- Area E includes Interstate 696 and 11 Mile Road, which are the primary roadways used to travel between Detroit and Novi, where there are numerous malls and stores

Area Code	Beat Code	2020	2021	2022
A	FHA1	286	395	531
A	FHF1	304	363	212
A	FHG1	323	382	223
B	FHB1	663	1079	908
B	FHF2	593	788	
C	FHC1			158
C	FHC2	760	916	1133
D	FHH1	727	997	698
D	FHD1	443	834	657
D	FHD2			155
E	FHG2	363	541	307
E	FHC1	305	327	185
E	FHD2	229	400	297
E	FHE1	1139	1963	1718
TOTAL		6135	8985	7182



2020-2022 Traffic Stop by Shift

Farmington Hills Police Department 2020-2022 Traffic Stop by Shift			
	2020	2021	2022
1 - 7:00 AM - 3:00 PM	2434	3611	3965
2 - 3:00 PM to 11:00 PM	2421	3665	3599
3 - 11:00 PM to 7:00 AM	1639	2239	1893
Grand Total	6494	9515	9457
	2020	2021	2022

2020 – 2022 Averages - FHPD Traffic Stop Summary District, Beat and Zone

Farmington Hills Police Department Traffic Stop Summary by District, Beat, Zone							
Area Code	Beat Code	2020	Avg/Month	2021	Avg/Month	2022	Avg/Month
A	FHA1	286	24	395	33	531	44
A	FHF1	304	25	363	30	212	18
A	FHG1	323	27	382	32	223	19
B	FHB1	663	55	1079	90	908	76
B	FHF2	593	49	788	66		0
C	FHC1		0		0	158	13
C	FHC2	760	63	916	76	1133	94
D	FHH1	727	61	997	83	698	58
D	FHD1	443	37	834	70	657	55
D	FHD2		0		0	155	13
E	FHG2	363	30	541	45	307	26
E	FHC1	305	25	327	27	185	15
E	FHD2	229	19	400	33	297	25
E	FHE1	1139	95	1963	164	1718	143
TOTAL		6135		8985		7182	

2020 – 2022 Top 20 Citations

Farmington Hills Police Department 2020 – 2022 Citations Top 20 Violations Issued by Year						
Order	2021		2021		2022	
1	SPEEDING - (NON-EXPRESSWAY) 01-5 OVER	565	IMPEDED TRAFFIC	1093	IMPEDED TRAFFIC	1206
2	IMPEDED TRAFFIC	563	SPEEDING - (NON-EXPRESSWAY) 01-5 OVER	593	PLATE - EXPIRED PLATE	1146
3	INSURANCE - NO PROOF OF INSURANCE	265	LICENSE - DROVE WHILE LICENSE SUSP/REV/DENIED	441	INSURANCE - NO PROOF OF INSURANCE	624
4	LICENSE - DROVE WHILE LICENSE SUSP/REV/DENIED	263	PLATE - EXPIRED PLATE	409	SPEEDING - (NON-EXPRESSWAY) 01-5 OVER	461
5	DISOBEYED TRAFFIC SIGNAL	230	INSURANCE - NO PROOF OF INSURANCE	383	LICENSE - DROVE WHILE LICENSE SUSP/REV/DENIED	442
6	ACCIDENT - FAILED TO STOP WITHIN ASSURED CLEAR DISTANCE AHEAD	210	ACCIDENT - FAILED TO STOP WITHIN ASSURED CLEAR DISTANCE AHEAD	339	SPEEDING - (NON-EXPRESSWAY) 16-20 OVER	385
7	DISOBEYED TRAFFIC CONTROL DEVICE	190	SPEEDING - (NON-EXPRESSWAY) 16-20 OVER	284	ACCIDENT - FAILED TO STOP WITHIN ASSURED CLEAR DISTANCE AHEAD	335
8	SPEEDING - XWAY 01-5 OVER	190	DISOBEYED TRAFFIC CONTROL DEVICE	266	REGISTRATION - EXPIRED REGISTRATION	287
9	DOMESTIC VIOLENCE	181	DISOBEYED TRAFFIC SIGNAL	266	SPEEDING - (NON-EXPRESSWAY) 11-15 OVER	283
10	SPEEDING - (NON-EXPRESSWAY) 16-20 OVER	181	IMPROPER LANE USE	198	DISOBEYED TRAFFIC CONTROL DEVICE	281
11	PLATE - EXPIRED PLATE	169	SPEEDING - XWAY 01-5 OVER	178	DISOBEYED TRAFFIC SIGNAL	204
12	EQUIPMENT VIOLATION - TAIL LIGHTS	114	SPEEDING - (NON-EXPRESSWAY) 11-15 OVER	153	INSURANCE - VIOLATION OF NO INSURANCE - CI	201
13	LOMC - EQUIPMENT VIOLATION	112	DOMESTIC VIOLENCE	152	SPEEDING - XWAY 16-25 OVER	198
14	SPEEDING - (NON-EXPRESSWAY) 06-10 OVER	112	EQUIPMENT VIOLATION - TAIL LIGHTS	149	DOMESTIC VIOLENCE	176
15	SPEEDING - XWAY 16-25 OVER	111	SPEEDING - XWAY 16-25 OVER	147	EQUIPMENT VIOLATION - TAIL LIGHTS	163
16	PARKING - HANDICAP PARKING	108	LOMC - EQUIPMENT VIOLATION	123	ACCIDENT - FAILED TO OBSERVE TRAFFIC	115
17	SPEEDING - XWAY 26+ OVER	108	INSURANCE - VIOLATION OF NO INSURANCE - CI	122	EQUIPMENT VIOLATION	112
18	EQUIPMENT VIOLATION	107	REGISTRATION - EXPIRED REGISTRATION	114	ACCIDENT - FAILED TO YIELD FOR ONCOMING TRAFFIC	98
19	SPEEDING - (NON-EXPRESSWAY) 11-15 OVER	107	SPEEDING - (NON-EXPRESSWAY) 21-25 OVER	112	SPEEDING - (NON-EXPRESSWAY) 21-25 OVER	96

20	DISOBEYED STOP SIGN	89	ACCIDENT - FAILED TO OBSERVE TRAFFIC	101	DISOBEYED STOP SIGN	93
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2020 Top 20 Citations Issued Data Table

Farmington Hills Police Department								
2020 Citations								
Monthly Violations Summary								
Additional Violations Option								
	Violation Description	Viol Count	Percentage	Accident	Shift One	Shift Two	Shift Three	Shift Four
1	SPEEDING - (NON-EXPRESSWAY) 01-5 OVER	565	9.22%	0	163	201	201	0
2	IMPEDED TRAFFIC	563	9.19%	8	96	225	242	0
3	INSURANCE - NO PROOF OF INSURANCE	265	4.33%	46	63	118	84	0
4	LICENSE - DROVE WHILE LICENSE SUSP/REV/DENIED	263	4.29%	24	69	114	80	0
5	DISOBEYED TRAFFIC SIGNAL	230	3.76%	0	63	123	44	0
6	ACCIDENT - FAILED TO STOP WITHIN ASSURED CLEAR DISTANCE AHEAD	210	3.43%	198	55	71	84	0
7	DISOBEYED TRAFFIC CONTROL DEVICE	190	3.10%	1	35	67	88	0
8	SPEEDING - XWAY 01-5 OVER	190	3.10%	0	64	107	19	0
9	DOMESTIC VIOLENCE	181	2.96%	0	49	73	59	0
10	SPEEDING - (NON-EXPRESSWAY) 16-20 OVER	181	2.96%	0	42	96	43	0
11	PLATE - EXPIRED PLATE	169	2.76%	2	59	73	37	0
12	EQUIPMENT VIOLATION - TAIL LIGHTS	114	1.86%	0	19	34	61	0
13	LOMC - EQUIPMENT VIOLATION	112	1.83%	0	0	36	76	0
14	SPEEDING - (NON-EXPRESSWAY) 06-10 OVER	112	1.83%	0	24	48	40	0
15	SPEEDING - XWAY 16-25 OVER	111	1.81%	1	15	76	20	0
16	PARKING - HANDICAP PARKING	108	1.76%	0	12	45	18	0
17	SPEEDING - XWAY 26+ OVER	108	1.76%	0	42	54	12	0
18	EQUIPMENT VIOLATION	107	1.75%	0	31	57	19	0
19	SPEEDING - (NON-EXPRESSWAY) 11-15 OVER	107	1.75%	0	23	52	32	0
20	DISOBEYED STOP SIGN	89	1.45%	1	25	40	24	0

2021 Top 20 Citations Issued Data Table

Farmington Hills Police Department 2021 Citations Monthly Violations Summary Additional Violations Option								
	Violation Description	Viol Count	Percentage	Accident	Shift One	Shift Two	Shift Three	Shift Four
1	IMPEDED TRAFFIC	1093	12.94%	27	293	419	381	0
2	SPEEDING - (NON-EXPRESSWAY) 01-5 OVER	593	7.02%	0	209	196	188	0
3	LICENSE - DROVE WHILE LICENSE SUSP/REV/DENIED	441	5.22%	26	152	174	115	0
4	PLATE - EXPIRED PLATE	409	4.84%	14	141	190	77	0
5	INSURANCE - NO PROOF OF INSURANCE	383	4.53%	49	129	151	103	0
6	ACCIDENT - FAILED TO STOP WITHIN ASSURED CLEAR DISTANCE AHEAD	339	4.01%	315	126	120	93	0
7	SPEEDING - (NON-EXPRESSWAY) 16-20 OVER	284	3.36%	0	121	103	60	0
8	DISOBEYED TRAFFIC CONTROL DEVICE	266	3.15%	3	47	97	122	0
9	DISOBEYED TRAFFIC SIGNAL	266	3.15%	2	111	108	47	0
10	IMPROPER LANE USE	198	2.34%	0	84	64	50	0
11	SPEEDING - XWAY 01-5 OVER	178	2.11%	1	73	56	49	0
12	SPEEDING - (NON-EXPRESSWAY) 11-15 OVER	153	1.81%	0	43	80	30	0
13	DOMESTIC VIOLENCE	152	1.80%	0	52	53	47	0
14	EQUIPMENT VIOLATION - TAIL LIGHTS	149	1.76%	0	71	41	37	0
15	SPEEDING - XWAY 16-25 OVER	147	1.74%	0	36	28	83	0
16	LOMC - EQUIPMENT VIOLATION	123	1.46%	0	0	49	74	0
17	INSURANCE - VIOLATION OF NO INSURANCE - CI	122	1.44%	16	55	39	28	0
18	REGISTRATION - EXPIRED REGISTRATION	114	1.35%	2	29	47	38	0
19	SPEEDING - (NON-EXPRESSWAY) 21-25 OVER	112	1.33%	0	52	34	26	0
20	ACCIDENT - FAILED TO OBSERVE TRAFFIC	101	1.20%	97	27	66	8	0

2022 Top 20 Citations Issued Data Table

Farmington Hills Police Department								
2022 Citations								
Monthly Violations Summary								
Additional Violations Option								
	Violation Description	Viol Count	Percentage	Accident	Shift One	Shift Two	Shift Three	Shift Four
1	PLATE - EXPIRED PLATE	306	11.31%	5	119	54	76	0
2	IMPEDED TRAFFIC	269	9.94%	2	58	29	151	0
3	LICENSE - DROVE WHILE LICENSE SUSP/REV/DENIED	183	6.76%	6	90	36	26	0
4	INSURANCE - NO PROOF OF INSURANCE	176	6.50%	11	86	20	37	0
5	SPEEDING - (NON-EXPRESSWAY) 01-5 OVER	124	4.58%	0	49	43	11	0
6	DOMESTIC VIOLENCE	108	3.99%	0	45	33	23	0
7	SPEEDING - (NON-EXPRESSWAY) 16-20 OVER	104	3.84%	0	25	21	40	0
8	ACCIDENT - FAILED TO STOP WITHIN ASSURED CLEAR DISTANCE AHEAD	72	2.66%	68	30	11	19	0
9	SPEEDING - XWAY 16-25 OVER	69	2.55%	0	8	0	60	0
10	REGISTRATION - EXPIRED REGISTRATION	67	2.48%	1	17	20	16	0
11	SPEEDING - (NON-EXPRESSWAY) 11-15 OVER	64	2.37%	0	21	18	13	0
12	INSURANCE - VIOLATION OF NO INSURANCE - CI	58	2.14%	6	29	13	11	0
13	EQUIPMENT VIOLATION - TAIL LIGHTS	55	2.03%	0	28	9	5	0
14	LOMC - EQUIPMENT VIOLATION	52	1.92%	3	11	0	41	0
15	DISOBEYED TRAFFIC SIGNAL	45	1.66%	0	17	15	6	0
16	ALCOHOL - OPERATING WHILE INTOXICATED 1ST OFFENSE	40	1.48%	11	19	10	6	0
17	DISOBEYED TRAFFIC CONTROL DEVICE	34	1.26%	0	17	3	8	0
18	ALCOHOL - OPEN INTOXICANTS IN VEH/DRIVER	33	1.22%	4	20	5	5	0
19	REGISTRATION - NO REGISTRATION ON PERSON	31	1.15%	0	15	11	3	0
20	EQUIPMENT VIOLATION	30	1.11%	0	18	9	2	0

2020 – 2022 Traffic Stop Search Data Table

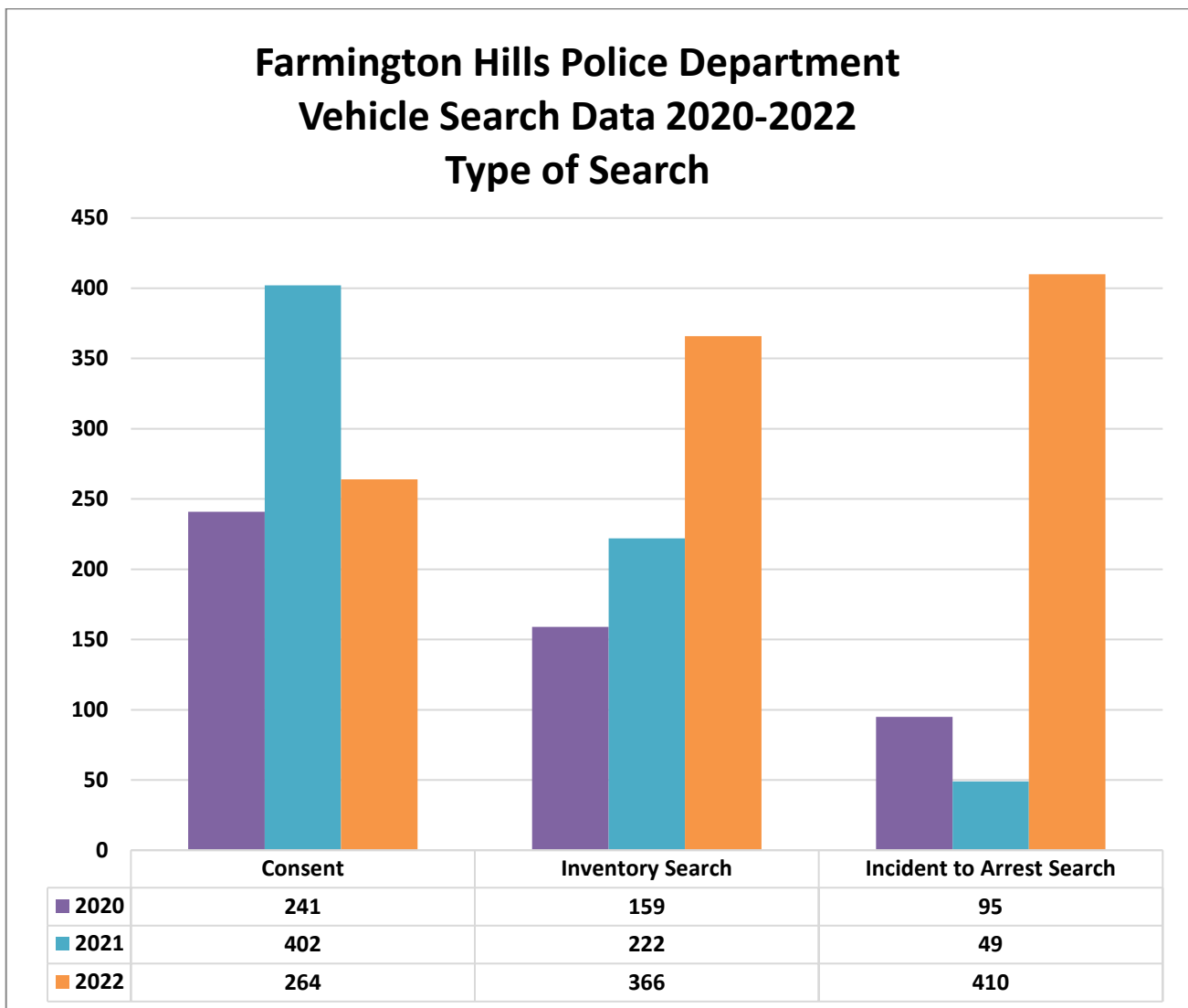
Note:

- Neither CAD nor Business Objects have a program to run a report for the Farmington Hills Police Department to collect Vehicle Search Data
- FHPD used light duty and new police officers to search through information to obtain the data manually

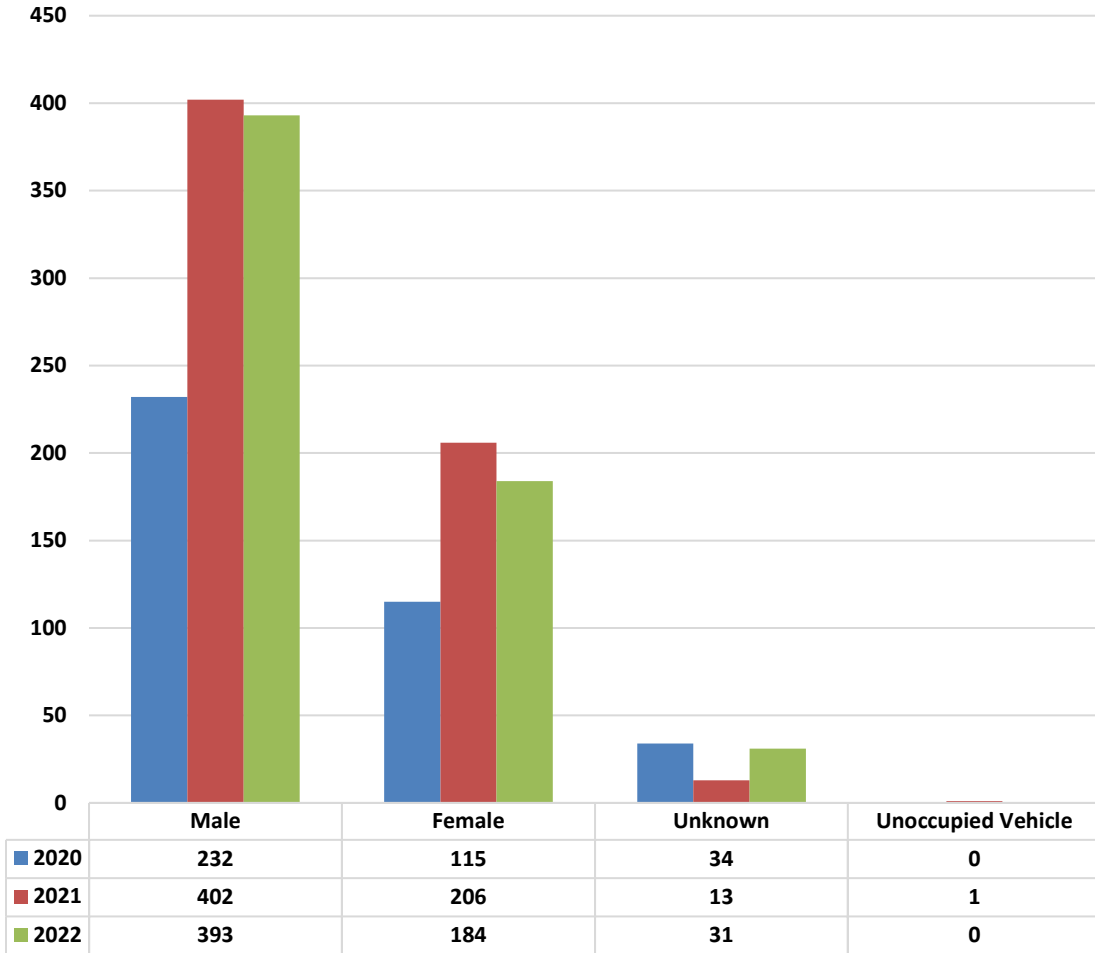
	2020	2021	2022
Traffic Stops	364	589	564
Vehicle Searches	337	560	565
Person Searches	280	301	496
Type of Search	2020	2021	2022
Consent	241	402	264
Inventory Search	159	222	366
Incident to Arrest Search	95	49	410
Driver Sex	2020	2021	2022
Male	232	402	393
Female	115	206	184
Unknown	34	13	31
Unoccupied Vehicle	0	1	0
Driver Race	2020	2021	2022
White	125	209	192
Black	221	386	375
Indian	0	0	1
Asian	0	0	4
Unknown	36	20	36
Unoccupied Vehicle	0	1	0
Driver Race/Sex	2020	2021	2022
White Male	85	140	132
Black Male	145	253	250
White Female	37	67	56
Black Female	75	131	126
Passenger Sex	2020	2021	2022
Male	40	89	67
Female	31	77	63

Unknown	0	0	0
Passenger Race	2020	2021	2022
White	23	53	50
Black	37	109	68
Asian	0	1	0
Unknown	3	3	0

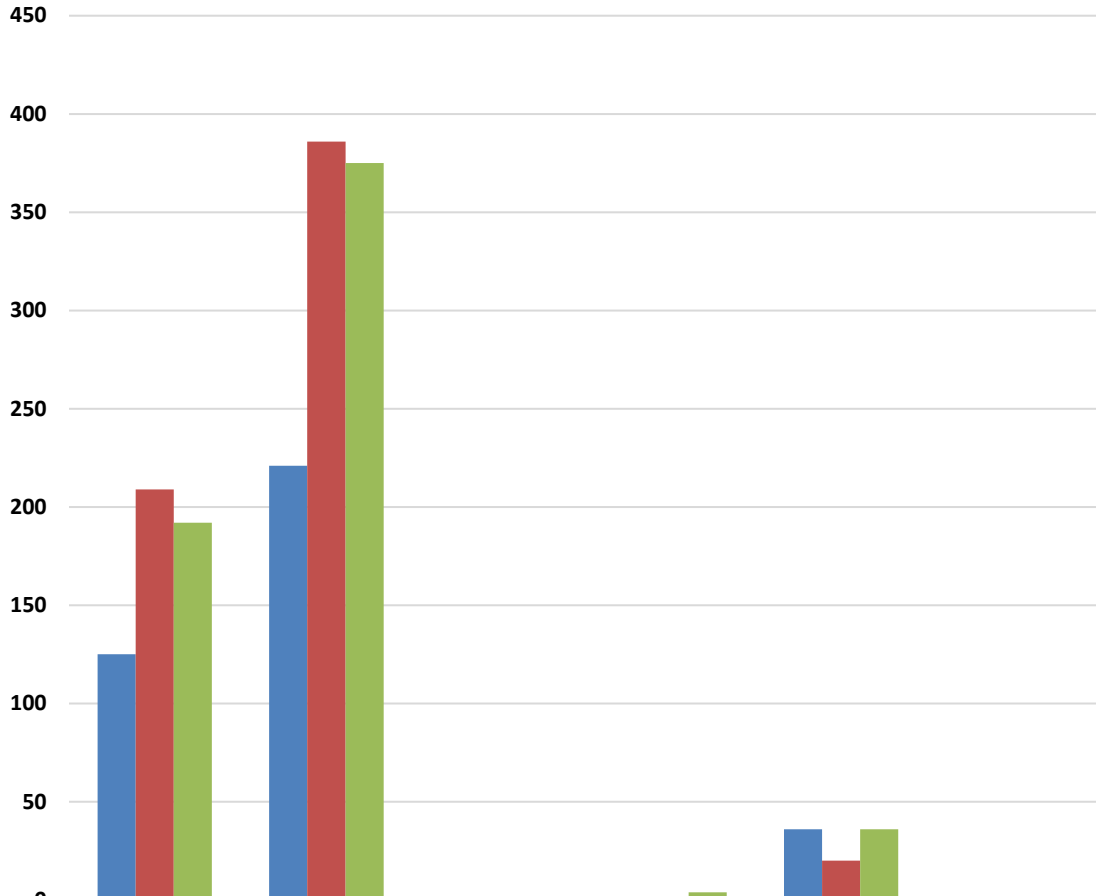
2020 – 2022 Traffic Stop Search Bar Charts



**Farmington Hills Police Department
 Vehicle Search Data 2020-2022
 Driver Gender**

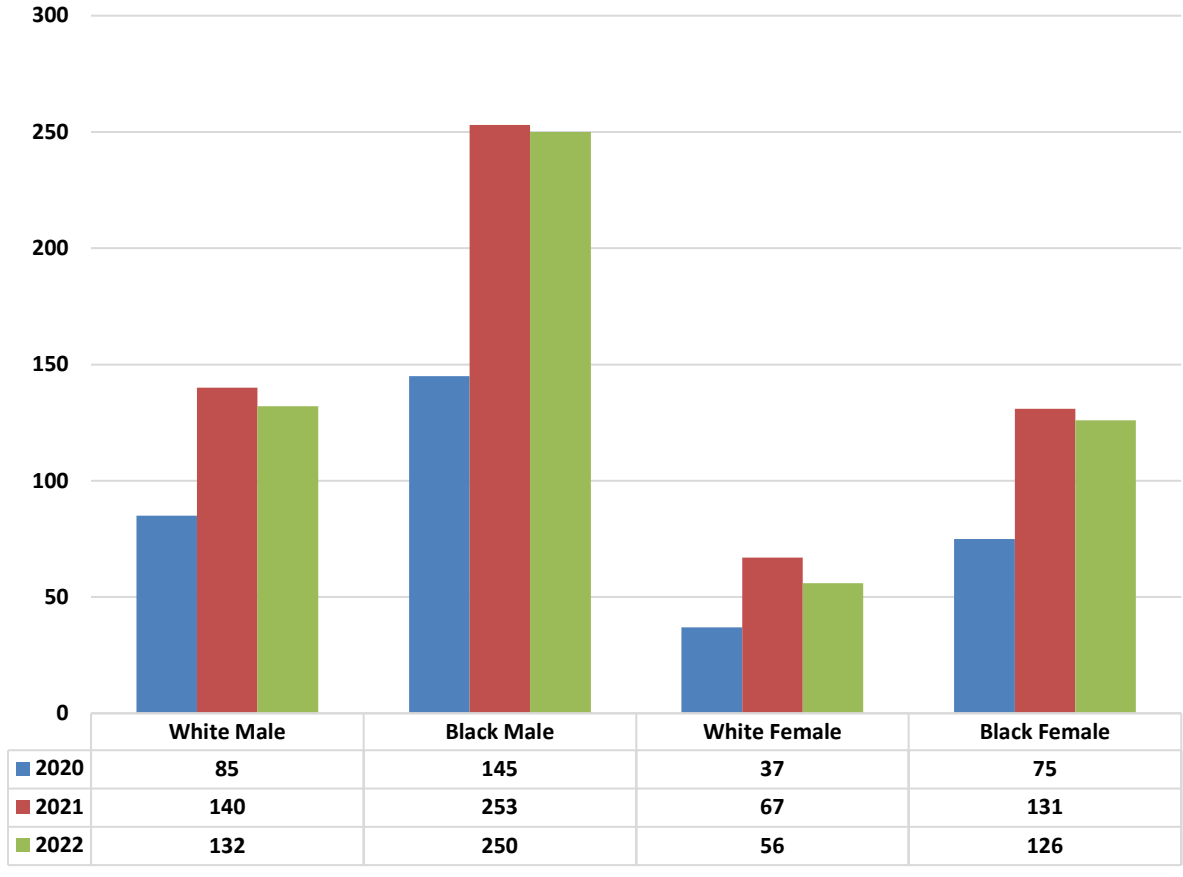


**Farmington Hills Police Department
 Vehicle Search Data 2020-2022
 Driver Race**

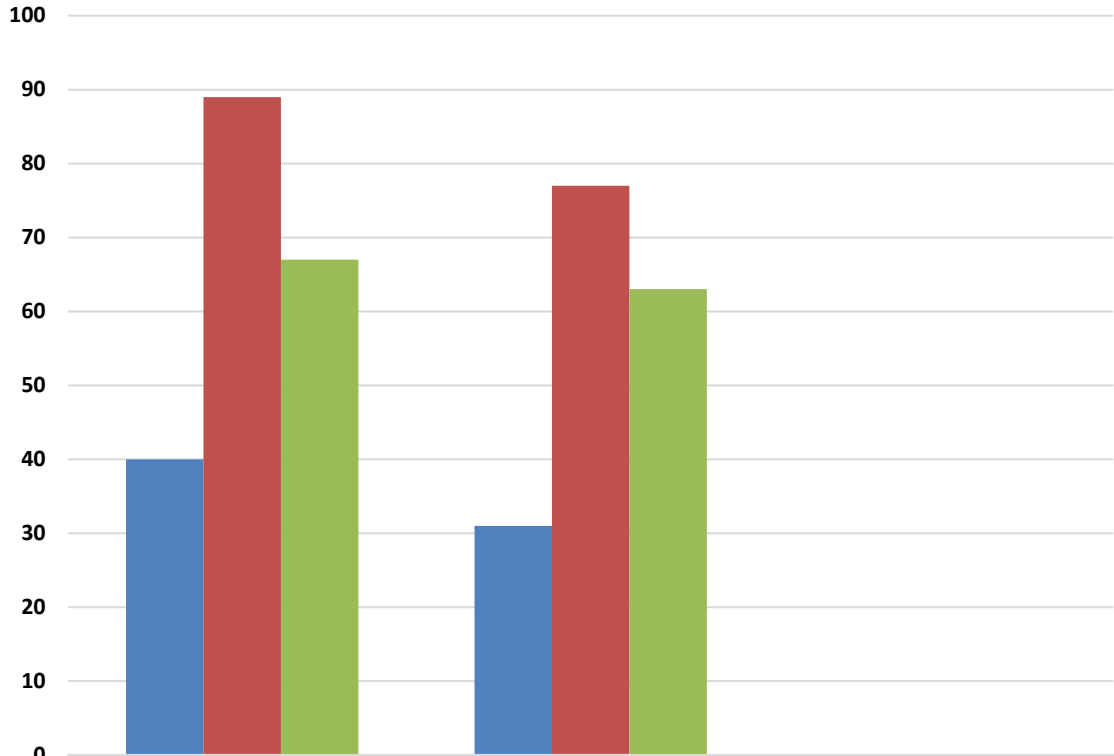


	White	Black	Indian	Asian	Unknown	Unoccupied Vehicle
2020	125	221	0	0	36	0
2021	209	386	0	0	20	1
2022	192	375	1	4	36	0

**Farmington Hills Police Department
 Vehicle Search Data 2020-2022
 Driver Race/Gender**

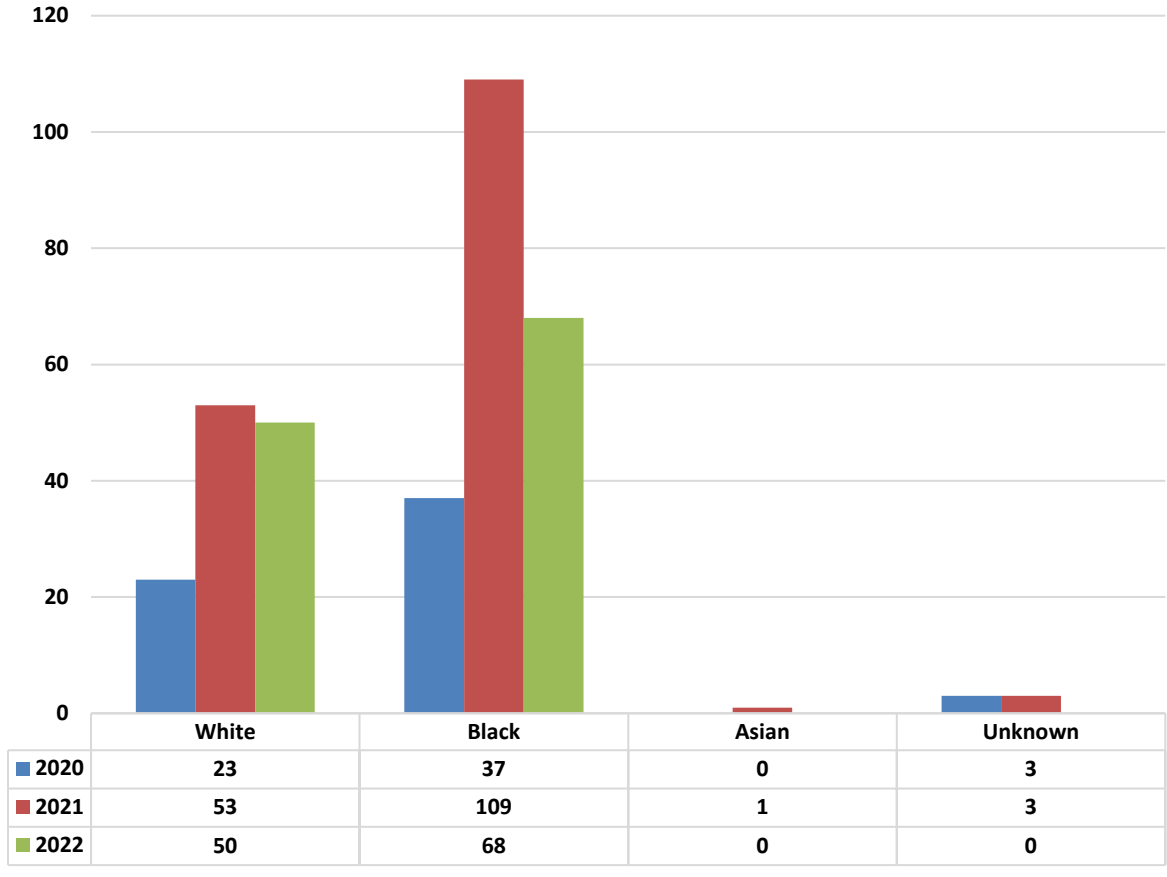


**Farmington Hills Police Department
 Vehicle Search Data 2020-2022
 Passenger Gender**



	Male	Female	Unknown
2020	40	31	0
2021	89	77	0
2022	67	63	0

**Farmington Hills Police Department
 Vehicle Search Data 2020-2022
 Passenger Race**



2022 Vehicle Search - Race/Gender Comparison Data

Farmington Hills Police Department 2022 Vehicle Search Comparison									
2022	Arrest	Consent	Consent/ Inventory	Inventory	Incident to Arrest	Incident to Arrest/ Inventory	Incident to Arrest/ Consent	Incident to Arrest, Consent, Inventory	Incident to Arrest/ Search Warrant
White Male	0	18	8	7	10	52	20	4	0
Black Male	0	57	20	27	10	81	34	6	1
Asian Male	0	2	0	0	0	1	0	0	0
Indian Male	0	1	0	0	0	0	0	0	0
White Female	0	6	3	2	2	27	11	2	0
Black Female	0	24	10	6	2	50	20	0	0
Asian Female	0	0	0	0	0	0	0	0	0
Unknown Male	0	1	0	0	0	0	3	0	0
Unknown Female	0	0	0	0	0	2	0	0	0
Unknown Unknown	0	0	0	1	3	20	4	0	0

2022 Vehicle Search - Age Comparison Data

Farmington Hills Police Department 2022 Vehicle Search Comparison Age Data									
Age	Drivers	W/M	B/M	W/F	B/F	Other	Unk/M	Unk/F	Unk/Unk
Below 16	0	0	0	0	0	0	0	0	0
16-19	21	4	9	0	6	0	1	1	0
20-29	228	42	106	18	58	2	1	1	0
30-39	179	40	79	22	35	1	1	0	1
40-49	76	24	32	6	14	0	0	0	0
50+	74	26	24	11	11	1	1	0	0
Age Unknown	30	0	0	0	0	0	0	1	29
TOTAL	608	136	250	57	124	4	4	3	30

FHPD Patrol Beat Demographic Data

Note: Data provided by FHPD.

Patrol Beat A Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat A	
White	8,110
Black/African American	2,113
Hispanic or Latino	391
Asian	1,854
American Indian or Alaskan Native	8
Hawaiian or Pacific Islander	1

Patrol Beat B Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat B	
White	7,287
Black/African American	2,080
Hispanic or Latino	310
Asian	2,681
American Indian or Alaskan Native	21
Hawaiian or Pacific Islander	2

Patrol Zone C Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat C	
White	5,355
Black/African American	2,497
Hispanic or Latino	383
Asian	468
American Indian or Alaskan Native	25
Hawaiian or Pacific Islander	8

Patrol Beat D Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat D	
White	3,859
Black/African American	1,324
Hispanic or Latino	154
Asian	452
American Indian or Alaskan Native	10
Hawaiian or Pacific Islander	2

Patrol Beat E Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat E	
White	6,536
Black/African American	2,293
Hispanic or Latino	276
Asian	1,500
American Indian or Alaskan Native	26
Hawaiian or Pacific Islander	2

Patrol Beat F Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat F	
White	4,607
Black/African American	1,901
Hispanic or Latino	360
Asian	4,526
American Indian or Alaskan Native	25
Hawaiian or Pacific Islander	3

Patrol Beat G Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat G	
White	9,042
Black/African American	1,940
Hispanic or Latino	344
Asian	781
American Indian or Alaskan Native	13
Hawaiian or Pacific Islander	4

Patrol Beat H Data Table

Farmington Hills Police Department Demographic Data by Patrol Beat H	
White	5,333
Black/African American	1,218
Hispanic or Latino	423
Asian	619
American Indian or Alaskan Native	19
Hawaiian or Pacific Islander	-

DEMOGRAPHIC INFORMATION

City of Farmington Hills

Data Source: FHPD SharePoint - P2 4.a and Wikipedia

The city demographics for the City of Farmington Hills are detailed below. The data used was collected from the US Census Bureau in 2020. The City of Farmington Hills has a population base of 83,292 residents.

A breakdown by age and sex are the following:

- Female 51.4%
- Male 48.6%
- Under five years of age 5.1%
- Under 18 years of age 18.0%
- Persons 65 years and over 20.4%
- Ages 18 to 64 56.5%

A Breakdown by Race:

- White Alone 62.1%
- Black or African American Alone 18.5%
- American Indian and Alaska Native 0.3%
- Asian Alone 14.3%
- Native Hawaiian Pacific Other 0.1%
- Two or More Races 4.0%
- Hispanic or Latino 2.3%
- Foreign Born Persons 21.1%

The City of Farmington Hills is 33.2 square miles in geographic size and is broken up into 36 one-square-mile geographical areas for mapping purposes.

The City of Farmington, which has its own Public Safety Department, is within the City of Farmington Hills and consists of 2.8 square miles.

Farmington Hills has numerous significant roadways that run through its city boundaries. These roadways include Grand River Avenue, Northwestern Highway, Twelve Mile Road, and Orchard Lake Road. In addition to these major surface streets, expressways 1-696, M-5, and 1-275 also run through the city's boundaries.

Adjacent Cities

Source of Data: Wikipedia

Detroit Metropolitan Area

- *Metro Detroit is a major metropolitan area in the U.S. state of Michigan, consisting of the city of Detroit and over 200 municipalities*
- *The Office of Management and Budget (OMB), a federal agency of the United States, defines the Detroit–Warren–Dearborn Metropolitan Statistical Area (MSA) as the six counties of Lapeer, Livingston, Macomb, Oakland, St. Clair, and Wayne. As of the 2010 census, the MSA had a population of 4,296,250 with an area of 3,913 square miles (10,130 km²)*
- *As of the census of 2010, there were 4,296,250 people, 1,682,111 households, and 1,110,454 families residing within the metropolitan statistical area. The census reported 70.1% White, 22.8% African American, 0.3% Native American, 3.3% Asian, 0.02% Pacific Islander, 1.2% from other races, and 2.2% from two or more races. Hispanic or Latino of any race were 6.2% of the population. Arab Americans were at least 4.7% of the region's population (considered white in the US Census)*

Detroit

- *2020 Census*
 - *Population of 639,111*
 - *Racial makeup – African American 77.7%, White 14.7%, Hispanic or Latino 8.0%, Asian 1.6%*

Farmington City

- *2020 Census*
 - *Population 11,594*
 - *The city's racial makeup was 71.5% White, 11.4% African American, 0.4% Native American, 13.9% Asian, 0.1% Pacific Islander, 0.4% from other races, and 2.1% from two or more races. Hispanic or Latino of any race were 2.1% of the population.*

Southfield

- *2020 Census*
 - *Population 76,618*
 - *The racial makeup of the city was 70.10% African American, 21.05% White, 0.17% Native American, 2.34% Asian, Hispanic, or Latino 2.23, Other races 4.1%*

Redford Township

- 2020 Census
 - Population 48,492
 - The ethnic and racial makeup of the population was 44.1% white, 50.2% African American, 0.4% Asian, 3.0% reporting more than one race, and 5.5% Hispanic. White alone non-Hispanic accounted for 41.5 % of the population

Novi

- 2020 Census
 - Population 66,243
- 2010 Census
 - The city's racial makeup was 73.0% White, 8.1% African American, 0.2% Native American, 15.9% Asian, 0.7% from other races, and 2.1% from two or more races. Hispanic or Latino people of any race were 3.0% of the population.

Livonia

- 2020 Census
 - Population 95,535
- 2010 Census
 - The city's racial makeup was 92.0% White, 3.4% African American, 0.2% Native American, 2.5% Asian, 0.4% from other races, and 1.4% from two or more races. Hispanic or Latino people of any race were 2.5% of the population

West Bloomfield

- 2020 Census
 - Population 65,888
- 2010 Census
 - The racial makeup of the township was 77.6% White, 11.4% Black or African American, 0.1% Native American, 8.4% Asian, 0.02% Pacific Islander, 0.4% from other races, and 2.0% from two or more races. Hispanic or Latino of any race were 1.60% of the population

Troy

- 2020 Census
 - Population 87,294
 - The city's racial makeup was 62.2% White, 4.0% African American, 0% Native American, 27.3% Asian, 1.2% from other races, and 5.1% from two or more races. Hispanic or Latino residents of any race were 2.2% of the population

FHPD DISTRICTS AND BEATS

FHPD Districts

Data Source: Phase II – 1. CAD Breakdown. A. Traffic Stops (2021 data)

ii Traffic Stops by District, Beat, Zone.

The City of Farmington Hills is 33.3 square miles (8 Mile Rd to 14 Mile Rd and Haggerty Rd to Inkster Rd).

The Farmington Hills Police Department has divided the jurisdiction into an eight (8) Car District Plan (Districts A, B, C, D, E, F, G, and H) on Day Shift and Afternoon Shift.

On the Midnight Shift, the five (5) Car District Plan is utilized, consuming Districts F, G, and H into Districts A, B, C, D, and E.

The Operations Division currently operates with three different shifts,

- *Midnight Shift 2300 - 0700 hours*
- *Day Shift 0700 - 1500 hours*
- *Afternoon Shift 1500 - 2300 hours*

There is also a Night Support Shift that runs from 1900 - 0300 hours but falls under the command of the respective shift depending on the time of the incident (Afternoon or Midnight Shift).

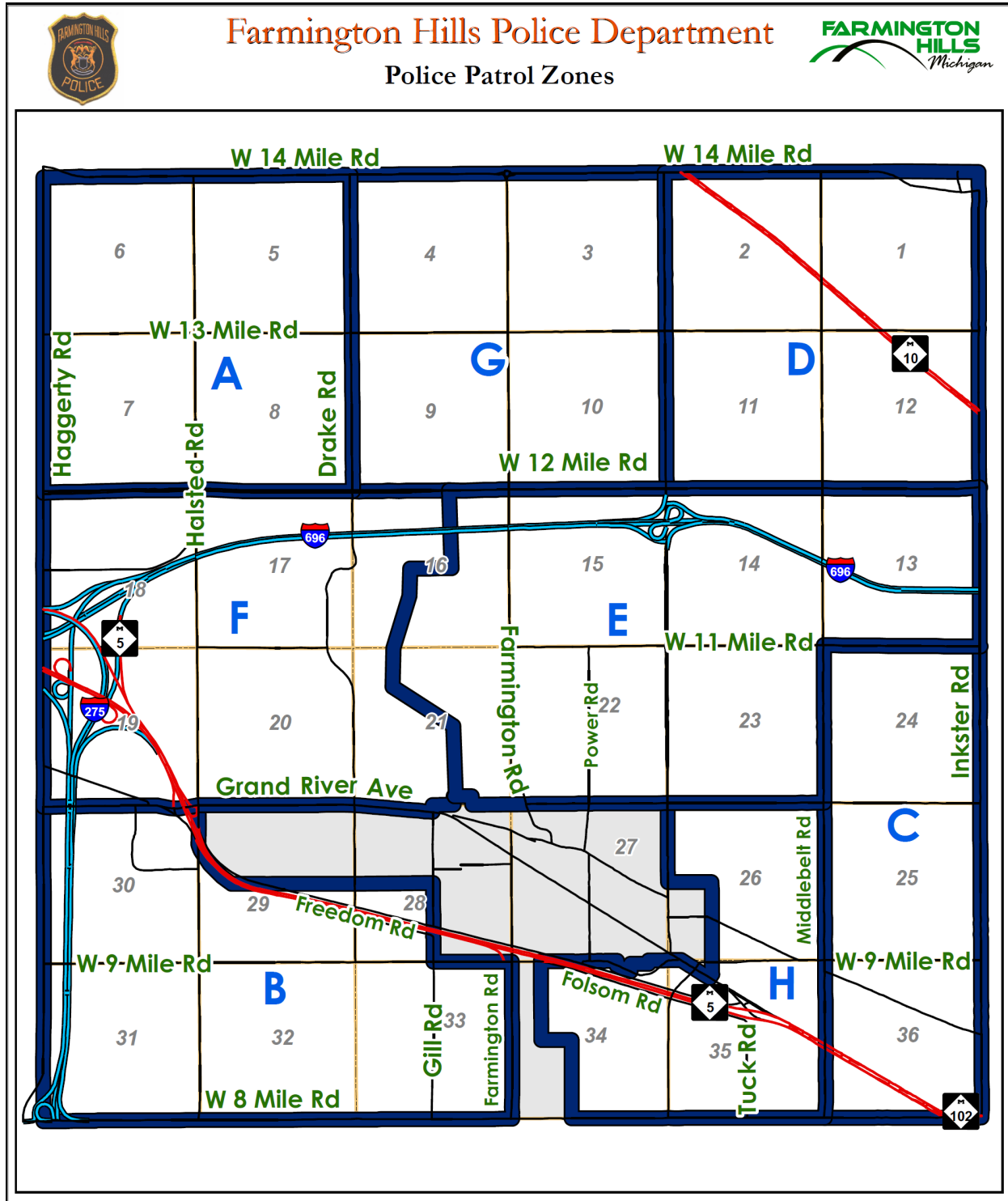
Additionally, there is a Traffic Section (various hours and events) and a Directed Patrol Unit (DPU) that may also conduct traffic stops.

FHPD Beats

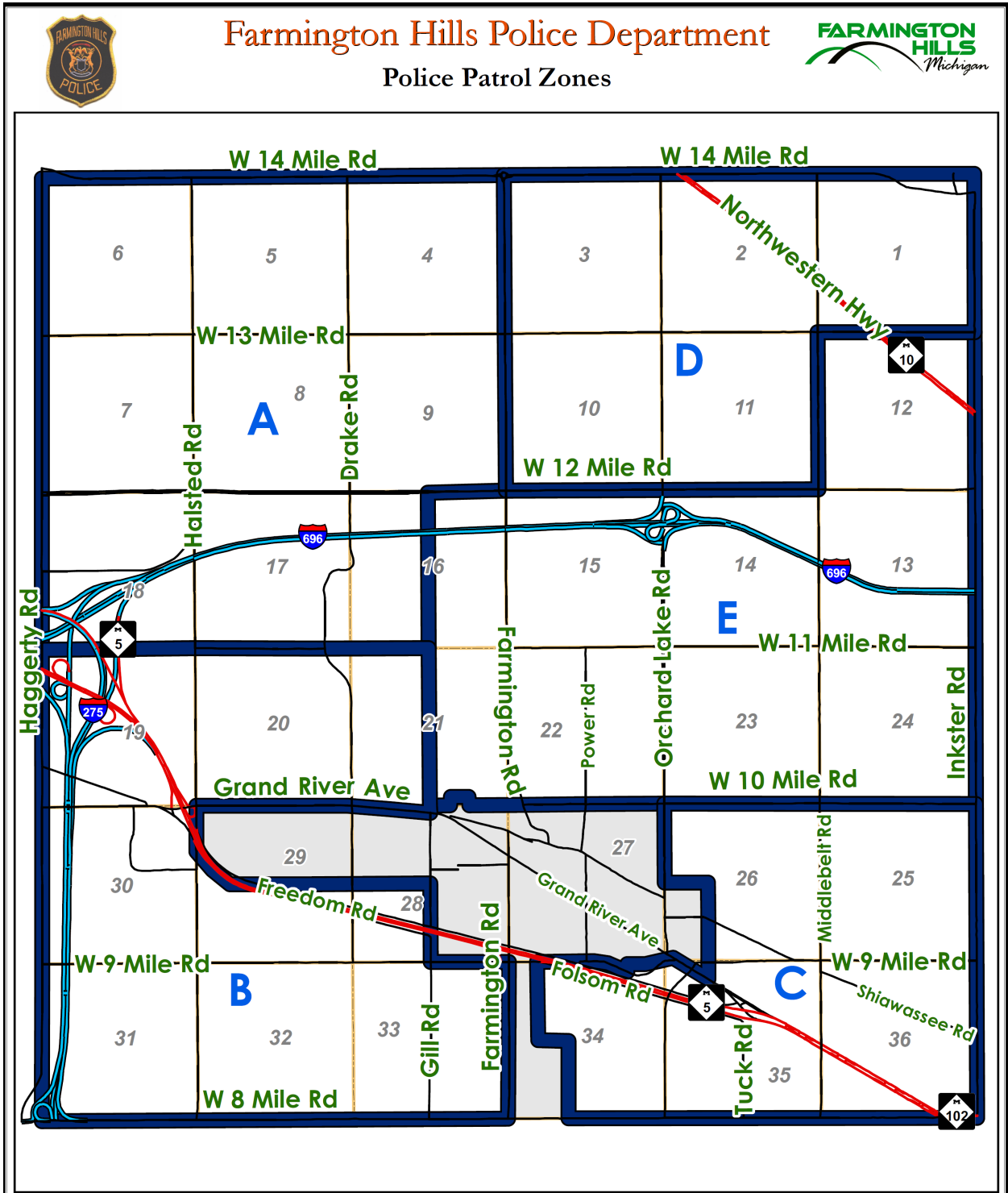
Beat Code explanation:

- FH is for Farmington Hills
- The city is broken up into Beats, or Districts as FHPD officers call them
- The Beats or Districts are labeled A, B, C, D, E, F, G, and H
 - H district covers 8 Mile – 10 Mile, from Middlebelt to Orchard Lake
- There are up to two cars assigned to each Beat or District, depending on the number of officers working that day
- If only five officers are working that day, FHPD will have cars in Districts A, B, C, D, and E
- The number 1 after the letter E means District E, one car assigned to that District
- If there is a two after that E, that means two cars

FHPD 8 Patrol Districts/Beats – Morning and Afternoon Shift



FHPD 5 Patrol Districts/Beats – Midnight Shift



PROBLEMS OBTAINING FHPD TRAFFIC STOP AND CITATION DATA

Summary of Issue

The Phase 2 analysis was significantly delayed and constrained due to the inability to obtain accurate, relevant, and comprehensive data.

1. The FHPD does not have any subject matter experts regarding the capabilities, functionality, and configuration of the CLEMIS CAD/Mobile system
2. The FHPD does not have any subject matter experts who can complete advanced data queries using the CLEMIS database
3. CLEMIS could only provide existing “canned” reports and could not generate the reports requested for this project

Data and Reports Available for the Analysis

The following reports, data tables, and bar charts were created for this report.

CIVIL INFRACTIONS

- Civil infractions issued 2006-2022

CIVIL CITATIONS – Residents vs. Non-residents

- 2020-2022 Data (Number issued/Residents/Non-residents/Percentage)
- Traffic stop Use of Force incidents
- 2020-2022 Use of Force Incidents/Traffic Stop Related

TRAFFIC STOP AND CITATION DATA

- 2020 - 2022 Three-Year Total
- 2020 – 2022 FHPD Traffic Stop Summary by Beat Data Table
 - 2020 – 2022 FHPD Traffic Stop Summary by Beat Bar Chart
- 2020-2022 Citation Data by Month Data Table
- 2020-2022 Traffic Stop by Shift Data Table
 - 2020-2022 Traffic Stop by Shift Bar Chart
- 2020-2022 Averages - FHPD Traffic Stop Summary by Beat Data Table
- 2020- 2022 Monthly Average Data Table
- Monthly Average By Year Data Table

2020 TRAFFIC STOP AND CITATION DATA

- 2020 Traffic Stop Summary by Beat Data Table
 - 2020 Traffic Stop Summary by Beat Bar Chart

- 2020 Traffic Stop Summary by Beat Pie Chart
- 2020 Traffic Stop By Shift Data Table
 - 2020 Traffic Stop By Shift Pie Chart
- 2020 Traffic Citation Data by Month Data Table
 - 2020 Traffic Citation Data by Month Bar Chart
- 2020 Additional Violation Options - Violation Summary Data Table
- 2020 Additional Violation Options - Secondary Officer Counts Data Table

2021 TRAFFIC STOP AND CITATION DATA

- 2021 Traffic Stop Summary by Beat Data Table
 - 2021 Traffic Stop Summary by Beat Bar Chart
 - 2021 Traffic Stop Summary by Beat Pie Chart
 - 2021 Traffic Stop Summary by Beat Bar Charts
- 2021 Traffic Citation Data by Month Data Table
 - 2021 Traffic Citation Data by Month Bar Chart
- 2021 Traffic Stop by Shift Data Table
 - 2021 Traffic Stop by Shift Pie Chart
- 2021 Additional Violation Options - Violation Summary Data Table
- 2021 Additional Violation Options - Secondary Officer Counts Data Table

2022 TRAFFIC STOP AND CITATION DATA

- 2022 Traffic Stop by Beat Data Table
 - 2022 Traffic Stop by Beat Bar Chart
- 2022 Traffic Citation by Month Data Table
- 2022 Traffic Stop by Shift Data Table
- 2022 Traffic Stop by Shift Data Table
- 2022 Additional Violation Options - Violation Summary Data Table
- 2022 Additional Violation Options – Full Table
- 2022 Additional Violation Options - Secondary Officer Counts Summary Data Table
- 2022 Additional Violation Options - Secondary Officer Counts – Full Table

TRAFFIC STOP VEHICLE AND PERSON SEARCH

- FHPD Level of Effort to Obtain Search Data
- 2020 – 2022 Traffic Stop Search Data Table
 - 2020 – 2022 Vehicle/Person Search Comparison Bar Chart
 - 2020 – 2022 Vehicle Search - Type of Search Comparison Bar Chart
 - 2020 – 2022 Vehicle Search - Driver Sex Comparison Bar Chart
 - 2020 – 2022 Vehicle Search - Driver Race Comparison Bar Chart
 - 2020 – 2022 Vehicle Search - Driver Race/Sex Comparison Bar Chart
 - 2020 – 2022 Vehicle Search - Passenger Sex Comparison Bar Chart
 - 2020 – 2022 Vehicle Search - Passenger Race Comparison Bar Chart

- 2022 Vehicle Search - Race/Sex Comparison Data Table
 - 2022 Vehicle Search - Race/Sex Comparison Bar Chart
- 2021 Vehicle Search - Race/Sex Comparison Data Table
 - 2021 Vehicle Search - Race/Sex Comparison Bar Chart
- 2020 Vehicle Search - Race/Sex Comparison Data Table
 - 2020 Vehicle Search - Race/Sex Comparison Bar Chart
- 2022 Vehicle Search - Age Comparison Data Table
 - 2022 Vehicle Search - Age Comparison Bar Chart
- 2021 Vehicle Search - Age Comparison Data Table
 - 2021 Vehicle Search - Age Comparison Bar Chart
- 2020 Vehicle Search - Age Comparison Data Table
 - 2020 Vehicle Search - Age Comparison Bar Chart

FHPD PATROL ZONE/DISTRICT DEMOGRAPHIC DATA

- Patrol Beat A Data Table
 - Patrol Beat A Bar Chart
- Patrol Beat B Data Table
 - Patrol Beat B Bar Chart
- Patrol Zone C Data Table
 - Patrol Beat C Bar Chart
- Patrol Beat D Data Table
 - Patrol Beat D Bar Chart
- Patrol Beat E Data Table
 - Patrol Beat E Bar Chart
- Patrol Beat F Data Table
 - Patrol Beat F Bar Chart
- Patrol Beat G Data Table
 - Patrol Beat G Bar Chart
- Patrol Beat H Data Table
 - Patrol Beat H Bar Chart

Examples of Data and Reports Not Available for the Analysis

1. At the start of the analysis, FHPD did not have a comprehensive list of dispositions related directly to traffic stops. For example:
 1. Type of traffic infraction
 2. Warning or citation issued
 3. Demographic information (e.g., race, gender)
 4. Vehicle search completed (e.g., voluntary probable cause, incident to arrest) (e.g., voluntary, probable cause, incident to arrest)
 5. Person search (e.g., voluntary, "Stop and Frisk/reasonable suspicion, incident to arrest)
 6. Traffic stop related to a crime (e.g., stolen tag, BOLO, wanted person)
2. Inability of the system to generate Traffic Stop Disposition reports based on FHPD disposition data
3. Inability to generate reports that differentiate the reason for traffic (e.g., traffic offense, crime, BOLO, wanted person, etc.)
4. Inability to pool different data sets to generate reports (e.g., all traffic stops, Beats in City and Disposition)
5. The CLEMIS system cannot map traffic stop data. For example:
 1. Pin map of all traffic stops for each District/Beat
 - i. All officers for a District/Beat
 - ii. Patrol squads/Shifts for a District/Beat
 - iii. Traffic Unit activity in each District/Beat
 2. Heat/Density map of all FHPD traffic stops
 3. Pin map of different types of traffic stops and dispositions

Example Traffic Stop and Citation Data Issues

Traffic Stop Disposition Data

Finding: The CLEMIS system disposition workflow prohibits the ability to generate Traffic Stop Disposition reports.

Data Source: Traffic Stop Workflow

Officers are responsible in CAD for clearing their own CFS with a disposition, but the disposition is in the notes section.

The CLEMIS representative also advised FHPD that the "Business Objects" software can only capture mandatory data and will not produce a report from the "Notes Section."

Inability to Separate Traffic Related Stops from Crime/Other Stops

Finding: The CLEMIS database cannot differentiate between traffic, crime, BOLO, or wanted person.

Data Source: *Phase II - 1. CAD Breakdown a. Traffic Stops {2021 Data} iv. Does the department differentiate a vehicle stop for a traffic offense versus a crime-related one (e.g., BOLO, suspicious activity, wanted person, suspect vehicle, etc.)?*

FHPD Response:

If an officer calls out on a traffic stop, that is recorded in CAD for traffic enforcement.

Any crime-related activity category is recorded as that related crime in CAD.

If it is a crime-related activity, this information would have to be looked at on a case-by-case basis in the narrative of the incident report to learn if a particular incident started as a traffic stop.

If an arrest is made by the officer or possible criminal activity has to be documented, a new incident number is generated, and the possible criminal activity or arrest is documented under that newly generated incident report number.

Data Source: *Phase II - 1. CAD Breakdown a. Traffic Stops (2022 Data)*
i. All traffic Stops for entire City by type of stop (event code) and disposition code

The CLEMIS representative advised that it is not possible in Business Objects to determine between the type of stop for either Found on Patrol (FOP – found by an officer on regular patrol) or whether the officer was dispatched to an incident, BOL or crime-related activities resulting in a traffic stop and would have to manually done (report by report).

We have found this extremely time-consuming when we manually mine data for vehicle searches, and current staffing cannot provide this information.

A disposition from each traffic stop also falls under the same conditions. If a citation is issued, the Traffic Stop “CFS” number should be documented at the top of the Citation.

Officers are responsible in CAD for clearing their own CFS with a disposition, but the disposition is in the notes section. The CLEMIS representative also advised FHPD that the “Business Objects” software can only capture mandatory data and will not produce a report from the “Notes Section.”

Inability to Develop Traffic Stop Disposition Reports

Finding: The CLEMIS system could not generate Traffic Stop Disposition reports.

Data Source: *Phase II - 1. CAD Breakdown a. Traffic Stops (2022 Data)*

i. All traffic Stops for entire City by type of stop (event code) and disposition code

The CLEMIS representative advised that it is not possible in Business Objects to determine between the type of stop for either Found on Patrol (FOP – found by an officer on regular patrol) or whether the officer was dispatched to an incident, BOL or crime-related activities resulting in a traffic stop and would have to manually done (report by report).

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A disposition from each traffic stop also falls under the same conditions. If a citation is issued, the Traffic Stop “CFS” number should be documented at the top of the Citation.

Officers are responsible in CAD for clearing their own CFS with a disposition, but the disposition is in the notes section.

The CLEMIS representative also advised FHPD that the “Business Objects” software can only capture mandatory data and will not produce a report from the “Notes Section.”

Data Source: *Phase II - 1. CAD Breakdown a. Traffic Stops (2021 Data)*

PDF - All traffic Stops for entire City by type of stop (event code) and disposition code

...If a citation is issued, the Traffic Stop “CFS” number should be documented at the top of the Citation.

Officers are responsible in CAD for clearing their own CFS with a disposition, but the disposition is in the notes section.

The CLEMIS representative also advised FHPD that the “Business Objects” software can only capture mandatory data and will not produce a report from the “Notes Section.”

Inability to Map Traffic Stop Data

Finding: The CLEMIS system cannot generate Traffic Stop map reports.

Data Source: *Phase II – 1. CAD Breakdown. a. Traffic Stops (2021 Data)*
v. *Can the CAD system map the above data {e.g., multiple map views} Traffic stops*

The representative of CLEMIS advised that CAD does not map this data.

FHPD “Basic User” versus “Advanced User Constraint

Finding: FHPD does not have any Advanced Users to generate CLEMIS reports

Data Source: *Phase II - 1. CAD Breakdown a. Traffic Stops {2021 Data}*
PDF - All traffic Stops for entire City by type of stop {event code} and disposition code

CLEMIS also advised that the "Basic User" of "Business Objects" cannot program for a specific report.

The Basic User can only run data from the reports already programmed in the system. To program for a specific report/data, an "Advanced User" would need to program for this information, and only a select few are "Advanced Users."

Since the "Business Objects" software is being updated in early 2023, there have been no "Advanced Users" classes for some time.

After the update, training courses for both "Basic User" and "Advanced User" will be provided.

This means we are left with what's already programmed in the system and CLEMIS programming a specific report for our use.

The representative advised that a new programmed report specific to our request will take some time, and depending on the data captured, the information requested may or may not be in the report.

Inability to Separate Traffic Citations Revenue from Civil Infractions

Data Source: P2 – 6 Ticket Revenue. FHPD response

I requested the District Court provide information on ticket revenue for 2021 and 2022. They stated that the information doesn't exist as they don't distinguish the origins of the money. (Emphasis added) Their records don't classify revenue in categories and could be from several different sources.

These sources include misdemeanors, ordinance violations, warrants, zoning violations, and traffic citations. I was also told, even if money is paid in a current year, it may be from an offense that happened years ago and is just being paid now which would skew the data.

CLEMIS DATA

The request for Phase 2 CLEMIS data started during the Phase 1/Training assessment. Numerous problems were encountered while attempting to obtain CLEMIS data, including:

1. FHPD does not have personnel who were experts regarding “canned” CLEMIS reports
2. FHPD does not have personnel who are trained to the “Advanced” level that would provide them access to the CLEMIS database
3. There are some significant limitations regarding CLEMIS data
4. CLEMIS was not able to produce any custom reports

The following is a high-level snapshot only of communications with CLEMIS. Numerous other emails are available if needed. The objective of this section is to provide background information only.

February 21, 2023 Email

From: Tom Maureau

Sent: Tuesday, February 21, 2023 10:38 AM

To: Manning, Jim <manningjt@oakgov.com>; Jeff King <jking@fhgov.com>

Cc: Richard Blendea <RBlendea@fhgov.com>; Mike Flatt <Mflatt@fhgov.com>; Michael Connolly <mconnolly@fhgov.com>; Amber Daughtry <adaughtry@w-llc.com>; Clark Kimerer <ckimerer@w-llc.com>

Subject: FHPD Analysis 2022 - CLEMIS Data Update

Chief King and Director Manning – Quick Update regarding CLEMIS data.

Director Manning – Please reply-all if there are any errors or edits that are needed.

Note: Below is the SOW scope for Phase 2 and Phase 3 to get Director Manning up to speed regarding project objectives.

- *Director Manning and I had a very productive conversation on Friday/February 16, 2023 regarding the capabilities of CLEMIS to provide data for the City of Farmington Hills “FHPD Analysis 2022” project*
- *CLEMIS produces a number of “canned” reports that are available to FHPD. These reports have some ad hoc capabilities to change attribute parameters*
- *The CLEMIS system has extensive data mining query capabilities via the utilization of Business Objects (e.g., Business Intelligence/Analytics solution)*
 - *All attributes in the system can be queried*
 - *The CLEMIS system has a data warehouse (my wording) with all FHPD data*
 - *FHPD personnel with appropriate Business Objects proficiency would have permission to go into the system and develop customized FHPD reports*

- *Note: There needs to be a discussion between FHPD and CLEMIS about Business Objects training for FHPD personnel*
- *Director Manning advised CLEMIS would assist the City as best possible depending on personnel bandwidth, planned activities, timeline to obtain the information, etc.*
- *We discussed the various reasons there could be differences between the FHPD Transparency Dashboard and the “canned” reports we were provided by FHPD. For example:*
 - *The queries could be different. One is asking for X while the other is asking for Y*
 - *Intentional exclusion – One report may intentionally exclude some type of data (e.g., juvenile, sex crime, domestic battery, etc.)*
 - *Timing – The submission of report data to the CLEMIS system is dynamic while the Transparency Dashboard is a static report (e.g., when the Transparency Dashboard was originally created not all report data was available)*
- *Next steps*
 - *Winbourne Consulting will send Director Manning:*
 - *A list of reports we are seeking relevant to Phase 2 and Phase 3 of the project. We will have that completed by tomorrow at the latest*
 - *Some questions regarding the FHPD Transparency Dashboard reports*

Will keep everyone posted.

Tom

April 7, 2023 Email

From: Armstrong, Michael <armstrongm@oakgov.com>

Sent: Friday, April 7, 2023 4:17 PM

To: Manning, Jim <manningjt@oakgov.com>; Long, Jeffrey A <longje@oakgov.com>

Cc: Turner, Shelly Lynn <turnersl@oakgov.com>; Corbett, Charles C <corbettc@oakgov.com>;

Nelms, Benjamin <nelmsb@oakgov.com>; McDonald, Robert C <mcdonaldrc@oakgov.com>

Subject: RE: FHPD Analysis 2022 - Phase 2 Traffic Stop/Citation CLEMIS Information Request

The questions have been copied and answered below. Please let Chuck, Rob, Shelly or myself know if you have any further questions.

Thank you,
Michael

General Questions

1. *Is there an ability to export the requested data to a spreadsheet so it can be sorted? **Yes, this can be placed into a spreadsheet. The report that Robert McDonald and I created along with exports out of CrimeView as well should be able to provide the data.***

2. *Is there an ability to map any of the data (e.g., traffic stop location, citation location address, etc.)? Yes, This functionality can be found in Crimeview, however demographic information may not be available based on system.*
3. *Is there an ability to create a heat map based on traffic stop location and/or citation location data? Yes, This functionality can be found in CrimeView, however demographic information may not be available based on system.*
4. *Does the CLEMIS database include any revenue information for State Motor Vehicle Code and City civil citations issued? Yes, the ePayments screen can be used to get total ticket payment information.*

Michael Armstrong

Contractor

CLEMIS Division

Information Technology

Oakland County, Michigan

All ways, moving forward

June 30, 2023 Email

From: Tom Maureau <TMaureau@w-llc.com>

Sent: Friday, June 30, 2023 4:14 PM

To: Long, Jeffrey A <longje@oakgov.com>; Manning, Jim <manningjt@oakgov.com>

Cc: Jeff King <Jking@fhgov.com>; Richard Blendea <RBlendea@fhgov.com>

Director Manning and Jeff – FHPD Chief King and Captain Blendea are copied on this email.

If it is more effective for everyone to talk, please let us know and we will schedule a meeting next week.

This discussion started in February 2023. As stated below in the email string, the City of Farmington Hills hired Winbourne Consulting to complete an analysis of FHPD Traffic Stop/Citation and Arrest procedures and data.

FHPD has a limited capability to generate the types of reports needed to complete the assessment.

We contacted CLEMIS to learn if the data for the needed information exists and if yes, could CLEMIS generate the desired reports. Director Manning advised CLEMIS did have the data, the business intelligence system capability and could generate some reports for FHPD. Understanding that CLEMIS personnel are busy, we limited the Traffic Stop/Citation report list and advised we would provide the Arrest data reports after the Traffic data reports we completed to avoid confusion and overwhelming CLEMIS personnel.

Apparently there was a misunderstanding and CLEMIS did not create the reports. Upon recognizing there was a misunderstanding, we restarted the discussion. However, CLEMIS is now advising that FHPD should create the reports, but they do not have the expertise to do so.

As requested by CLEMIS, we can have FHPD be the originator of the report request if that is what needs to occur.

Bottom line question – Will CLEMIS create the requested reports? Thanks.

Tom Maureau
Vice President
Winbourne Consulting, Inc
Cell 850.545.3508
Tmaureau@w-llc.com
1101 Wilson Boulevard, 6th Floor, Arlington, VA 22209
www.winbourneconsulting.com

July 5, 2023 Email

From: Manning, Jim <manningjt@oakgov.com>
Sent: Wednesday, July 5, 2023 9:35 AM
To: Tom Maureau <TMaureau@w-llc.com>; Long, Jeffrey A <longje@oakgov.com>
Cc: Jeff King <Jking@fhgov.com>; Richard Blendea <RBlendea@fhgov.com>
Subject: RE: FHPD Analysis 2022 - Phase 2 Traffic Stop/Citation CLEMIS Information Request

Hello Tom,

For clarification, CLEMIS can assist with information that FHPD cannot access. If we can pull the information. Talking with my staff, the information you have requested should be accessible to FHPD and personnel have been trained on how to pull the data. If we need to assist with some refresh training on how to pull the data that is requested, we can assist with this effort. I apologize for the delay regarding the “questions” that were sent back in February. I didn’t understand that your “objectives” were an actual request of CLEMIS staff.

Bottom line –

CLEMIS can provide a refresher to personnel who have been trained how to create the reports. We have a list of personnel trained if needed.

If FHPD personnel cannot create the reports, CLEMIS will assist with pulling the data if available.

Let me know if you have any questions.

Jim Manning
Applications Services Director - Public Safety
Information Technology / CLEMIS
Oakland County, Michigan

Limitations with CLEMIS System – Ann Arbor Traffic Stop Policy News Article

Note: This article is included to provide background information. The issue is directly related to FHPD.

Ann Arbor has new directive for police after updating traffic stop policy

Updated: Aug. 30, 2023, 11:27 a.m.

Published: Aug. 29, 2023, 8:00 a.m.

By Ryan Stanton | ryanstanton@mlive.com

ANN ARBOR, MI — City Council has issued another new directive for the Ann Arbor Police Department regarding traffic stops.

Council is calling on AAPD to create an official policy mandating how officers must log stops, following up on the city’s recent decision to put an end to pulling over motorists for relatively minor offenses in hopes of avoiding racial profiling.

Officers use a digital reporting system known as the Court and Law Enforcement Management Information System, or CLEMIS, to log data. But the system has limitations, according to city officials and researchers who’ve analyzed traffic stop data.

The pull-down menus allow officers to select only one reason for making a stop (such as speeding, alcohol/drugs and equipment violations) and only one outcome (arrest, citation, verbal warning, or assist), even when there may be multiple reasons and multiple outcomes, council stated in a resolution approved 10-0 on Aug. 21.

“The inability to capture data reflecting every reason for contact and every outcome of each police interaction makes full transparency between law enforcement and the public impossible to achieve,” the resolution states.

Officers are trained to select the most serious reason for contact and the most serious outcome when using CLEMIS, but there has been no policy requiring them to do so, leading to the possibility that data could be improperly recorded, either intentionally or inadvertently, creating an inaccurate picture of policing in Ann Arbor, the council resolution states.

“The proper recording and representation of all reasons for contact and all outcomes is essential for obtaining more accurate, comprehensive and insightful data, improving the efficacy of policy-making, ensuring data-driven law enforcement strategies and reinforcing public trust and transparency in law enforcement,” the resolution states.

Council directed AAPD to make selecting the most serious reason for contact and the most serious outcome the department’s official policy as long as CLEMIS remains in its current form, and to record all reasons and outcomes if the system is ever updated to allow that.

In the meantime, council also is directing City Administrator Milton Dohoney to advocate for changes to CLEMIS.

Council approved those directives after approving a nearly \$530,000, five-year agreement for continued use of the CLEMIS system through Oakland County.

“It wasn’t long ago when we passed the ordinance against low-level, secondary stops — driven by our collective realization that they disproportionately affected our Black and brown community members,” said Council Member Cynthia Harrison, D-1st Ward, who also sponsored the latest resolution about logging stops.

“Together we made a commitment to our residents to ensure equitable treatment for all,” she said. “But our work toward a more just Ann Arbor is not done.”

Ann Arbor’s approval of Michigan’s first driving equality ordinance in July came after an Eastern Michigan University research study — in partnership with the city’s police oversight commission — found significant racial disparities in Ann Arbor traffic stops, with non-white motorists stopped and searched more frequently. The study also identified limitations with the CLEMIS system and recommended more robust data reporting.

“I can’t say enough about how important data is to understanding what’s going on,” said Council Member Dharma Akmon, D-4th Ward, agreeing there are gaps.

It’s important to have data painting a more comprehensive picture for all parties involved in traffic stops, said Council Member Ayesha Ghazi Edwin, D-3rd Ward.

Council Member Chris Watson, D-2nd Ward, hinted at more to come on matters related to policing.

“There’s a lot of steps that I think we’re going to be taking, looking at dispatching, looking at our dashboards,” he said.

47th DISTRICT COURT ADMINISTRATOR COMMUNICATION

Numerous issues were encountered while attempting to obtain District Court information that caused a delay in the completion of the report. For the most part, the information requested was received.

The following is a high-level snapshot of communication with the District Court Administrator's Office. Numerous other emails are available if needed. The objective of this section is to provide background information only.

Questions to 47th District Court Regarding Traffic Citation Data

From: Tom Maureau <TMaureau@w-llc.com>
Sent: Monday, October 02, 2023 8:49 AM
To: Matt Friedrich <MFriedrich@fhgov.com>
Cc: Stacy Parke <SParke@fhgov.com>
Subject: FHPD Analysis - Fine/cost breakdown at the 47th District Court

Mr. Friedrich – Thank you for your email.

Quick Background Information

The City of Farmington Hills contracted with Winbourne Consulting, Inc. to complete an analysis of several areas related to FHPD operations. One area is a Traffic Stop and Citation Analysis for the years 2020, 2021 and 2022. Below is a copy and paste from our contract with the City that fully explains the project.

During the assessment it was learned that FHPD issues a significant number of City Ordinance Traffic Citations versus a State Motor Vehicle Code Citations.

We have been attempting to obtain the below data for a significant amount of time. We understand your office is busy. We are hoping most of the information is already in existing reports and/or can be obtained with a nominal level of effort. Anything you can do to help expedite this process is appreciated. That said, it is more important for us to receive the information versus speed.

Let us know if you would like to talk.

The following is the information we are seeking to complete the analysis.

Does the 47th District Court Administrator have any documentation and information regarding the following areas:

1. The legal authority of City Police Departments in the State of Michigan to issue a City Ordinance Traffic Citation for a State of Michigan Motor Vehicle Code Traffic Offense
2. The City of Farmington Hills City ordinance that provides FHPD officers the legal authority to issue a City Ordinance Traffic Citation based on a State of Michigan Motor Vehicle Code Violation
3. The practice of other police departments in the 47th District Court jurisdiction regarding the issuance of a City Ordinance Traffic Citation versus a State of Michigan Motor Vehicle Code Citation
 - a. Do other Cities in the 47th District have the same type of City Ordinance the City of Farmington Hills has (e.g., City Ordinance that replicates State of Michigan Motor Vehicle Code violations allowing a City Ordinance Traffic Citation to be issued for the violation)
 - b. Is the issuance a City Ordinance Traffic Citation versus a State of Michigan Motor Vehicle Code Citation a common practice in the 47th District
 - c. Does the 47th District Court have any documentation or information that shows the City Police Departments that employ this practice
 - d. Does the 47th District Court have any reports that show the number of City Ordinance Traffic Citations and State Motor Vehicle Code Citations issued for 2020, 2021 and 2022 for the 47th District
4. Documentation and information how City Ordinance Traffic Citation fines in the 47th District Court are developed and implemented
 - a. For example, what entity has the authority to develop and implement City Ordinance fines
 - b. Any information that shows the history of City Ordinance Traffic Citation fines for the past ten years
5. Breakdown regarding the distribution of Traffic Citation revenue for:
 - a. State Motor Vehicle Code Traffic Citation
 - b. City Ordinance Traffic Citation
 - c. For example:
 - i. For a State Motor Vehicle Citation for Speeding 15 MPD over the speed limit, how is the revenue distributed to the State, County and City
 - ii. For a City Ordinance Citation for Speeding 15 MPD over the speed limit, how is the revenue distributed to the State, County and City
6. Documentation that shows the Traffic Citation fine amount for:
 - a. State Motor Vehicle Code Traffic violations
 - b. City Ordinance Traffic Violations
7. For 2020, 2021 and 2022, how many traffic citations did FHPD issue for:
 - a. State Motor Vehicle Code Traffic violations
 - b. City Ordinance Traffic violations

8. For 2020, 2021 and 2022, does the 47th District Court have any reports or information that show a breakdown on the type of traffic citations that were issued by FHPD for:
 - a. State Motor Vehicle Code Traffic Citation
 - b. City Ordinance Traffic Citation
9. For 2020, 2021 and 2022, please provide the amount of revenue FHPD generated each year for:
 - a. State Motor Vehicle Code Traffic Citations
 - b. City Ordinance Traffic Citations
10. Any information the 47th District Court can provide on this topic is appreciated

Tom Maureau
Vice President
Winbourne Consulting, Inc
Cell 850.545.3508
Tmaureau@w-llc.com
1101 Wilson Boulevard, 6th Floor, Arlington, VA 22209
www.winbourneconsulting.com

47th District Court Administrator Response

Information provided by District Court Administrator Stacey Parke in blue.

Does the 47th District Court Administrator have any documentation and information regarding the following areas:

1. The legal authority of City Police Departments in the State of Michigan to issue a City Ordinance Traffic Citation for a State of Michigan Motor Vehicle Code Traffic Offense.

While this inquiry is more appropriately directed to the FHPD and/or their legal counsel, you may want to refer to Michigan's Motor Vehicle Code (Act 300 of 1949), including, but not necessarily limited to MCL 257.742.

2. The City of Farmington Hills City ordinance that provides FHPD officers the legal authority to issue a City Ordinance Traffic Citation based on a State of Michigan Motor Vehicle Code Violation

While this inquiry is also more appropriately directed to the FHPD and/or their legal counsel, you may want to refer to the Code of Ordinances for the City of Farmington Hills, including, but not necessarily limited to Chapter 30, Article II, which can be found at: https://library.municode.com/mi/farmington_hills/codes/code_of_ordinances?nodeld=PTIIC_OOR_CH30TRMOVE.

3. The practice of other police departments in the 47th District Court jurisdiction regarding the issuance of a City Ordinance Traffic Citation versus a State of Michigan Motor Vehicle Code Citation

- a. Do other Cities in the 47th District have the same type of City Ordinance the City of Farmington Hills has (e.g., City Ordinance that replicates State of Michigan Motor Vehicle Code violations allowing a City Ordinance Traffic Citation to be issued for the violation)

This inquiry is also more appropriately directed to the FHPD, Farmington Public Safety, and/or their legal counsel. Having stated that, and by way of example only, you may want to refer to the Code of Ordinances for the City of Farmington, including, but not necessarily limited to Chapter 31, Article II, which can be found at:

https://library.municode.com/mi/farmington/codes/code_of_ordinances?nodeId=PTII_COOR_CH31TRMOVE.

- b. Is the issuance a City Ordinance Traffic Citation versus a State of Michigan Motor Vehicle Code Citation a common practice in the 47th District

Any inquiry regarding any “practice” related to the issuance of a traffic ticket should be directed to the law enforcement agency issuing the ticket. The Court and police departments fall under separate branches of government (Executive/Judicial), and are independent of one another, and separate agencies.

- c. Does the 47th District Court have any documentation or information that shows the City Police Departments that employ this practice

See above.

- d. Does the 47th District Court have any reports that show the number of City Ordinance Traffic Citations and State Motor Vehicle Code Citations issued for 2020, 2021 and 2022 for the 47th District

This inquiry should be directed to any/all law enforcement agencies who are responsible for issuing civil infractions, including the FHPD, as the Court cannot and does not track how many civil infractions are issued by any/all law enforcement agency(ies) during any particular time period. Having stated that, the Court’s incoming caseload information can be found on the State Court Administrative Office’s Interactive Court Data Dashboard using the following link:

<https://www.courts.michigan.gov/publications/statistics-and-reports/interactive-court-data-dashboard/>

4. Documentation and information on how City Ordinance Traffic Citation fines are developed and implemented in the 47th District Court. For example, what entity has the

authority to develop and implement City Ordinance fines. Michigan law, including MCL 257.907 for civil infractions. Any information that shows the history of City Ordinance Traffic Citation fines for the past ten years.

The Court's current Fine Schedule, which covers both local ordinance violations and state law civil infractions, is found here: <https://fhgov.com/stay-connected/forms,-permits-applications/download/47th-district-court/trafficfines1-1-2018.aspx>.

5. Breakdown regarding the distribution of Traffic Citation revenue for:
 - a. State Motor Vehicle Code Traffic Citation
 - b. City Ordinance Traffic Citation
 - c. For example:
 - For a State Motor Vehicle Citation for Speeding 15 MPD over the speed limit, how is the revenue distributed to the State, County and City
 - For a City Ordinance Citation for Speeding 15 MPD over the speed limit, how is the revenue distributed to the State, County and City

a.-c. For civil infractions, the first \$40 goes to the State of Michigan for a Justice System Assessment. The fine and costs go to the City general fund (for the jurisdiction in which the ticket was issued) if the ticket is written as a local ordinance violation, while the fine would go to the Oakland County Library Fund if the ticket was written under state law (costs go to the City for the jurisdiction in which the ticket was issued).

With that, for a typical ticket for 15 MPH over the speed limit (non- construction, non-expressway, non-school zone, etc.), the breakdown of fines/costs/fees would be: \$40 goes to the State of Michigan for a Justice System Assessment, fines of \$30, costs of \$80, and the only difference in how the money is distributed would be whether the fine goes to the Library Fund or the City.

6. Documentation that shows the Traffic Citation fine amount for:
 - a. State Motor Vehicle Code Traffic violations
 - b. City Ordinance Traffic Violations

See link provided in response to #4.

7. For 2020, 2021 and 2022, how many traffic citations did FHPD issue for:
 - a. State Motor Vehicle Code Traffic violations
 - b. City Ordinance Traffic violations

	OI	SI
2020	3935	77
2021	5223	103
2022	5762	65

a.-b. This inquiry should be directed to the FHPD, as the Court does not track how many civil infractions were issued by any law enforcement agency during any particular time period. Having stated that, below you will find the Court's incoming caseload information related to FHPD local ordinance and state law civil infractions which the Court processed:

For additional information, we refer you to the Michigan Interactive Data Dashboard: <https://www.courts.michigan.gov/publications/statistics-and-reports/interactive-court-data-dashboard/>

For 2020, 2021 and 2022, does the 47th District Court have any reports or information that that show a breakdown on the type of traffic citations that were issued by FHPD for:

- c. State Motor Vehicle Code Traffic Citation
- d. City Ordinance Traffic Citation

a.-b. This inquiry should be directed to the FHPD, as the Court does not track how many civil infractions were issued by any law enforcement agency during any particular time period.

- 8. For 2020, 2021 and 2022, please provide the amount of revenue FHPD generated each year for:
 - a. State Motor Vehicle Code Traffic Citations
 - b. City Ordinance Traffic Citations

No such report exists at the Court for traffic citations for FHPD revenue. Our case processing system does not separate fines/costs payments by case types--e.g., civil infractions, misdemeanors, nontraffic. All monies collected and transmitted to the City for fines/costs are grouped for all the case types.

District Court Administrator October 18, 2023, Letter

STATE OF MICHIGAN

THE DISTRICT COURT



47TH JUDICIAL DISTRICT

31605 W. ELEVEN MILE ROAD
FARMINGTON HILLS, MICHIGAN 48336
PHONE 248-871-2900
FAX 248-871-2901

MARLA E. PARKER
DISTRICT JUDGE

STACY PARKE
COURT ADMINISTRATOR

JAMES B. BRADY
DISTRICT JUDGE

October 18, 2023

Mr. Tom Maureau
Winbourne Consulting, LLC.
1101 Wilson Boulevard, 6th Floor,
Arlington VA 22209

Dear Mr. Maureau:

This letter is in response to your request dated October 2, 2023 sent to Matt Friedrich's attention with a cc to me via email. Let the record reflect that this is the first request we have received in writing from your Company. I will assume your statement of "we have been attempting to obtain the below data for a significant amount of time" is meant toward other agencies.

We have given our best efforts to supply answers to questions that pertain to our Court—even with limited resources. Many of the questions seem geared toward the police departments and the attorneys representing the City. As Court Administrator of the 47th District Court, I must be cautious and not make legal interpretations or give answers that pertain to other agencies. Please see the attachment for the Court's responses and helpful caseload data links.

The Michigan State Court Administrator's Office recently rolled out a new interactive data dashboard and that system may help you glean the information needed in order to conduct your study for the City of Farmington Hills. Our Court's caseload data is accessible at this new site. The link for the Dashboard can be found at www.courts.mi.gov and/or:

- [Michigan Data Dashboard](http://www.courts.michigan.gov/publications/statistics-and-reports/interactive-court-data-dashboard) -- www.courts.michigan.gov/publications/statistics-and-reports/interactive-court-data-dashboard.

Another helpful link may be:

- [District Court Fee and Assessment Table](#) (PDF page 3 starts the civil fine breakdown and shows how things are distributed)

Regards,

47th District Court

A handwritten signature in blue ink that reads "Stacy Parke".

Stacy Parke, Court Administrator

Attachment

c. Mr. Matt Friedrich, Deputy Court Administrator

State of Michigan Versus City Ordinance Traffic Citation Revenue



Authored by Lieutenant Michael Connolly

I requested the District Court provide information on ticket revenue for 2021 and 2022. They stated that the information doesn't exist as they don't distinguish the origins of the money. Their records don't classify revenue in categories and could be from several different sources. These sources include misdemeanors, ordinance violations, warrants and zoning violations, as well as traffic citations. I was also told, even if money is paid in a current year, it may be from an offense that happened years ago and is just being paid now which would skew the data. I was given the breakdown on monies for a typical speeding citation that might be issued by a Farmington Hills officer. A speeding ticket (1-5 miles over) costs the motorist \$140 in fines/costs. The breakdown of that ticket is \$40 goes to the State of Michigan, and \$100 would remain with the City of Farmington Hills. The Court would then send that portion of the fine/cost to the City, and it is deposited in the City's general fund. I was provided with the total number of tickets issued by Farmington Hills officers over the last 16 years.

2006 through 2021 City of Farmington Hills Civil Infractions Issued

<u>Year Issued</u>	<u>Total Issued</u>
• 2006	16202
• 2007	17316
• 2008	15681
• 2009	13317
• 2010	10123
• 2011	8773
• 2012	8592
• 2013	9349
• 2014	8036
• 2015	6753
• 2016	6407
• 2017	8269
• 2018	9337
• 2019	7310
• 2020	4012
• 2021	5326

The yearly ticket average over the last 16 years is 9675 tickets per year, with the median being 8773 in 2011. The 3-year average high is 16400 tickets a year, with a 3-year average low at 5248 tickets for years. In 2021, tickets issued are down almost 70% from its 15-year high point in 2007 and down 45% from the 16-year average.

NEW FHPD POLICY AND PROCEDURES

Traffic Stop Activity Log

- As previously stated in the report, at the start of the project, the FHPD did not collect relevant disposition information for traffic stops and citations
- During a January 15, 2023, onsite meeting with FHPD management, anti-bias, and Professional Traffic Stop initiative criteria were discussed, including an assessment of all options
- FHPD personnel learned of new functionality with the CLEMIS Mobile application intended for this initiative. While the CLEMIS functionality is not ideal, it does provide a significant improvement for FHPD
- The FHPD developed and implemented an Activity Log pilot project to collect relevant data. The pilot project was started on February 3, 2023
- The Activity Log pilot project was successful, and the program was implemented in all Patrol operations in August 2023
- The initiative includes monthly reports and a management review process

Activity Log Pilot Project

From: Mike Flatt <Mflatt@fhgov.com>

Sent: Friday, February 3, 2023 5:39 PM

To: Domenic Lauria <DLauria@fhgov.com>; Kurt Bearer <KBearer@fhgov.com>; Jason McDonald <JMcDonald@fhgov.com>; Brian Moore <BMoore@fhgov.com>

Cc: Justin Barry <JBarry@fhgov.com>; Richard Blendea <RBlendea@fhgov.com>; Andrius Radze <ARadze@fhgov.com>; Daniel Rodriguez <DRodriguez@fhgov.com>; John Piggott (JPiggott@fhgov.com) <JPiggott@fhgov.com>; Brian Kersanty <BKersanty@fhgov.com>

Subject: Activity Log Pilot Program

Lieutenants,

Due to the recent ongoing third-party assessment of the Farmington Hills Police Department's Yearly Officer Training, Citations, and Arrests, some of the data requested could not be collected easily or at all. New information came to light regarding CLEMIS being able to capture this data. This information that came to light refers to the Officer's Activity Logs, parameters that can be set, and those parameters that were already available in the system. In lieu of every officer having to put data for person and vehicle tabs of every CFS, the Activity Logs have the capabilities of capturing this data for us. For regular patrol officer activity logs these parameters are not set up, but we would like to start a Pilot Program to see how this may or may not work. We have set the parameters (mandatory fill in boxes) in the Traffic Selection

(1st attached document) of the Activity Logs Division. The parameters added to the Traffic Division of the Activity Log can be found under Traffic Stop-TS and the parameters are as follows,

1. Reason for Contact
 - a. Speed
 - b. Belt/Restraint
 - c. Alcohol/Drugs
 - d. Assist
 - e. Traffic Vio
 - f. Crime BOL
 - g. Equipment Vio
 - h. Weighmaster
 - i. Other
2. Outcome of Stop
 - a. Citation
 - b. Arrest
 - c. Assist
 - d. Verbal Warn
3. Search
 - a. After Arrest
 - b. Plain View
 - c. Consent
 - d. Impound
 - e. P/C
 - f. K-9
 - g. No Search
4. Gender
 - a. Male
 - b. Female
5. Age
6. Race Known prior to Stop
 - a. Yes
 - b. No
7. Race
 - a. White
 - b. Native Am
 - c. African Am
 - d. Pacific Island
 - e. Hispanic
 - f. Middle Eastern
 - g. Asian
 - h. Multi Racial
8. Hazardous / Non-Hazardous / Warning / Code Violation

Now, these were the parameters that were in the CLEMIS Activity Log that can be added or subtracted to depending on what information we get back from our Pilot Program. CLEMIS is currently creating a program to run in Business Objects to mine this data and we will be able to run this program to collect this data. Currently there are three officers on Day Shift utilizing this data collection (Officers Rohrer, Ward, and Quinonez), so in the next week we will be able to see how this data sheet will look. I have received feedback from one of the Officers already, that advised it doesn't take any time to complete the mandatory fields, but there are some concerns. One, that the CFS does not appear in the Activity Log right away. If the Officer did not collect/keep the information after clearing the incident in CLEMIS (specifically AGE), when filling out the Activity Log the Officer may have trouble completing the mandatory fields. Two, the questions of race and gender, not all drivers' licenses are reporting race and gender, which would leave the Officer to ask or guess. Third, the mandatory field of search was originally specified to search of the vehicle, but this field does not stipulate between person or vehicle. And Fourth, there is only one portal for demographics and what if there is more than one occupant in the vehicle.

Some of the above questions I am attempting to answer, but The Chief would like to expand this Pilot Program to a Three-Month Assessment with Three Officers from each of the Three Different Shifts.

What I am requesting from the three Patrol Shift Lieutenants is to pick the Three Officers from your shift, advise them on this Pilot Program and the length of this program, to have them use Traffic Division Activity Logs on their shift for the duration of Pilot Program, to use the Traffic Stop-TS for all their traffic stops, and complete the mandatory fields for Traffic Stops-TS. Please let me know who selected and if you have any questions concerning this.

Depending on how this Pilot Program works out, the idea is to implement these parameters in anything the Officers find on patrol (FOP) (suspicious person, suspicious circumstance, etc.....). We are also looking for any suggestions that you may have concerning this Pilot Program.

Sorry for such a long e-mail but thank you and the officers you select in advance for participating in this Pilot Program. As soon as selection of the officers involved in this program are made and any questions that you or they have answered. We will start the Pilot Program.

The attachments to this email are to help explain the program.

Thank You,
Lt. Flatt



INTEROFFICE CORRESPONDENCE

RECEIVED

TO: Chief Jeff King (Through the Chain of Command)
FROM: Lieutenant Michael Flatt *MF*
DATE: Tuesday, January 24, 2022
SUBJECT: Officer Activity Logs / CLEMIS Activity Log Pilot Test

1-28-22
Jeff King, Chief of Police
Farmington Hills Police Dept.
*PILOT APPROVED FOR
3 MONTH PERIOD.
- EXPANA TO INCLUDE
3 OFC'S FROM EACH
PATROL SHIFT (D/A/M)
DC - CHIEF
CC - LT. FLATT.

The following information was not in the original request from or for "The FHPD Analysis 2022". But, through the collection of various data (i.e., Traffic Stops, Vehicle Searches, Demographics, etc....) and investigating the current capabilities of the CLEMIS system that FHPD is currently using, another possible way of collecting data has been discovered. All FHPD Officers in Patrol are required to produce an Activity Log for each day worked on their shift. This log encompasses activities the officer was dispatched to, found on patrol, any special attentions or details worked, and any other pertinent information the officer handled during their shift. With these Activity Logs, various parameters can be set in CLEMIS (*must fill in drop down categories) to capture demographics of an individual, if a vehicle was searched during this encounter, and if an arrest, citation or warning had been issued. CLEMIS advised at this point there is only one port for this information and does not have the capability to capture more than one individual. CLEMIS further advised that these parameters are not available to close out a Call For Service (CFS) and this information would only be enclosed if a report (CFS Report/CR Report) or a citation/warning was issued.

At this point, FHPD is conducting a "Pilot" with three Day Shift Officers (Officer Rohrer, Officer V. Rodriguez, and Officer Quinonez) asking them to use the Traffic Module for the Activity Logs, which engages the parameters covered in this document. CLEMIS advised that a report can be programed in "Business Objects" to extract this information and after a couple weeks this information can be compiled and will be sent to me for review and assessment. As an example, a current patrol activity log, a pilot activity log and an example MSP Activity Log has been attached to this interoffice. MSP has used their activity log for many years, using their own software UD-2x MSP form (Activity Log / Field Services Bureau Automated Daily). Also attached to this interoffice are the screen shots for the current Activity Log close out of an incident and the "Pilot" screen shots of the Activity Log close out of an incident. Both close out functions would add that incident to the Officer's Activity Log.

READ & FORWARDED
[Signature] 1-26-23
CAPTAIN
ADMINISTRATIVE BUREAU

RECOMMEND APPROVAL
[Signature] 1.26.23
ASSISTANT CHIEF
ADMINISTRATIVE BUREAU

RECOMMEND COMPILED DATA REVIEW IN 3 MO'S TO DETERMINE

Example MDC Screen Shots

Screenshot provided by FHPD.

Activity -[FHFLATTM] - [Log Activity]

CLEMIS

PRODUCTION (APP.33)
1.0.0.402

Welcome FHFLATTM

Activity Log ID:2778468(FHFLATTM)
Shift Date/Time:01/23/2023 12:04--01/23/2023 16:30

Activity Log (4) Edit Activity Import Activity Refresh

Activity Category *
 TRAFFIC STOP-TS
 POLICE TRAINING
 PRISONER BOOKING
 PRISONER DETAIL
 PRISONER TRANSPORT
 PROSECUTOR OFFICE
 PUBLIC RELATIONS
 RADAR
 RANGE DETAIL
 REPORT WRITING
 REVIEW/ASSIGN REPORT
 RIDE ALONG
 ROAD SUPERVISION
 S.E.
 SCHOOL CHECK
 SCHOOL CROSSING
 SHORT BREAK
 SIGNAL X
 SPECIAL EVENT DETAIL
 STATION DETAIL
 SUBDIVISION CHECK
 SUBPOENA DELIVERY
 SURVEILLANCE
 TRAFFIC CONTROL
 TRAFFIC ENFORCEMENT
 TRAFFIC STOP BU
 TRAFFIC STOP-TS
 UNION ACTIVITIES
 VACATION HOUSE CHECK
 VEHICLE INSPECTION
 VEHICLE MAINTENANCE

Location

Reporting Area
Select Reporting Area

Activity End Date *
1/23/2023 12:02

Source Type
CAD

Incident No/Ticket ID

mins - 5 mins

Imported Comments
Status: UO-UNIT ON |Time: Jan 23 2023 12:02:47 PM |Duration (secs): 0

Activity Count 0 Overtime

Reason of Stop * Search * Gender * Age *

Male Female

Hazardous Non-Hazardous Warning Code Violation

0 0 0 0

save reset

Example Activity Log Reports

Screenshots provided by FHPD.

CURRENT LOG

FARMINGTON HILLS PD DAILY ACTIVITY LOG									
Officer Name / Badge C Towns / 00527		Division Patrol		Date 01/13/2023		Total Time / Overtime 480 / No		Unit 1 / Car Nbr 1 527 / 858	
Officer Name / Badge S Ahmed / 00462		Shift Days (2)		Start / End Time 07:00 / 15:00		Activity/ Patrol Time (%) 327 / 153 (32 %)		Start / End / Total Mileage 71440 / 71507 / 67	
Checklist									
PATROL VEHICLE CHECKLIST		CHECKED OK?		COMMENTS		PATROL VEHICLE CHECKLIST		CHECKED OK?	
EQUIPMENT		YES	NO			EQUIPMENT		YES	NO
Does the Fire Extinguisher need to be charged?		<input type="checkbox"/>	<input checked="" type="checkbox"/>			Is Heavy Vest Bag missing the security tag?		<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is Plano Box missing a PBT or an AED?		<input type="checkbox"/>	<input checked="" type="checkbox"/>					<input type="checkbox"/>	<input type="checkbox"/>
Activity Details									
START	END	TOTAL	CATEGORY	COUNT	INCIDENT NBR	LOCATION	NOTES / ACTIVITIES		
07:03	07:03	0.00	UNIT STATUS			Reporting Area:	STATUS	TIME	
							UD-UNIT ON	Jan 13 2023 07:03:21 AM	
07:30	07:50	20.00	MISC ROAD DUTIES			Reporting Area:	Comments:	patrolling district	
							Misc Text:		
07:53	08:01	8.00	TRAFFIC STOP	0	230002091	W TWELVE MILE RD/ORCHARD LAKE RD Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 07:53:28 AM	
08:30	08:50	20.00	MISC ROAD DUTIES			Reporting Area:	Comments:	patrolling district	
							Misc Text:		
08:52	08:59	7.00	TRAFFIC STOP	0	230002102	W TWELVE MILE RD/NEW MARKET ST Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 08:52:29 AM	
08:55	08:57	2.00	CITATIONS		230002102 / CIT: 23H00425	UPON NB ORCHARD LAKE RD and AT/NEAR 12 MILE RD Reporting Area:	Misc Text:	Status: USCIT-CITATION ON Time: 1/13/2023 8:55 AM Ticket updated from ticket entry main screen screen (Uniform).	
09:01	09:15	14.00	DISPATCHED CALLS	0	230002105	26315 FIELDSTONE Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 09:01:28 AM	
09:19	09:29	10.00	DISPATCHED CALLS	0	230002107	31455 NORTHWESTERN HWY Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 09:19:46 AM	
09:57	10:05	7.92	TRAFFIC STOP		230002111	W TWELVE MILE RD/ORCHARD LAKE RD Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 09:57:57 AM	
10:00	10:02	1.75	CITATIONS		230002111 / CIT: 23H00427	UPON NB ORCHARD LAKE RD and AT/NEAR 12 MILE RD Reporting Area:	Misc Text:	Status: USCIT-CITATION ON Time: 1/13/2023 10:00 AM Ticket updated from ticket entry main screen screen (Uniform).	
10:03	10:03	0.35	CITATIONS		230002111 / CIT: 23H00427	UPON NB ORCHARD LAKE RD and AT/NEAR 12 MILE RD Reporting Area:	Misc Text:	Status: USCIT-CITATION ON Time: 1/13/2023 10:03 AM Ticket updated from ticket entry main screen screen (Uniform).	
10:19	10:39	19.50	DISPATCHED CALLS		230002114	30239 W THIRTEEN MILE RD Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 10:19:51 AM	
10:43	12:45	122.00	DISPATCHED CALLS	0	230002116	36024 WILLOW CT S Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 10:43:08 AM	
12:59	13:30	31.00	DISPATCHED CALLS	0	230002141	VENICE CT/MIDDLEBELT RD Reporting Area:	STATUS	TIME	
							USDP-DISPACHED	Jan 13 2023 12:59:56 PM	
13:30	13:57	27.00	CRASH REPORTS	0	230002141	PRIMARY RD: VENICE CT/INT. RD: MIDDLEBELT RD Reporting Area:	Misc Text:	Status: USCR-CRASH ON Time: 1/13/2023 1:30 PM Crash updated in main header screen.	
13:59	14:30	31.00	MEAL BREAK	0		Reporting Area:	STATUS	TIME	
							USMB-MEAL BREAK	Jan 13 2023 01:59:03 PM	
14:30	14:40	10.00	OTHER			Reporting Area:	Comments:	fuel	
							Misc Text:		
Category Summary									
CATEGORY		TOTAL	CATEGORY		TOTAL	CATEGORY		TOTAL	

PILOT LOG

FARMINGTON HILLS PD DAILY ACTIVITY LOG									
Officer Name / Badge B Rohrer / 00391		Division Traffic		Date 01/18/2023		Total Time / Overtime 490 / No		Unit 1 / Car Nbr 1 391 / 867	
Officer Name / Badge		Shift Days (2)		Start / End Time 06:50 / 15:00		Activity/Patrol Time (%) 333 / 157 (32 %)		Start / End / Total Mileage 95836 / 95888 / 52	
Checklist									
PATROL VEHICLE CHECKLIST		CHECKED OK?		COMMENTS		PATROL VEHICLE CHECKLIST		CHECKED OK?	
EQUIPMENT		YES	NO			EQUIPMENT		YES	NO
Does the Fire Extinguisher need to be charged?		<input type="checkbox"/>	<input checked="" type="checkbox"/>			Is Heavy Vest Bag missing the security tag?		<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is Plano Box missing a PBT or an AED?		<input type="checkbox"/>	<input checked="" type="checkbox"/>					<input type="checkbox"/>	<input type="checkbox"/>
Activity Details									
START	END	TOTAL	CATEGORY	COUNT	INCIDENT NBR	LOCATION	NOTES / ACTIVITIES		
06:50	07:15	25.00	BRIEFING			STATION	Comments: ROLL CALL EQUIP Misc Text:		
07:17	07:25	8.00	DISPATCHED CALLS	0	230002962	31550 NORTHWESTERN HWY Reporting Area:	STATUS: TIME USDP-DISPATCHED: Jan 18 2023 07:17:07 AM		
07:27	07:30	3.00	TRAFFIC STOP	0	230002963	NORTHWESTERN HWY/MIDDLEBELT RD Reporting Area:	Comments: VERY POLITE, ONLY 1 PRIOR TICKET STATUS: TIME USDP-DISPATCHED: Jan 18 2023 07:27:42 AM ADDITIONAL INFO: VALUE Reason for Contact: TVI:Traffic Vio. Outcome of Stop: VWR:Verbal Warn Search: NS:No Search Race: M:Middle Eastern Gender: M:Male Known: N Age: 89		
07:35	07:57	22.00	TRAFFIC ENFORCEMENT			16/POWER Reporting Area:	Comments: TRAFF ENFORCE, STOP SIGN Misc Text:		
07:58	08:02	4.45	TRAFFIC STOP	0	230002967	TEN MILE RD/POWER RD Reporting Area:	Comments: VERY POLITE, NO PRIOR TICKETS, TINTED WINDOWS STATUS: TIME USDP-DISPATCHED: Jan 18 2023 07:58:38 AM ADDITIONAL INFO: VALUE Reason for Contact: TVI:Traffic Vio. Outcome of Stop: VWR:Verbal Warn Search: NS:No Search Race: M:Middle Eastern Gender: F:Female Known: N Age: 41		
08:20	08:40	20.00	DETAIL	0		126KENDALWOOD Reporting Area:	Comments: GIFT CARD DETAIL FOR THE DESK. JF83141 OH Misc Text:		
09:00	09:30	30.00	SUBDIVISION CHECK			SECTION 14 Reporting Area:	Comments: NO PROBLEMS Misc Text:		
09:45	10:13	28.95	OTHER	0		ETWORK Reporting Area:	STATUS: TIME USST-STATION: Jan 18 2023 09:45:07 AM		
10:01	10:09	7.70	CLEAR CR		230002987	24899 WOODVIEW CT FARMINGTON HILLS MI 48335 Reporting Area:	Misc Text: Status: USCLR2-CLEAR - CR ON Time: 1/18/2023 10:01 AM CLEAR report updated in admin tab.		
10:19	10:26	7.00	VEHICLE MAINTENANCE	0	230002988	30775 W TWELVE MILE RD Reporting Area:	STATUS: TIME USDP-DISPATCHED: Jan 18 2023 10:19:35 AM		
10:37	10:58	21.00	TRAFFIC STOP	0	230002990	W TWELVE MILE RD/ORCHARD LAKE RD Reporting Area:	Comments: OFC QUINONEZ MADE A STOP FOR LINK REASONS AND I BACKED. THE DRIVER WAS ARRESTED FOR A WARRANT AND EVENTUALLY GCW VIO STATUS: TIME		

MSP LOG

FIELD SERVICES BUREAU AUTOMATED DAILY

Start	Clear	Activity	Code	Location	Activity Description / Notes
UD-2X Michigan State Police OFFICER(S) KURISH, CHRISTOPHER ASSIGNMENT(S) GENERAL 6:00 AM - 2:00 PM BADGE 1367 DATE 07/04/2017 Tuesday WEAPONS RIFLE 2228 - LE231667 TASER 624667 - KURISH EQUIPMENT PORTABLE 9630582 - SPEED MEASUREMENT INFORMATION OUT IN Display Segment Check N N Internal Verification N N Range Test N N Audio Test N N Patrol Speed Test N N					
Assignment #1 GENERAL DISTRICT 2 METRO SOUTH POST POST 22 HOLIDAY OT 32925 7/4/2017 6:00 AM KURISH					
6:00 AM		ON			ON-DUTY
6:00 AM	8:10 AM	CI		SCHOOLCRAFT / 96	2016 DODG BLU, NO DAMAGE
6:10 AM		STOP	8062	E 96 / MERRIMAN	1999 TOYT CAMRY TAN B/F, F1 - FRONT DRIVER
		VI			1999 TOYT TAN
	8:20 AM	VW	8062		B/F, [REDACTED]
6:20 AM		STOP	8062	E I-96 / TELEGRAPH	2007 CHEV IMPALA TAN B/M, F1 - FRONT DRIVER
		VI			2007 CHEV TAN
		VW	8062		B/M, [REDACTED]
	8:35 AM	S	8180		#X2553746 B/M, [REDACTED]
6:35 AM		STOP	8062	E 96 / JOY	2017 FORD EXPLORER WHI W/M, F1 - FRONT DRIVER
		VW	8062		W/M, [REDACTED]
	8:45 AM	VI			2017 FORD WHI
6:55 AM		STOP	8062	W I-96 / LIVERNOIS	2015 FORD FUSION MAR B/M, F1 - FRONT DRIVER
		VW	8062		B/M, [REDACTED]
		S	8180		#X2553747 B/M, [REDACTED]
	7:10 AM	VI			2015 FORD MAR
7:15 AM		STOP	8061	S M-10 / LIVERNOIS	2015 CHRY WHI B/F, F1 - FRONT DRIVER
		S	8180		#X2553748 B/F, [REDACTED]
		VI			2015 CHRY WHI
	7:30 AM	VW	8061		B/F, [REDACTED]
7:30 AM	7:40 AM	CA		S 10 / LINWOOD	2008 DODG SIL
8:15 AM	8:35 AM	OA	54003	LONYO / MICHIGAN	OFFICER ASSIST
8:35 AM	8:45 AM	PIV		MICHIGAN / LONYO	U/M, [REDACTED] USED MOBILE FINGERPRINT SCANNER FOR TROOPER GREEN.
8:47 AM	9:40 AM	OA	93001	W 94 / 30TH	OFFICER ASSIST
9:45 AM		ORIG	93001	W. I-94 / W GRAND	#022-0006025-17 B/M, F1 - FRONT DRIVER
		VW	8144		B/M, [REDACTED]
		ARR	8281		B/M, [REDACTED] 1
		VI			2009 CHEV WHI
	10:15 AM	TOW			2009 CHEV WHI, GOCH & SONS TOWING
10:15 AM		STOP	8062	W I-96 / SCOTTEN	2013 FORD FUSION BLK B/M, F1 - FRONT DRIVER
		VW	8062		B/M, [REDACTED]
		VI			2013 FORD BLK
	10:30 AM	S	8180		#X2553750 B/M, [REDACTED]
10:30 AM		ORIG	54003	E I-96 / JOY	#NRO-000022-17 B/M, [REDACTED] F1 - FRONT DRIVER
		S	8180		#X2553751 B/M, [REDACTED]
		VI			TEMP/MI 2003 MERC BLK
		ARR	8271		B/M, [REDACTED] 1
	10:45 AM	VW	8062		B/M, [REDACTED]
10:55 AM		STOP	8062	W I-96 / MERRIMAN	2017 CHEV SILVERADO SIL B/M, F1 - FRONT DRIVER
		VI			2017 CHEV SIL
		S	8180		#X2553752 B/M, [REDACTED]
	11:05 AM	VW	8062		B/M, [REDACTED]
11:15 AM	11:25 AM	PI		SCHOOLCRAFT / MERRIMAN	EXXON

Officer Activity Log Example – MSP Trooper

MICHIGAN STATE POLICE
 FIELD SERVICES BUREAU DAILY REPORT

Redacted						HOURS WORKED	DATE
KURISH, CHRISTOPHER		Trooper	1307	POST		0600 - 1400	07/04/2017
ASSIGNMENT 1	HOURS	ASSIGNMENT 2	HOURS	ASSIGNMENT 3	HOURS	ASSIGNMENT 4	HOURS
GENERAL (32925)	0600 - 1400						
JURY		JURY		REMOVED BY		BY AUTHORIZED BY	
8.00		8.00		COUTURIER, LANCE		SGT LISS	

HOURS WORKED		
	JR.	SR.
Obligated		
Administrative		
Enforcement		3.97
Incident		1.00
Investigative		0.67
Other		2.36
TOTAL	0.00	8.00

PATROL HOURS/MILES				
	JR/TIME	MILES	SR/TIME	MILES
Freeway			2.37	70
Trunkline				
County				
Traffic				
Other				71
TOTAL	0.00	0	2.37	141

CRIMINAL INCIDENT HOURS				
DESCRIPTION	FILE CLASS	INCIDENT NUMBER	JR.	SR.
NONCRIMINAL INCIDENT HOURS				
DESCRIPTION	FILE CLASS	INCIDENT NUMBER	JR.	SR.
TRAFFIC VIOLATIONS	54003			0.33
ACCIDENT, TRAFFIC	93001			0.88
ACCIDENT, TRAFFIC	93001	022-0006025-17		0.50
TRAFFIC VIOLATIONS	54003	NRO-0000022-17		0.25
TRAFFIC VIOLATIONS	54003	NRO-0000022-17		0.25

ARRESTED						
NAME	RACE	SEX	INCIDENT NO.	OFFICER LAST	LOCATION	CODE
	B	M	022-0006025-17	KURISH	W I-94	8281
	B	M	NRO-0000022-17	KURISH	E I-96	8271
	B	M	NRO-0000022-17	KURISH	W I-96	8281

VEHICLES TOWED			
PLATE/IN	MAKE	COLOR	TOWED TO/ BY
	CHEV	WHI	GOCH & SONS TOWING

USE OF FORCE	
Type	Total

PROPERTY SEIZED	
Type	Total

CRASH SUMMARY	
Type	Total
Traffic	1

	JR/TIME	JR.	SR/TIME	SR.
ADMINISTRATIVE	Total: 0:00	0	0:00	0
Operational Support				
Court				
Desk				
Training				
Recruiting				
Sergeant Duties				
Public Relations				
Pos/District Meeting				
Report Writing				
Subpoena Service				
CONTACTS	Total: 0:00	0	3:25	16
Contacts Per Hour				6.28
Original Traffic Stops			0.50	2
Traffic Stops			2.25	11
Citations (Non-Driver)				
Verbal Warnings (Non-Driver)				
Car Assists			0.33	2
Person(s) Investigated			0.17	1
INVESTIGATIVE	Total: 0:00	0	0:56	18
Vehicles Inspected				14
Cars Investigated			0.33	2
Property Inspections			0.33	2
Liquor Inspections				
Background Investigation				
Address Check				
ENFORCEMENT	Total: 0:00	0	1:22	26
Citations (Driver)				9
Verbal Warnings (Driver)				14
Vehicles Towed				1
Officer Assists			1.22	2
SOR - Address Check				
INCIDENTS	Total: 0:00	0	1:00	3
AT POST				
DISPATCHED			0.50	1
OTHER AGENCY				
PATROL			0.50	2
Supplementals				
ARRESTS	Total: 0:00	0	0:00	3
FUGITIVE FELONY				
FUGITIVE MISDEMEANOR				
ORIGINAL FELONY				
ORIGINAL MISDEMEANOR				3

Impeding Traffic Citations – New FHPD Policy

- Analysis of traffic citation data revealed the highest volume of citations for 2020, 2021, and 2022 was Impeding Traffic
- FHPD personnel advised it was a long-standing practice for officers to offer drivers the choice to receive a Civil Citation for Impeding Traffic versus a State Motor Code offense that could have a negative impact on driver licenses and vehicle insurance
- The City adopted all State Motor Code violations in 2003
- Research revealed:
 - An Impeding Traffic Civil Citation (\$200) could cost more than the original offense (Speeding 1 – 5 MPH over \$140)
 - The City receives the entire fine amount for a Civil Citation versus a shared amount for a State Motor Code Citation
 - The City Attorney manages city Civil Citations, while State Motor Code Citations are managed by the State Prosecutor, who is employed at the county level
 - Other local Cities employ the same practice, including:
 - City of Novi
 - City of Troy
 - Township of West Bloomfield

2003 City Ordinance Resolution

CITY OF FARMINGTON HILLS

CITY CLERK'S OFFICE
31555 W. 11 Mile Road, Farmington Hills, MI 48336-1165
(248) 871-2410

R-191-03

RESOLUTION

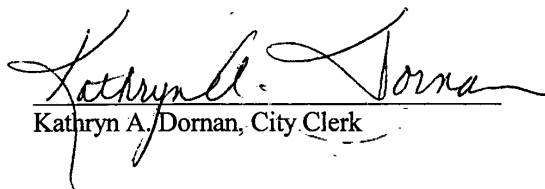
IT IS RESOLVED, that City Council hereby ENACTS Ordinance C-7-03, amending the City Code, Chapter 30, Article II, to adopt by reference the Michigan Vehicle Code (MVC) and the Uniform Traffic Code for Cities, townships, and Villages (UTC), for the purpose of regulating traffic and motor vehicles in the City of Farmington Hills and to repeal the version of the City Uniform Traffic Code as previously set forth in Article II and the vehicle size, weight and load regulations as previously set forth in Article VII of Chapter 30; and

FURTHER RESOLVES, approval of the ordinance summary for publication.

Motion by:	MC RAE
Support by:	OLIVERIO
Roll Call Vote:	
Yeas:	BARNETT, BATES, BRICKNER, ELLIS, MC RAE AND OLIVERIO
Nays:	NONE
Absent:	NONE
Abstentions:	NONE

MOTION CARRIED 6-0.

I, Kathryn A. Dornan, the duly authorized City Clerk of the City of Farmington Hills, Oakland County, Michigan, do hereby certify that the foregoing is a true copy of a resolution adopted by the City Council of the City of Farmington Hills on October 20, 2003.


Kathryn A. Dornan, City Clerk

DATE: October 21, 2003

2003 City Ordinance

ORDINANCE NO. C-7-2003
CITY OF FARMINGTON HILLS
OAKLAND COUNTY, MICHIGAN

AN ORDINANCE TO AMEND CHAPTER 30, ARTICLE II OF THE CITY CODE OF THE CITY OF FARMINGTON HILLS TO ADOPT BY REFERENCE THE MICHIGAN VEHICLE CODE (MVC) AND THE UNIFORM TRAFFIC CODE FOR CITIES, TOWNSHIPS, AND VILLAGES (UTC), FOR THE PURPOSE OF REGULATING TRAFFIC AND MOTOR VEHICLES IN THE CITY OF FARMINGTON HILLS, AND TO REPEAL THE VERSION OF THE CITY UNIFORM TRAFFIC CODE AS PREVIOUSLY SET FORTH IN ARTICLE II AND THE VEHICLE SIZE, WEIGHT AND LOAD REGULATIONS AS PREVIOUSLY SET FORTH IN ARTICLE VII OF CHAPTER 30.

THE CITY OF FARMINGTON HILLS ORDAINS:

Section 1.

The Farmington Hills City Code, Chapter 30, "Traffic and Motor Vehicles", Article II, "Uniform Traffic Code", is hereby amended in its entirety to read as follows

ARTICLE II. MOTOR VEHICLE AND UNIFORM TRAFFIC CODES DIVISION 1. MOTOR VEHICLE CODE

Sec. 30-26. Adoption by Reference.

The Michigan Vehicle Code, being Act 300 of the Public Acts of 1949, MCL 257.1, et seq., as amended now and in the future is hereby adopted and incorporated by reference as an ordinance of the City of Farmington Hills.

Sec. 30-27. References in the Vehicle Code.

Where necessary to the enforcement of the Michigan Vehicle Code or the collection of fines, costs and penalties for violations as a City Ordinance, references in the Michigan Vehicle Code to "local authorities", "local authority" or "authority having jurisdiction" shall mean the City Council of the City of Farmington Hills; references to "municipality" shall mean the City of Farmington Hills; references to "municipal charter" shall mean the Charter of the City of Farmington Hills; references to "local ordinances" shall mean the Code of Ordinances of the City of Farmington Hills, and references to the "city" shall mean the City of Farmington Hills.

Sec. 30-28. Copies

Printed copies of the Michigan Vehicle Code, as amended from time to time, shall be kept on file in the office of the City Clerk and made available to the public at all times the office is open.

Sec. 30-29. Limitations.

Violations of the Michigan Vehicle Code for which the maximum period of imprisonment is greater than 93 days shall not be enforced by the City of Farmington Hills as an ordinance violation.

Sec. 30-30. Penalties.

The penalties provided in the Michigan Vehicle Code are adopted by reference subject to the limitations stated in Section 30-29.

DIVISION 2. UNIFORM TRAFFIC CODE

Sec. 30-51. Adoption.

The City of Farmington Hills hereby adopts and incorporates by reference as an ordinance of the City the Uniform Traffic Code for Cities, Townships, and Villages as promulgated by the Director of the Michigan Department of State Police pursuant to the Administrative Procedures Act of 1969, 1969 PA 306, MCL 24.201 to 24.328 and made effective October 30, 2002, and all future amendments and revisions of the Uniform Traffic Code when they are promulgated and effective in this state.

Sec. 30-52. References in the Uniform Traffic Code.

References to "governmental unit" and "municipality" in the Uniform Traffic Code for Cities, Townships, and Villages shall mean the City of Farmington Hills

Sec. 30-53. Copies.

Printed complete copies of the October 2002 edition of the Uniform Traffic Code for Cities, Townships, and Villages, and all future amendments and revisions thereto, shall be kept on file in the office of the City Clerk and made available to the public at all times the office is open.

Sec. 30-54. Penalties.

The penalties provided in the Uniform Traffic Code for Cities, Townships, and Villages are adopted by reference

Section 2. Repealer

Chapter 30, Article II, Divisions 1 through 10, inclusive, and Article VII of the City Code, as such existed prior to the effective date of this Ordinance, are hereby expressly repealed in their entirety. All other ordinances, parts of other ordinances, or other sections of the

City Code in conflict with this Ordinance are repealed only to the extent necessary to give this Ordinance full force and effect.

Section 3. Severability

Should any section, subdivision, clause, or phrase of this Ordinance be declared by the courts to be invalid, the validity of the Ordinance as a whole, or in part, shall not be affected other than the part invalidated.

Section 4. Savings

All proceedings pending and all rights and liabilities existing, acquired, or incurred at the time this Ordinance takes effect, are saved, and may be consummated according to the law in force when they were commenced.

Section 5. Effective Date

The provisions of this Ordinance are ordered to take effect twenty-one (21) days after enactment.

Section 6 – Enactment

This Ordinance is declared to have been enacted by the City Council of the City of Farmington Hills at a meeting called and held on the 20th day of October, 2003 and ordered to be given publication in the manner prescribed by law.

47th District Court Traffic Fine Schedule

47th District Court Traffic Fine Schedule
 Traffic Division Telephone: (248) 871-2920

INFRACTION	Violation Fees (SOS Points)	If in a Construction or School Zone
Speed 1 - 5 Over	\$140 (1)	\$150 (3)
Speed 6 - 10 Over	\$145 (2)	\$170 (3)
Speed 11- 15 Over	\$150 (3)	\$180 (4)
Speed 16 - 20 Over	\$160 (4)	\$200 (5)
Speed 21- 25 Over	\$240 (4)	\$340 (5)
Speed 26 + Over	\$240 (4)	\$340 (5)
Expressway Speeding: 1 - 5 over limit	\$150 (0)	\$160 (3)
Expressway Speeding: 6 - 10 over limit	\$160 (1)	\$180 (3)
Expressway Speeding: 11 - 15 over limit	\$180 (2)	\$220 (4)
Expressway Speeding: 16 - 25 over limit	\$210 (3)	\$280 (5)
Expressway Speeding: 26+ over limit	\$240 (4)	\$340 (5)
Impeded Traffic	\$200 (0)	\$260 (0)
Impeded Traffic (w/ Accident)	\$225 (0)	\$310 (0)
Seat Belt Viol. - Driver	\$65 (0)	
Seat Belt Viol. - Passenger	\$65 (0)	
Child Restraint Law	\$130(0)	
No Proof of Insurance	\$160 (0)	
With Valid Proof *For Date of Incident*	\$25 (0)	
Parking - Handicap	\$140(0)	
Parking - Farminton Downtown Hourly Viol.	\$25 (0)	
All Other Parking Violations	\$70 (0)	
Equip. Viol.: (Waivable)	\$130 (0)	
Equip. Viol.: Excessive Smoke (Waivable)	\$130 (0)	
Equip. Viol.: Headlight (Waivable)	\$130 (0)	
Equip. Viol.: Mirror (Waivable)	\$130 (0)	
Equip. Viol.: Tinted Windows (Not Waivable)	\$130 (0)	
Equip. Viol.: Tinted Windows (Waivable)	\$130 (0)	
Equip. Viol.: Tail Lights (Waivable)	\$130 (0)	
Equip. Viol.: Excessive Noise/Muffler (Waivable)	\$130 (0)	
Equip. Viol.: Bumper (Miss./Defect.)(Waivable)	\$130 (0)	
Equip. Viol.: Tires (Waivable)	\$130 (0)	
Equip. Viol.: Turn Signals (Waivable)	\$130 (0)	

Equip. Viol.: Windshield (Waivable)	\$130 (0)	
Expired Plate	\$130 (0)	
No Registration on Person (Waivable)	\$130 (0)	
Careless Driving	\$210 (3)	\$280 (3)
Careless Driving (w/ Accident)	\$240 (3)	\$340 (3)
Disobey Stop Sign	\$140 (3)	\$150 (3)
Disobey Traffic Control Device	\$140 (2)	\$150 (2)
Disobey Traffic Signal	\$140 (3)	\$150 (3)
Drove in an Unsafe Manner	\$140 (2)	\$150 (2)
Drove Left of Center	\$140 (2)	\$150 (2)
Drove on Priv. Prop. to Avoid Traf. Contr. Dev.	\$140 (2)	\$150 (2)
Drove w/o Lights	\$140 (2)	\$150 (2)
Failed to Dim Lights	\$140 (2)	\$150 (2)
Failed to Signal/Observe	\$140 (2)	\$150 (2)
Failed to Stop for School Bus	\$240 (3)	\$340 (3)
Failed to Stop in Assured Clear Distance	\$180 (2)	\$220 (2)
Failed to Stop Leaving Private Property	\$140 (2)	\$150 (2)
Failed to Use Due Care	\$140 (2)	\$150 (2)
Failed to Yield for Oncoming Traffic	\$140 (2)	\$150 (2)
Failed to Yield Right of Way	\$140 (2)	\$150 (2)
Failed to Yield Right of Way at Stop Sign	\$140 (2)	\$150 (2)
Failed to Yield to Emerg. Veh. (w/ Accident)	\$240 (2)	\$340 (2)
Failed to Yield to Emergency Vehicle	\$210 (2)	\$280 (2)
Failed to yield to Stationary Emergency Responder	\$540 (2)	\$940 (2)
Followed too Closely	\$140 (2)	\$150 (2)
Improper Backing	\$140 (0)	\$150 (0)
Improper Crossing - Divided Highway	\$140 (2)	\$150 (2)
Improper Lane Use	\$140 (2)	\$150 (2)
Improper Passing	\$140 (3)	\$150 (3)
Improper Turn	\$140 (2)	\$150 (2)
Obstructed Vision	\$130 (2)	
Pedestrian Viol.	\$140 (0)	
Prohibited Turn	\$140 (2)	\$150 (2)
Refused PBT (Non-commercial Vehicle)	\$240 (0)	
Squealing Tires/Unsafe Start	\$140 (0)	\$150 (0)
Texting While Driving: 1st Viol.	\$240 (0)	\$340 (0)
Texting While Driving: 2nd or Subseq. Viol.	\$340 (0)	\$540 (0)
Too Fast for Conditions	\$140 (2)	\$150 (2)
Unattended Vehicle - Running	\$130 (0)	
Viol. of Basic Speed Law	\$140 (2)	\$150 (2)

Wrong Way on One-Way Street	\$140 (2)	\$150 (2)
Violation with Accident (except as listed above) (SOS points are equal to the non-accident charge)	\$180	\$220

If your violation is not listed, a mandatory appearance may be required. Please contact the Traffic Division at (248) 871-2920.

Inability to Separate Traffic Citations from Civil Infractions

Data Source: P2 – 6 Ticket Revenue

FHPD requested the District Court provide information on ticket revenue for 2021 and 2022. They stated that the information doesn't exist as they don't distinguish the origins of the money. Their records don't classify revenue in categories and could be from several different sources. (Emphasis added)

These sources include misdemeanors, ordinance violations, warrants, zoning violations, and traffic citations. I was also told, even if money is paid in a current year, it may be from an offense that happened years ago and is just being paid now which would skew the data.

Civil Infraction Revenue Breakdown

Data Source: P2 – 6 Ticket Revenue

- A speeding ticket (1-5 miles over) costs the motorist \$140 in fines/costs
- The breakdown of that ticket is \$40 goes to the State of Michigan, and \$100 would remain with the City of Farmington Hills.
- The Court would then send that portion of the fine/cost to the City, and it is deposited in the City's general fund.

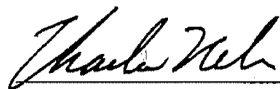
FHPD 2017 Impeding Traffic General Order

**FARMINGTON HILLS POLICE
 GENERAL ORDER**

17-007

SUBJECT IMPEDED TRAFFIC VIOLATIONS		
REFERENCE Michigan Motor Vehicle Code 257.676b		
DATE OF ISSUE 3-1-2017	EFFECTIVE DATE 3-1-2017	DISTRIBUTION
AMENDED DATE	EXPIRATION DATE 2-28-2017	CANCELS TB 13-036

The 47th District Court authorizes and supports the practice of allowing officers to issue a violation for Impeding Traffic when an officer has stopped a driver for a moving violation. The Impeded Traffic charge is appropriate when using discretion for a moving violation, as opposed to Parked- Blocking Traffic. Impeded Traffic is considered a moving violation and allows the court to impose sanctions against drivers who neglect to respond appropriately and therefore is the only "no point" violation authorized for use.



 Charles Nebus
 Chief of Police

Updated FHPD Alternative Traffic Violations General Order – November 10, 2023

FARMINGTON HILLS POLICE GENERAL ORDER

SUBJECT ALTERNATIVE TRAFFIC VIOLATIONS		
REFERENCE Michigan Motor Vehicle Code		
DATE OF ISSUE 11-10-2023	EFFECTIVE DATE 11-10-2023	EXPIRATION DATE 11-10-2028
AMENDED DATE/PURPOSE REPLACES G.O. 17-007 "IMPEDED TRAFFIC VIOLATIONS"		

It shall be the policy of the Farmington Hills Police Department to issue traffic citations to motorists for the infraction they are responsible for. As an alternative it shall be acceptable to issue a citation that is a reduction in severity of the same offense. For example, an officer (as a matter of discretion) is permitted to issue a citation for speeding; 5 miles-per-hour over the limit, in lieu of a citation for 15 miles-per-hour over the limit, if that was the reason for the stop. Officers may also elect to issue a written warning where appropriate. Properly documented traffic stops resulting in verbal warnings are also permitted.

Under no circumstances shall an officer issue a citation for Impeding Traffic, or any other violation if the driver did not commit that infraction.

The reduction of an infraction to an alternate lesser violation that was not committed in view of the officer is a function of the judicial process.


 Jeff King
 Chief of Police

Traffic Enforcement Plan for 2024

Inter-Office Communication

To: Chief Jeff King (Through the Chain of Command)
From: Sergeant Hadar Saad f
Date: December 14, 2023
Subject: Traffic Enforcement Plan for 2024

The Farmington Hills Police Department has studied the map of crash statistics from the Michigan State Police (Mi-CAT) in reference to the City of Farmington Hills. With keeping public safety as the utmost priority, the attached map shows the various locations of reported crashes within the city. Additionally, since August 2023, MSP has asked FHPD for assistance on primary investigations involving serious and fatal crashes on the local expressways (I-696 and 1-275) within the City of Farmington Hills. The Traffic Enforcement Plan for 2024 will be as follows;

This is based on complaints from citizens, parents, and Farmington Public School Staff.

3. **School bus stops** (i.e. Failure to Stop for School Bus) in the morning and afternoon.
4. **School zones** during school days, thirty (30) minutes before and after school hours (i.e. Speeding and Disobey Traffic Control Signal).

The analysis of the Mi-CAT map shows that the listed locations had the highest numbers of traffic crashes and severity of the crashes.

5. **Expressways** (i.e. Speeding, Following Too Close, Careless and Distracted Driving).
6. **Orchard Lake Road, from 1-696 overpass to 14 Mile Road** (i.e. Speeding, Unsafe Change, Reckless/Careless Driving).
7. **Grand River Avenue, Halsted to Haggerty Roads.** (i.e. Speeding, Reckless/Careless Driving and Failed to Yield)

This proposed plan does not restrict Farmington Hills Police Officers from the continued enforcement of the Michigan Motor Vehicle Code throughout the city but highlights areas of the police department that receives routine complaints or responds to a high volume of traffic crashes.

FHPD TRAFFIC ENFORCEMENT PROGRAM: MISSION, POLICY AND PROCEDURES AND BEST PRACTICE ANALYSIS

The Building Blocks to Create a Mission, Vision, and Core Values Statement to Govern FHPD Traffic Enforcement

Overview

In FHPD 2022 Phase One Report, a high priority was placed on the connection between the Department’s Mission, Vision, and Core Values – representing the high order goals and principles which guide and define the FHPD – and Department policies, procedures, training programs and accountability measures. As a starting point to assess the “big picture” as concerns FHPD’s traffic stop strategies and tactics, Winbourne returns to this methodology in our Phase Two Report. The foundation for this approach was cited at various points in the Phase One Report:

The essential values of FHPD are the basis of all policy, training, operations, and accountability systems. This principle is a foundational finding of the seminal study American Policing in 2022, commissioned by the US Department of Justice – Office of Community Oriented Policing Service (COPS):

“When police departments establish a set of organizational values – especially when they do so with community input - they create a foundational belief system on which all organizational and individual decision-making can be based. For instance, articulating that collaboration, leadership, ethics, excellence, and the respect for human dignity are an organization’s core values means that everything within [that organization] - from recruitment, promotions, discipline, to training.... must be in alignment with that which the organization purports to believe” –Jim Buermann, Director of the National Police Foundation.

Of priority focus in both the Phase One and this (Phase Two) report is verifying a consistent, unbroken connection between Mission/Vision/Core Values and the Department’s specific policies, strategies, operations, training, and accountability systems, in this case as pertains to FHPD Traffic Safety and Enforcement System and Programs. This nexus was represented graphically as follows:

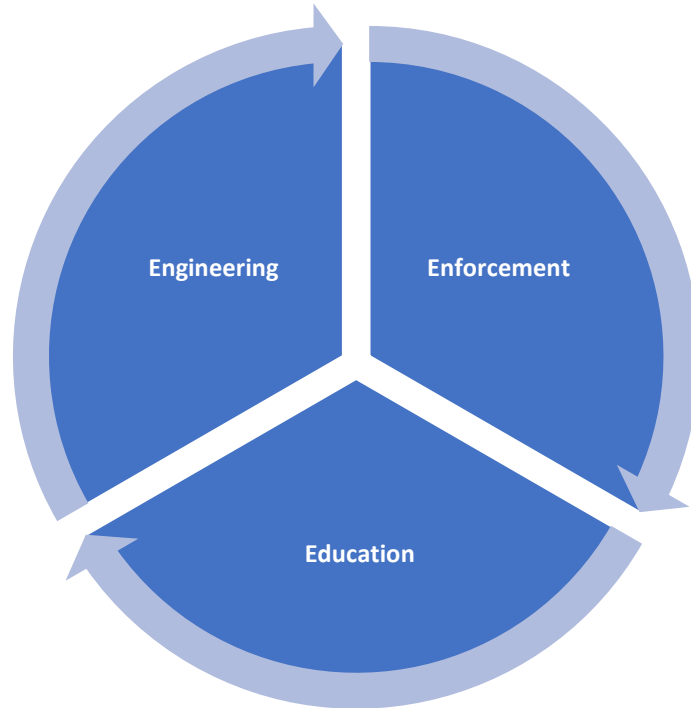


In other words, the FHPD traffic safety and enforcement system needs a clear and uniform Mission/Vision/Core Values manifesto as a subset of the overarching priorities and guiding principles of the Department as a whole. To put it plainly, the days of random, generalized issuance of traffic citations have been replaced – in model police departments – by purposeful, intensively data driven focus upon value-based strategies and practices.

When it comes to the strategic and tactical program of police traffic enforcement, model practices have evolved in high capability police agencies which are based upon the following formulae:

Triad One

The “Engineering, Enforcement, Education” safety triad. To that, we will argue, an emerging component of engineering is technology.



Triad Two

Police Strategies – especially traffic enforcement – must be Data informed, Community informed, and Police Practice informed



Below we will adapt these principles to present the practical components of a leading-edge Traffic Safety Mission and Strategic Mission Statement.

Commentary: The Traffic Safety Dilemma: Two Opposing, Unresolved Positions About the Value of Traffic Enforcement Programs in the U.S.

Case Statement One: The Case for Rigorous, Focused Enforcement of Traffic Laws

For all the controversy arising from the strategies and tactics used by police in the enforcement of traffic laws, the most important fact is that traffic stops are the most prevalent way – for better or worse – that police have contact with the public. In 2020, for example, the police contacted 61.5 million persons ages 16 and older. Of that number, 33 million were drivers or passengers in a traffic stop (7.9 million more were contacted as a result of traffic collisions). [Rand June 30, 2021; NHTSA, 2022]. Of greater significance is the statistic that on average, about 41 million citations for traffic infractions or citations are issued in the US every year [NHTSA 2022]. Given the coincidence of the annual number of police traffic related contacts – 46 million – and the annual number of traffic tickets issued – 41 million - this suggests that almost every person stopped receives a citation of some kind. This is not true, of course, as it is not uncommon for multiple tickets to be issued for a single traffic stop. (Regrettably, the

data for both multiple violations as well as warnings in lieu of citation is imprecise, inconsistent, and unreliable – a fact which will be discussed later in this report) But the point to focus upon is that the opportunity to promote traffic safety and – more tenuously – build relations between the police and public are overwhelmingly represented by the traffic enforcement function of the police.

Of additional significance (but much less frequency) is the legitimate traffic enforcement function leading to discovery of more significant crimes and threats to public safety, including the apprehension of dangerous criminals. The precise number of traffic stops that result in discovery of more serious crime remains statistically unknown on a consistent basis, however.

Most important, numerous studies establish – or suggest - a connection between traffic enforcement and reduction in automobile related death and injury.

For example, annual miles driven has increased throughout the United States since 1970, topping out at the end of 2019 at 3,269 million miles. By February 2021, the total had dropped 30% to 2,772 million miles. In this same period, traffic enforcement through citation and arrest decreased by about 18% on a national basis. Despite driving less, the 24% increase in the rate of motor vehicle deaths in 2020 was the largest since 1924 with 42,060 people dying in crashes. [Rand/Police One 2022]

Two significant studies support this correlation. The first is the often-cited “Fresno Study.” While somewhat distant in time (2004, but periodically tested and updated) the experiment was documented in the report titled “Aggressive traffic enforcement: a simple and effective injury prevention program” {Davis, Bennick et al}. What follows is our summary:

- **Method:** A vigorous enforcement program was established within Fresno, Calif, city boundaries using increased traffic patrol officers, and subsequent evaluation of data on citations, collisions, fatal collisions. The salient methodology was dramatic: the City of Fresno Police Department increased the staffing of its traffic division from 20 to 84 officers in 2003,
- **Results:** There were significant increases in citations issued – a 229% increase - with marked decreases in motor vehicle crashes, injury collisions, fatalities, and fatalities related to speed. There was a decrease in admissions from MVCs, a significant decrease in the number of patients with moderate injury severity), a decrease in hospital length of stay for all MVC victims, and a decrease in hospital charges for MVC patients. These changes were not seen in the area of Fresno County outside the area of increased enforcement.
- **Conclusions:** Aggressive traffic enforcement decreased MVCs, crash fatalities, and fatalities related to speed, and it decreased injury severity. This is a simple, easily implemented injury prevention program with immediate benefit.

The 2nd study of interest was conducted by NYPD in 2020. In a sense, it analyzed the reverse of the Fresno study. What follows is our summary:

- From March 12 to December 31, 2020, the NYPD wrote 52.9% fewer tickets than it did during the same period in 2019. During that same period, fatal crashes spiked 16%, resulting in 31 more traffic deaths, compared with the previous year. In the first quarter of 2021, traffic enforcement was down 37.2%, when compared with the same period the previous year—and fatal crashes were up 9.7%, compared with the first quarter of the previous year. The increase in fatal crashes argues for more enforcement of dangerous driving behavior, not less [Study of the Correlation Between Decreased Traffic Enforcement and Traffic Fatalities/Injuries, NYPD 2020]

It is important to note that both these studies – supporting the value of rigorous traffic enforcement and its favorable impacts to life safety – did not validate that any and all traffic enforcement correlates to positive outcomes. Equipment violations, non-dangerous, regulatory infractions, etc., were not included in what has been termed aggressive traffic safety enforcement initiatives:

- NYPD In 2019, the department wrote 747,343 tickets for moving violations. Of those, 90.4% were for “hazardous violations”—offenses such as speeding, texting, DUI, reckless/aggressive driving, and failing to yield to pedestrians. [NYPD Study 2022]
- A strategic vision, then, must be founded on illegal traffic acts *which imperil safety and result in injury or death*. (Our emphasis) These include DUI, aggressive/reckless driving, speed, disregard of pedestrian safety systems, inattention, violation of control laws in high vulnerability areas (e.g. school zones) etc.

In conclusion, there is a strong case to be made that traffic enforcement – specifically focused upon dangerous driving incidents rather than random, generalized citation – strongly correlates with improving life safety.

Case Statement Two: Uneven Enforcement, Racial Disparities, Tragic Outcomes and False Conclusions About the Value of Aggressive – or even Routine - Traffic Enforcement Demands New Approaches and Priorities.

When arguing that traffic enforcement saves lives, “correlation does not equate to causality”, as noted above. Researchers tend to be skeptical – even dismissive – of studies like Fresno or NYPD that argue the value of traffic enforcement based on life

safety outcomes. Even the idea that traffic stops deter crime by establishing a regular police presence has been debunked as long ago as the Kansas City Preventive Patrol Experiment in the 1970s, which concluded that there aren't sufficient numbers of police in the nation to be consistently present and visible to deter crime, and that random patrol and enforcement yields random and ineffectual results. The "science" of law enforcement and police activity doesn't exist and consists instead of inferences and wishful thinking.

Although the prevention of damage, death, and injury that results from a narrow bandwidth of dangerous driving supports the need for strategic traffic enforcement, "racial disparities in who the police stop make it a flashpoint for those who want to end the practice. As research continues to show widespread racial disparities of those stopped, it is increasingly seen as a practice that, if stopped, would serve the cause of social justice" [David D. Kirkpatrick, Steve Eder, Kim Barker, and Julie Tate New York Times, Oct. 31, 2021, Updated Nov. 30, 2021]

As a research fellow for the International City-County Managers Association (ICMA) [Winbourne – Kimerer] reviewed 118 Police and African American men/women fatal encounters (2014-2022) reveal the following as precursors to fatal encounters:

1. *Traffic stops*
2. *Vehicle pursuits*
3. *Misdemeanor crimes or low-level criminality (including infractions that would result in a ticket)*
4. *Arrest and control tactics, particularly involving bodily compression that endangers breathing or blood flow.*
5. *Subject resistance and non-compliance. In particular, relatively minor crimes or incidents which degenerate into resisting arrest, obstruction, or assault on an officer (the latter varying in degrees of severity)*
6. *In-custody management of arrestees*
7. *Volleys of gunfire (i.e., a large number of gunshots) by one or multiple officers are frequent: this fact both decreases chances for survival, and brings into question the training, capabilities, and self-discipline of officers involved in fatal encounters*
8. *Bystander or non-criminal victim fatalities*
9. *Finally, a number of fatal police encounters are best classified as accidental and non-intentional (this classification does not preclude a finding of negligence, however)*

Note: While metropolitan cities predominate in the number of fatal police encounters, smaller jurisdictions are also represented. Some cities have remarkably higher numbers of fatal police encounters, which are not attributable to large per capita population effects (e.g., Minneapolis, Baltimore).

While there are various statistics quantifying fatal encounters between police and African American men and women from 2014 to the present day, a conservative statistical estimate of the number of these encounters is nearly 180. Please bear in mind, this is the *beginning* of analysis: it is not a proxy for rigorous research and evidence-based examination. A small sampling of these incidents

- **Rayshard Brooks**, June 12, 2020: Atlanta, Georgia - Officer Involved shooting - DUI stop, grappling, attempted to flee when shot. One officer charged with murder; 2nd charged with aggravated assault. Chief Erika Shields resigned.
- **Ronald Greene**, May 10, 2019: Monroe, Louisiana – Arrest following a high-speed chase for traffic infraction. TASER/asphyxia/other force. Louisiana State Patrol. Greene was TASER-ed, punched and choked, then dragged face down after handcuffing and remained face down for nine minutes. No criminal charges against involved officers: one officer later died in a car crash; one was fired for a different excessive force complaint; one was suspended.
- **Philando Castile**, July 6, 2016: Falcon Heights, Minnesota - Traffic Stop. Officer involved shooting. Castile, who had a CCW permit, informed the officer he had a weapon in his glove compartment. The officer alleged Castile pulled out the weapon; the passenger (who was in the car with her 4-year-old child) disputes that account. Officer acquitted of manslaughter; fired from the department.
- **Samuel Vincent DuBose**, July 19, 2015: Cincinnati, Ohio – Traffic Stop/alleged assault (vehicle) on officer. Officer involved shooting. DuBose was stopped for a missing front license plate by a University of Cincinnati police officer. The officer alleged that DuBose tried to drive off, dragging the officer. Body-cam footage disputes that account. Officer tried twice, both resulted in hung juries.
- **Sandra Bland**, July 13, 2015: Waller County, Texas (Texas State Trooper) – Traffic Stop/arrest for obstructing/resisting. Committed suicide three days after arrest – discussed in this paper and Malcolm Gladwell's *Talking to Strangers*.
- **Walter Scott**, April 4, 2015: North Charleston South Carolina – Traffic stop (broken taillight). Scott shot as he attempted to flee. Officer involved shooting. Officer sentenced to 20 years imprisonment.

The argument, then, in opposition to the huge number of traffic citations issued in this country: *Is it worth the risk of tragic outcomes?* This argument begs the question about public safety priorities. As David Weisburd (renowned GMU Criminologist) posed in so many words: “Why do the police need to be overly committed to enforcing low-consequence laws and being in places they don't need to be?”

This last point, we argue, is foundational: Most reasonable people would point out that it wasn't the petty shoplift, the broken taillight, or selling single cigarettes that were the reasons for the use of deadly force; it was subsequent, escalating behaviors, like assault or resisting arrest, that were the true predicate for these outcomes. That rationale, however, does not preclude reasonable people from questioning the need for these

potentially tragic outcomes to be set in motion in the first place, particularly given the escalating number of police contacts with subjects in behavioral health crisis or co-occurring mental/alcohol/drug disabilities. And lest we forget, police officers – injured, killed, fired, or imprisoned – are also casualties of these arguably unnecessary encounters.

Lastly, routine traffic stop tactics and protocols may also contribute to tragic outcomes. A traffic stop is almost always a single officer event; sometimes the officer logs out – sometimes not. The environment of a stop is inherently dangerous – in traffic, often compounded by the capricious unsafe vehicle placement of the driver being stopped.

Further, documentation of stops (e.g., warning, citation) may be inconsistent, confounding analysis to address safety and equity through data analysis.

Finding “fishing holes” or zealously enforcing trivial regulations often results in antagonism and alienation, with no appreciable positive effects in terms of improving life or traffic safety.

In the extreme, tragic encounters can turn on what criminologists describe as “officer-created jeopardy: When Officers regularly — and unnecessarily — place themselves in danger by standing in front of fleeing vehicles or reaching inside car windows, then fired their weapons in what they later said was self-defense. A mindset of elevated risk of traffic stops can result in mutual defensiveness and sense of danger. (In fact, because the police pull over so many cars and trucks — tens of millions each year — an officer’s chances of being killed at a vehicle stop are less than 1 in 3.6 million, excluding accidents, two studies have shown. At stops for common traffic infractions, the odds are as low as 1 in 6.5 million, according to a 2019 study by Jordan Blair Woods, a law professor at the University of Arkansas) [David D. Kirkpatrick, Steve Eder, Kim Barker and Julie Tate - New York Times, Oct. 31, 2021, Updated Nov. 30, 2021]

Concluding Comment

This dilemma is the foundation of both the Risk and SWOT analysis and Mission-Strategic Vision sections which follow.

Validating that baseline FHPD Policies, Procedures, and Training Programs meet or exceed model practices and baseline state and national accreditation standards.

Meeting the State of Michigan and National Law Enforcement Accreditation Standards is a benchmark in Winbourne's FHPD traffic safety and enforcement programs evaluation. At the state level, the Michigan Commission on Law Enforcement Standards (MCOLES) has statutory responsibilities for establishing mandatory minimum standards for law enforcement officers and civilian staff.

Additionally, the Michigan Association of Chiefs of Police (MACP) has established a comprehensive accreditation process founded upon compliance with the MCOLES policy, training, and operational standards for law enforcement. The Michigan Law Enforcement Accreditation Commission (MLEAC) oversees the evaluation of Michigan law enforcement agencies for accreditation purposes.

The national Commission on Accreditation for Law enforcement Agencies (CALEA) was established in 1978 to create national standards for law enforcement agencies. Accreditation through CALEA is voluntary. MCOLES and CALEA standards are similar.

Traffic Safety and Enforcement standards comprise five broad categories found in MCOLES Section 3.6: Conducting motor vehicle stops; Enforcement of violations; Enforcement options, specifically warnings; Uniform traffic issuance and arrest; Special classes of offenders; and Direction and control of traffic.

In the MCOLES reaccreditation on-site assessment report 2021, the assessment team found that the Farmington Hills Police Department meets standards established by MCOLES through written directives, comprehensive training, and operational protocols. For Section 3.6 - Traffic Enforcement, the assessment team found that FHPD "has applicable policies regarding traffic violation enforcement options; traffic direction and control; procedures to conduct motor vehicle stops, including high risk stops, and options to arrest, including warning citations."

CALEA standards are similar to MCOLES and warrant confirming FHPD compliance. However, we note that state and national accreditation standards represent more of a baseline than a body of policies and practices aimed at an agency ascending from good to great.

Winbourne's charge in preparing the three reports is to identify a path forward for FHPD that preserves the high standards they've worked hard to implement and continuously improve. We term this process as moving from the basic to the best. While Winbourne finds that policy and procedural training integrity based upon

accreditation benchmarks are met or exceeded by FHPD, there are certain areas for additional examination, as will be discussed later in this report. In particular, we will mention the following topics for review and improvement: Enforcement uniformity and transparency, the documentation of warnings instead of enforcement, and the rigorous use of data as the foundation for a life safety focus in traffic enforcement programs.

Winbourne finds policy, procedural, and training integrity with the benchmarks listed.

“The agency shall have a written directive establishing procedures for directed traffic enforcement based on a review of the jurisdiction’s crash data, requests for traffic enforcement, crime data or trends, school zones, and roadway conditions, which justify the need for directed enforcement in a specific location.” [MLEAC 5.15, CALEA 61 60 31.3.]

“The department appropriately warns, cites, or arrests traffic violators to gain compliance with traffic laws and to develop driver awareness of the causes of traffic accidents. Rather than ignore a minor violation, officers may stop the vehicle and call the violation to the driver’s attention. Officers have discretion to issue a traffic warning to urge caution and alertness in the operation of the vehicle.”

Moving from the basic to the best – Addressing risks and developing a model strategic traffic safety and enforcement program

Overview

The single most important element of a leading-edge traffic safety/traffic enforcement program is to base a strategic and tactical program on 1. Objectives focused on accidents, injuries, and inherently dangerous driving behaviors; and 2. Data and evidence allow for focused presence and enforcement to prevent and deter accidents and, risk of injuries, and inherently dangerous driving behaviors.

Why is this a defensible, desirable strategic focus?

Best practice case studies over the last 40+ years have been consistent in concluding that a strategic vision founded on illegal traffic acts **that imperil safety and result in injury or death** should govern traffic enforcement priorities: DUI; aggressive/reckless driving; excessive speed – especially in high consequence corridors, like school zones; disregard of red light and other crucial traffic control systems – primarily because of inattention, like texting; disregard of pedestrian safety, and the like should constitute the

overwhelming number of citations – and, where necessary, arrests – in the total annual enforcement picture for a police agency.

Everything else – except commercial vehicle inspection/enforcement (more on that to come) – should focus on warnings and education. The argument that traffic enforcement must be based on life safety, not revenue, or an unproven belief in deterrence through visibility is why this overarching strategic vision is defensible and desirable. Both of these false, problematic assumptions will be examined in detail below.

The studies described in the case statement (above) support this argument:

- The Fresno Study focused on selective enforcement of dangerous driving behaviors and their correlation to reducing collisions, injuries, and fatalities. Following a massive increase of dedicated traffic enforcement officers (from 20 to 84), citations/arrests focused upon dangerous driving, NOT equipment violations, non-critical traffic control rules, etc., positively affected life safety outcomes. **Over 80% of all enforcement actions were expressly focused upon dangerous driving behaviors listed above** DUI; aggressive/reckless driving; excessive speed – especially in high consequence corridors, like school zones; disregard of red lights and other crucial traffic control systems – primarily because of inattention, like texting; disregard of pedestrian safety, and the like.
 - On the one hand, the huge increase in dangerous driving prevention through enforcement – a 229% increase – correlated with a marked decrease in motor vehicle crashes, injury collisions, fatalities, and fatalities related to speed; no correlation was established between random enforcement of minor violations and infractions. The focus of this approach was unequivocally “injury prevention” as opposed to generalized, unfocused issuance of citations or NOIs.
- The NYPD in 2020 validated these conclusions through a kind of reverse mirroring of the Fresno Study: From March 12 to December 31, 2020 (remember the first wave of COVID?), the NYPD wrote 52.9% fewer tickets than it did during the same period in 2019. During that period, fatal crashes spiked 16%, resulting in 31 more traffic deaths compared with the previous year. In the first quarter of 2021, traffic enforcement was down 37.2% compared with the same period the previous year—and fatal crashes were up 9.7% compared with the previous year's first quarter. In 2019, the department wrote 747,343 tickets for moving violations. Of those, 90.4% were for “hazardous violations”—offenses such as speeding, texting, DUI, reckless/aggressive driving, and failing to yield to pedestrians. [NYPD Study 2022]

The important point here is that in both these studies, which supported the value of rigorous dangerous driving traffic enforcement and its favorable impacts on life safety –

there was no validation that *any traffic enforcement correlates to positive outcomes*. Equipment violations (not including commercial vehicle regulatory enforcement), non-dangerous regulatory infractions, etc., were not included in aggressive traffic safety enforcement initiatives.

Two fallacies about traffic enforcement buttress these outcomes and Winbourne's overarching recommendation:

First, there is a belief that regular, visible enforcement of **any** traffic violation creates a deterrent effect that positively reinforces safe driving behaviors through the threat of citation or apprehension. Plainly stated, no reliable study or best practice we (Winbourne) are familiar with validates this conclusion. Over 50 years ago, the often derided (but never disproven) Kansas City Preventive Patrol Experiment empirically established a commonly held conclusion in policing: *Random patrol and random enforcement yield random results*.

Now, even more than in the past, the notion that visible police presence and visible police action create a deterrent effect is a resounding fallacy. Why? Among many reasons, staffing levels and 911-driven patrol/dispatch patterns expose just how absent and thinly spread police proactive officers are, rather than being omnipresent, immediately responsive, and in a predictable position to arrest and enforce. It is self-evident that the ability of the police to intervene and enforce through broad deployment and occasional transitory visibility is more luck than purposeful predictability. And those with diminished respect for the law and the safety of others know it.

Second, enforcement of non-dangerous driving infractions has a higher probability of creating contempt rather than respect for the law and police. In the extreme, it creates unnecessary and elevated potential for confrontation, as discussed in the case statement (above). Little good comes from a citation or notice of infraction that will be viewed, not without reason, as simple revenue collecting. Educational – no. Respectful – no. Detering – no. As we will examine in more detail through the lens of Ferguson, Missouri (below), A strategic vision must be founded on targeting illegal traffic acts that *imperil safety and result in injury or death*. Everything else should predominantly focus on warning, education, and a commitment that public safety is not defined by or linked to the process of gathering cash.

A recent Police One survey (conducted from April 22, 2021, to May 4, 2021) directed to a total of 1,036 police officers or deputies who work in patrol illustrates the dilemma between serious and questionable traffic stop patterns:

“When asked about the most common reasons for a traffic stop, speeding was unsurprisingly the top choice at 43%. Interestingly, *equipment violations were second*

with 24% selecting it as their top reason for the stop. Suspicion of criminal activity, at 9%, was far lower...”

We note two important exceptions to the preceding analysis:

First, while random enforcement is mainly ineffective as either a deterrent or to buy down injury risk, *directed, concentrated enforcement programs* at locations of high risk and hazard are one of the few enforcement approaches of proven efficacy. One reason we will continue to examine is the nexus between data-informed locations of dangerous driving and drivers and focused aggressive enforcement at those “hotspots.” The idea that the problem drivers relocate to another location has been debunked mainly by research (see Weisburd et al. in Sources and Citations)

Second, the specialized enforcement capacity and authority of FHPD concerning commercial vehicle operation is a critical function. The insufficiency of police and other regulatory agents to help ensure safe commercial vehicle operation on US roadways is profound, and the danger posed to the public by an overloaded semi-tractor trailer is obvious. Suffice it to say, there is a big difference between brake or equipment failure on an 18-wheeler than a broken taillight on a mini-van, and FHPD’s ability and authority to augment agents to enforce equipment safety standards for commercial vehicles is eminently justified and important.

Traffic Safety Mission Statements

As discussed above, the first and most essential building block to create a model Traffic Safety program is to articulate a concise and universally applied mission statement while the overall statement of objectives and core values discussed in detail in the Phase 1 report constitute a model for progressive constitutional policing. Winbourne finds that spelling out the guiding principles and high-level objectives specifically as relates to traffic enforcement is both a best practice and the foundation for all training protocols, procedural guidelines, and accountability.

Here are two examples for consideration or adaptation. Both are versions we modified from existing language provided by the National Highway Traffic Safety Administration, the International Association of Chiefs of Police, and the Major Cities Police Chiefs Association:

1. *The primary purpose of the FHPD in enforcing traffic laws is to reduce crash-related injuries and fatalities. This entails an explicit, uniform enforcement program focused on collision-prone locations, drunk-driving arrests, and approach to safety education and outreach. We strive to ensure that officers enforce the*

right violations in the right places while focusing on the overarching goal of the department's traffic strategy: injury reduction on the roads.

- 2. FHPD is committed to uniformity and consistency in enforcement, with the goal of obtaining and promoting voluntary compliance. To this end, FHPD focuses on opportunities for qualitative enforcement – centered on prevention and education through warnings and non-punitive contacts – as preferable over quantitative enforcement whenever practical*

One element which merits consideration and inclusion in a mission statement is summarized by the term intelligence LED policing. To understand the potential value of including this is part of a mission statement Consider the following case study from Oakland CA:

In her 2019 book, *Biased: Uncovering the Hidden Prejudice That Shapes What We See, Think and Do*, Dr. Jennifer Eberhardt examined how implicit bias and detrimental outcomes of traffic enforcement may be overcome, managed, or mitigated with intelligence-led training.

“In Oakland, I and a number of my colleagues were able to help the police department to reduce the number of stops – including traffic stops - they were making of people who weren't committing any serious crimes or dangerous driving. That was the goal. And we did this by pushing officers to ask themselves a simple question before each stop they made: Is this stop intelligence-led? Yes or no. What they meant by intelligence-led was, did I have prior information to tie this specific person to a particular crime? Just adding that question to the form that officers complete during a stop slows them down. They're thinking, Why am I considering pulling this person over? We're trying to push them to use the objective standards that they have and tie people to specific crimes rather than Do I have an intuition about this person? It was kind of a change in mindset at the moment of action.

So in 2017, Oakland officers made about 32,000 stops across the city. And in 2018, with the addition of that question, they made about 19,000 stops.”

Therefore, absent intelligence or information connecting an individual to a crime or public safety concern, less attention, for example, should be given to observations of vehicle equipment violations where no strong causal connection to collisions, and hence public safety, exists. (This principle should not be extended to regulating commercial vehicle operation, which presents unique risk to public safety)

Findings in assessing FHPD's adherence to the principles articulated above

Discussion and adoption of a mission statement specific to traffic enforcement is an easy enhancement. Whether these proposed principles are operationalized in policy, procedures, and training remains open and will be the focus of most of the following analysis.

Organizational Guidelines

Organizational guidelines are the overarching policy parameters that are the foundation for specific protocols and procedures. Like the Mission Statement (discussed above), all FHPD policies, tactics, and procedural rules must be consistent with the following guidelines intended to mirror the Department's Mission.

In a review of the substantial body of academic and practical data on this topic, as well as multiple decades of experience, the Winbourne team has identified the essential elements of leading-edge public safety organizations. It has identified the building blocks of leading-edge Traffic Safety programs with remarkable consistency, to include CALEA and other state and local accreditation standards.

The following list is adapted from numerous sources, most notably from IACP:

GOAL: To establish guidelines for the uniform enforcement of traffic laws within the jurisdiction, consistent with and agencies mission vision and core values. Toward that end, every department will develop a written directive to support the concept of uniform enforcement action for traffic law violations.

HIGH-LEVEL GUIDELINES:

A. The department will create a process to ensure that traffic officers apply uniform traffic enforcement action for similar traffic violations. The directive will define the circumstances that may prompt a physical arrest, a citation, or a warning, depending on the nature of the violation and the applicable statutes.

B. The department will encourage officers to take the appropriate type of enforcement action available whenever the situation warrants. Officers will draw upon their training, experience, and common sense when deciding the appropriate course of action.

C. The department will encourage the uniform and consistent enforcement of traffic laws to promote voluntary compliance with traffic laws and to avoid strictly quantitative enforcement action. Qualitative enforcement action should be

continuously stressed, and center the value of prevention and education, which is best achieved through warnings.

Winbourne notes that the essential elements of these guidelines are *uniformity and consistency in enforcement*, aiming to obtain and promote voluntary compliance. In addition, the emphasis on *qualitative* enforcement – centered on prevention and education through warnings and non-punitive contacts – should be prioritized whenever practical over *quantitative* enforcement.

Findings in assessing FHPD's adherence to the principles articulated above

As will be examined in both the data and policy/procedural sections of this report, the kind of uniformity and consistency that are the foundation of model practices needs improvement; moreover, the priority of qualitative (rather than quantitative) enforcement priorities, strategies, and tactics remains problematic. As discussed at numerous points in this report, life safety priorities must be the foundation of any model traffic enforcement program, noting once again that random enforcement yields random results. More significantly, in post-Ferguson law enforcement, the revenue implications of issuing traffic citations must be unequivocally divorced from any agency's Traffic Safety mission and program.

Enforcement Integrity and Uniformity

The Lessons from Ferguson, MO

We must state clearly at the outset there is virtually no credible comparison between the 2023 Farmington Hills Police Department and the 2014 Ferguson, MO, Police Department. In a real sense, these two departments should reasonably be assessed as opposites, particularly regarding professionalism, expertise, and commitment to constitutional policing. Nevertheless, the conclusions the Department of Justice reached in evaluating the 2014 Michael Brown officer-involved shooting demand study and consideration, particularly in light of the previous discussion how about clarity of mission and unity of effort. More to the point, about the consequences of placing any level of priority on the revenue gathering than traffic safety.

Ferguson was a cautionary tale, although admittedly an extreme example of what happens to community trust and police legitimacy when public safety is either perceived to be, or is, in fact, subordinate to, balancing a budget or harvesting revenue. As discussed above, the single most effective way to defeat the argument of randomly enforcing traffic laws for revenue, is to meet a police and community standard based on the priority of life safety.

While specific to Ferguson PD, the following recommendations are included in this report as a set of benchmarks. They are synthesized from the DOJ report and other analytical sources in the appendix.

- *Prohibit the use of ticketing and arrest quotas, whether formal or informal*
- *Require that officers report in writing all stops, searches, and arrests, including pedestrian stops, and that their reports articulate the legal authority for the law enforcement action and sufficient description of facts to support that authority*
- *Require documented supervisory approval before 1) Issuing any citation/summons that includes more than two charges; 2) Making an arrest on any of the following charges: i. Failure to Comply/Obey; ii. Resisting Arrest; iii. Disorderly Conduct/Disturbing the Peace; iv. Obstruction of Government Operations; 3) Arresting or ticketing an individual who sought police aid, or who is cooperating with police in an investigation; 4) Arresting on a municipal warrant.*

Resident vs. Non-resident citations

The last procedural example that Winbourne considers inconsistent with the overarching organizational priority of uniform enforcement is the statistical inconsistency between citations issued to residents and nonresidents. While, again, there are a number of reasons to adopt this approach, the status of being a resident, versus a non-resident, is a questionable and problematic criterion for unequal treatment in traffic enforcement or any other police enforcement arena. This disparity will be examined in greater detail in the data analysis section of this report.

Finding in assessing FHPD's adherence to the principle articulated above

Winbourne recommends re-thinking and revising this practice on grounds of the uniform and equitable enforcement requirement.

Strategic, Data and GIS Driven Planning and Tactical Traffic Safety Deployments

The findings above concerning using uniform, consistent, reliable data are the foundation of this critical subsection. It bears repeating that *random enforcement yields random results*. Generalized traffic enforcement is not a strategy: There is an unfortunate tendency to treat traffic enforcement as a monolith, which ignores the truth that not all traffic stops are created equal or are of equal value.

As a matter of best practice, traffic enforcement goals, procedures, strategies, and tactics are among the most intensive, data-reliant missions in high-capability police organizations. A number of fundamental capabilities are required to integrate a life safety traffic enforcement program, discussed throughout this report. Two in particular are considered foundational:

First, a robust capacity to obtain data in real-time and in granular detail to establish criteria to guide deployments, obtain a reliable picture of traffic risk, and facilitate strategic planning, particularly to meet the overarching priority of saving lives.

Second, the capacity to rigorously analyze the data. This capacity represents the data dependent, strategic planning element of a high order Traffic Safety program. This analytical capacity is made up of numerous components: GIS analytics (for example, heat mapping); the ability to correlate enforcement patterns/outcomes with injuries, accidents, complaints of dangerous driving, and other relevant criteria; engineering and crash data, and other information sets discussed in detail throughout this report.

This two-part standard is universal. Both the State (MCOLES) and CALEA include almost identical language:

“The agency shall have a written directive establishing procedures for directed traffic enforcement based on a review of the jurisdiction’s crash data, requests for traffic enforcement, crime data or trends, school zones, and roadway conditions, which justify the need for directed enforcement in a specific location.” [MLEAC 5.15, CALEA 61 60 31.3.]

The basic elements of any robust system of data, GIS, and other information sources to inform a comprehensive Traffic Safety strategic and tactical plan have been consistently articulated by the National Highway Traffic Safety Administration (NHTSA) in collaboration with the IACP in their regularly revised and updated Manual of Police Traffic Services Policies and Procedures.

- *“POLICY The department will develop—or participate in—a traffic records system to capture and store vital traffic records for agency use. Moreover, the department will cooperate to the extent possible with its counterparts throughout the jurisdiction in promoting the development of standardized collection instruments, procedures, and reporting guidelines to enhance the effectiveness and efficiency of the data collected.*

- **PROCEDURES**
 - *The department will use a precise traffic records management system to gather and maintain timely data on traffic crashes, traffic enforcement, roadway hazards, traffic analytical reports, and other traffic information the agency deems necessary.*
 - *The traffic records system will be a reliable resource of traffic-related information, aiding traffic patrol personnel in the performance of their duties and assisting police executives in their critical decision-making roles.*
 - *The traffic records system will detail the method by which records are received, processed, stored, and disseminated within the agency and to the public. The security of the files will be taken into account.*
 - *The traffic records system will contain specific and compatible location information both on crashes and on citation issuance to enable the department to analyze the data for informed and expeditious decisions involving traffic enforcement.*
 - *As part of the traffic records system plan, regular reviews will be conducted, and a record retention and archive schedule will be in place.”*

NHTSA outlines this data-based focused approach in their Selective Traffic Enforcement Program, which may be found in Appendix Three.

Findings in assessing FHPD's adherence to the principles articulated above

Data access insufficiencies and lack of capacity concerning analytical tools present a challenge. Problem-solving these shortfalls must remain a high priority, and we encourage continuous vigilance in creating and maintaining a data-driven life safety-based program and to further commit to and systematize the highest reasonable level of transparency and information sharing with community and elected leaders.

By way of summarizing this important sub-section, a recent article published by Winbourne provides some relevant commentary:

“... a critical way to exploit the value of data to inform best practices is describable as an “information access audit.” In other words, does necessary, relevant data - if even available - have to be painstakingly retrieved, researched and created from nothing or, preferably, has the data been pre-programmed to be immediately accessible and available for study and analysis.”

To meet this need, RFPs, CAD-RMS projects, and contractor requirements need to include explicit provisions for data retrieval according to the priorities of the public safety entity, rather than the vendor.

Relatedly, geospatial (GIS) and temporal coordinates and correlations are among the consistent data-based characteristics which inhere in every important human event or activity. For police, this includes criminal acts, victimization, the trajectories which bring people into and out of incident locations, and the presence or absence of people to observe or deter a crime. The “policing of place” - utilizing evidence-based approaches - is as important to the history of the policing profession as any of our precedent, pragmatic public safety “revolutions”, from the Basic Car Plan to the Professional Model to the SARA model.”

Warnings: Data requirements, strategic priority, and empowering and supporting officer discretion

In the Phase 1 report, emphasis was placed on the value of setting forth clear definitions – and encouragement – for the prudent use of discretion:

“Recommendation 4.1 *The effective use of officer discretion builds community trust, improving safety and efficacy. We recommend scenario-based training using discretion as an element in the comprehensive annual training programs. Current theories about police discretion in the main posit that discretion is to be exercised according to a framework or parameters. However, oftentimes, these policy provisos are unclear or not to be found. Police discretion holds the key to a lot of potentially favorable police engagements. It is an area of focus, informed by data and community input, which would be a worthwhile investment of time and care”*

Throughout this report, Winbourne has taken a strong position about the priority of linking Traffic Enforcement to Life Safety, which inevitably moves the dial toward prevention and education. Admittedly, some dangerous drivers don't want to listen or learn, and there is no argument that the consequence of causing risk to innocent people should be as severe as the law allows. An in-depth statistical analysis will reveal these

driving patterns and the criminals who create this risk. Still, in the main, many, if not most, drivers will benefit and positively reflect upon a warning versus a citation. The need to emphasize and provide guidelines for discretion in issuing warnings, versus a citation or notice of infraction, is characteristic of all the high-capability police organizations we are familiar with.

This position has a clear mandate to consistently and comprehensively record stops that end in a warning and the circumstances of the initial stop, demographic, and other critical information. Winbourne has identified some shortfalls in this important data and analysis category, which FHPD has worked hard to correct.

A model policy statement that is embedded in FHPD values and training is worth repeating:

“The department appropriately warns, cites, or arrests traffic violators to gain compliance with traffic laws and to develop driver awareness of the causes of traffic accidents. Rather than ignore a minor violation, officers may stop the vehicle and call the violation to the driver’s attention. Officers have discretion to issue a traffic warning to urge caution and alertness in the operation of the vehicle.”

Findings in assessing FHPD's adherence to the principles articulated above

Discussion and adoption of a mission statement specific to traffic enforcement is an easy enhancement. Whether these proposed principles are operationalized in policy, procedures, and training remains open and will be the focus of most of the following analysis.

Pretext stops

Among the most significant emerging policies, particularly in high-capability police organizations, are the explicit parameters governing pretext stops, whether in specific jurisdictional policy or as a matter of State Law (as in California). Parameters and responsibilities for police personnel involving pretextual stops will continue to face extreme scrutiny in court on 4th and 14th Amendment grounds and in the court of public trust.

In evaluating California's Racial and Identity Profiling Act (RIPA), the RAND Corporation offered the following analysis:

“The first consideration is to capture the data about traffic stops in your jurisdiction and then act on that information. California's Racial & Identity Profiling

Act (RIPA) reporting requirements will soon apply to every traffic stop in that state. Other states and likely federal action may create a similar mandate for all. As it does, policymakers can base subsequent legislation on data instead of supposition. Agencies could also seek to match the appearance and perceived identity of persons stopped for suspected criminal conduct against the appearance of persons the public reports as being responsible for criminal conduct, another way to assess the frequency of stops for that purpose against the actual crime data in that community.”

The requirement for clear policy language governing pretext stops is obvious. While the Supreme Court allows for limited, specifically defined “pre-textual stops” in certain situations, there remains significant concern regarding the manner and frequency with which officers are stopping individuals (pedestrians, cyclists, and motorists) for perceived minor violations to investigate other crimes. Their fears largely stem from a belief that such enforcement activities are arbitrary, capricious, and a reflection of an individual officer's implicit or explicit bias(es). Moreover, some community members question how such pretextual stops affect crime reduction.

Finding in assessing FHPD's adherence to the principles articulated above

Winbourne recommends a comprehensive examination – to include community, peer agencies, legal advisors, and city leadership on the model policies, procedures, and training/accountability programs governing so-called “pretext stops” and adapting FHPD policies and procedures accordingly. A sample policy is attached in the appendix.

Community Engagement

In the Phase 1 Report, Winbourne found FHPD to have commendable outreach, transparency, and community engagement programs. In Winbourne's experience, police department engagement with the community on the topics found in this report is uncommon, if not rare. To capitalize on FHPD's relationship with the Farmington Hills community, creating opportunities and agendas to talk about traffic enforcement priorities and community concerns related to traffic safety, the findings and outcomes of traffic-related data analysis, and the evidence-based nexus to more serious crime would be a leading edge initiative in public safety, and well within the capabilities and values of FHPD.

“This proposition finds support in recent surveys conducted by Lexis-Nexis Police One: This survey finding validates officer and public engagement to discern levels of local support for the continuance (or alteration) of police responsibilities for traffic enforcement. These conversations can address both an officer's reluctance to stop vehicles as well as issues of racial profiling, procedural justice, and the concerns of many who want to lower the frequency of police-involved shootings of Black and Latino men. One core issue to resolve in this work is addressing the racial disparities found in the pretext and traffic stops for minor violations. The police should consider how they want to participate in that dialog and whether ending some traffic stop practices is something they want to support.”

In the Phase 1 report, a key recommendation describes the value of regular, systematic community assessment surveys. While they don't replace in-person dialogue, surveys can provide a credible overview of community values and priorities- including traffic enforcement and safety. That recommendation is reiterated here:

“R.2.5 The systematic use of citizen assessment surveys is widely utilized by police agencies and governments throughout the US. The benefits of regular public safety assessments via well-designed community survey instruments contribute to situational awareness and provide insight into community priorities. Numerous models of objective and productive community survey instruments are available to FHPD, which the Winbourne Team will gladly assist with as a separate project. Model citizen assessment surveys are specific to the jurisdictional police agencies. While some approaches attempt to integrate more general assessment instruments (city-county general government, fire, and emergency management surveys, for example), a law enforcement-specific survey instrument is generally most effective and informative”

Finding in assessing FHPD's adherence to the principle articulated above

Winbourne finds a FHPD is a model agency in their community engagement and outreach programs. Expanding the topics about Traffic Safety and Enforcement included in community meetings and discussions have been a focal point of this Phase 2 report, and – along with the Phase 1 report observations on the desirability of community surveys and a dedicated FHPD social media platform – will be proposed in the Recommendation section (below)

Subject/suspect non-compliance training, awareness, and procedures

An element that permeates every strategic analysis of police traffic stops is non-compliance. More than any other factor, non-compliance (resistance) is central to both officer and violator safety and dominates the debate about traffic stops, social justice, and race-based jeopardy.

In another survey conducted by Lexis-Nexis Police One for RAND [1020 officers from 72 different police jurisdictions reporting from 2019 to 2021], the findings were striking, although somewhat inconsistent:

“513 respondents noted that driver cooperation had remained about the same, 504 said it had worsened. The highest levels of noncompliance were those suspected of drug or alcohol influence and persons suspected of criminal conduct.

Almost 80% said their agency had not altered their traffic stop policies to address driver noncompliance.

Only 6% strongly agree that they have received adequate training for traffic stops involving noncompliant drivers; 46% disagree or strongly disagree. At the same time, more than 83% agree or strongly agree that they are confident in handling such an instance.”

RAND concluded, *“Officers feel they are ready to handle such a circumstance; this presumption may be in error and not discovered until it is too late. Agencies could develop or enhance existing training to deal with opportunities to improve the ways officers manage a non-compliant driver, especially if it progresses to a stage where the person will be taken into custody. Trained skills in voice command, arrest, and control techniques that do not involve weapons and de-escalation skills could be taught and tested until they can be employed without delay. Finally, we argue that these challenges are tied to separate strategic needs – traffic enforcement to reduce harm, and non-compliance must be treated as separate priorities (not lumped together)*

[One final survey result of significance: *“Eighty-seven percent said they do not support proposals that reduce the role of the police to enforce traffic laws”*]

Findings in assessing FHPD's adherence to the principles articulated above

Winbourne finds policy, procedural, and training integrity with the benchmarks listed.

The Phase 1 report concluded that the FHPD training program was thorough, particularly concerning non-compliant resistive subjects. Comprehensive analysis of data, as well as a systematic review of body-worn camera video consistent with department policy, will further illuminate and refine both the policy and training implications.

Monitoring and Measuring Implicit Bias In Traffic Stops

The FHPD program addressing implicit bias was evaluated at length in Phase 1. In conversations with the authors of the nationally recognized implicit bias awareness program used by FHPD, and the majority of police agencies around the nation, Dr. Lorrie Fridell and Anna Laszlo concurred the training must be supported by systematized monitoring and oversight. The following reflects their conclusions.

While there have been concerted efforts in some jurisdictions to measure biased policing, these measurements were based mainly on vehicle stop data collection, which is limited by being impersonal and inferential. Some agencies, however, have extended measurement efforts to correlate the use of force, police contacts with the public, and other police activities. *“The collection and reporting of data that includes subject demographics can be an important part of an agency’s efforts to promote fair and bias-free policing and how these efforts are perceived by the community. A data collection program can convey to the community a commitment to fair and bias-free policing, accountability, and transparency.”*

However, these efforts must be undertaken with an understanding on the part of law enforcement and community members regarding what this data can and cannot reveal about bias in policing. While it is not difficult to show disparity where one group is disproportionately represented among people who are stopped in their vehicles or detained on the street, the real challenge for data analysts is determining the causes or sources of that disparity. *“Concluding that “disparity” is the same as “bias” is not correct, as some portion of the identified disparity may be produced, in part, by other legitimate factors.”* Although measuring biased policing is not a simple task, such efforts

can help assess the efficacy of programmatic efforts and determine if changes are needed to training, policy, and procedures.

One option that appears to be underutilized by FHPD is the systematic review of body-worn camera footage for enforcement actions or stops that don't result in a complaint. The authority for this kind of review appears to be permitted within the FHPD body worn camera policy, specifically under the training proviso:

“A supervisor may review/audit a BWC/ICC video(s) at any time necessary for legitimate departmental and criminal justice purposes. Reasons include in the interest of improving officer safety and performance, investigating citizen complaints, any use-of-force incident and any foot or vehicle pursuit. Training personnel may review BWC/ICC video(s) to research and develop departmental training. BWC video(s) that may be presented as training video(s) to department personnel shall have the approval of the Training Section’s chain of command [Winbourne emphasis added].”

The key to systematizing this kind of leading-edge commitment to accountability and improvement is to ensure that it is non-punitive and instead collaborative, instructional, and squarely in the realm of improvement, individual and departmental. This concept will be dealt with in more detail in the recommendations section of this report.

The benchmarks for this kind of non-disciplinary, neutral, and objective approach are found in the procedural justice doctrine which was a central recommendation in the Phase 1 report:

Phase 1 – Recommendation 4.2: *We recommend evaluation and possible integration of Procedural Justice protocols. A significant finding of the President’s Commission on 21st Century Policing, Procedural Justice policies training and protocols speaks to people’s perceptions of the fairness of a process, like law enforcement, not necessarily the outcome of the process. It is well documented that perceptions of fairness are strongly influenced by the quality of the experience, not just the result. There have been several applications of Procedural Justice models, supported by DOJ, IACP and the Police Executive Research Forum (PERF), among others. The cornerstone of Procedural Justice theory is a consistent process applied equally to everyone. The theory rests on four pillars:*

- *Treat everyone with dignity and respect*
- *Be as neutral and unbiased as possible.*
- *Give people a voice by listening to their side of the story.*
- *Convey a sense that decisions are based on trustworthy motives”*

In that (Phase 1) report, we noted that “the Winbourne team strongly believes that FHPD is using Procedural Justice principles but would do well to audit their current programs for consistency in curricula, naming conventions, and other more explicit alignments to Procedural Justice doctrines and practices”.

Finally, as will be elaborated in the recommendations section, the use of the Guardian Tracking System should include explicit categories within personnel early warning policies dealing with documented training or correctives related to implicit bias.

“The Farmington Hills Police Department uses the Guardian Tracking software package in part to document uses of force incidents, discipline, counseling, and other corrective or complimentary action. The “Personnel Early Warning System” will assist supervisory personnel in monitoring employee performance. It will give warning to a supervisor when a subordinate records a set number of incidents in any chosen category. The supervisor will be required to respond to the warning by conducting an “Early Intervention Review.” [General Order - Guardian Tracking DOI 4-16-2020] “

Findings in assessing FHPD's adherence to the principles articulated above

Winbourne finds policy, procedural, and training integrity with the benchmarks listed. The Phase 1 report concluded that FHPD policies, procedures, and – in particular – training and accountability programs on implicit bias and fair and impartial policing were thorough and comprehensive.

Analysis of data, as well as a random, regular, and systematic review of body-worn camera video, consistent with department policy, will further illuminate and refine both the policy and training implications of the findings listed above.

Commentary on Automated License Plate Reader Enforcement

Implementing a robust ALPR system in Farmington Hills, permitted by State Law, is both an opportunity and a potential risk. Enhanced enforcement options through ALPR technology are a clear opportunity, both to identify serious offenders and provide some level of deterrence (noting that deterrence is a sometimes fickle and unreliable phenomenon). On the downside, if ALPR is not deployed with scrupulous neutrality and strategic necessity, the potential for both public objection and civil legal action are real risks. At an operational level, the potential for an increase in high-risk apprehension scenarios – particularly high-speed pursuits – will require clearly stated rules of engagement and – as noted elsewhere in the report – a reassessment of FHPD vehicle pursuit and emergency vehicle operations policies and training.

FHPD advised they completed comprehensive research to purchase the FLOCK ALPR system and develop ALPR system policies, procedures and training protocols. FLOCK system anti-bias features and audit trail capabilities have been incorporated into the FHPD plan. The final ALPR plan and policy was vetted and approved by City legal counsel.

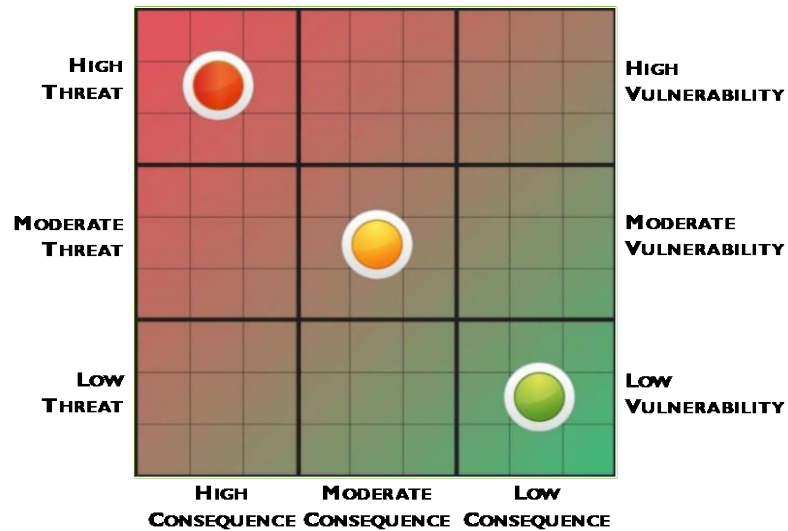
At this time, 28 of 40 total cameras have been installed and the FHPD remains in the “trial period” pending full implementation.

RISK INFORMED CONSOLIDATED RECOMMENDATIONS

A third critical building block in creating a Traffic Safety Mission Statement was previously examined in the Phase 1 Report:

Recommendation 2.2: We urge FHPD to continue its process of assessing risk and expand risk assessment models to all department operations. The upcoming Phase 2 and Phase 3 studies focused on traffic stops and arrests, should further this objective. System-wide, top-to-bottom risk assessment regularly invariably pays dividends, particularly to high-reliability organizations like FHPD.

A focus on situational awareness is the foundation for identifying risk. Risk modeling provides a roadmap for policy and training priorities and specific programs to buy down risk. There are standard models for risk analysis, and each requires the kind of situational realities that have been consequential to police officers, their organizations, and the communities they serve. Three of the most widely accepted formularies for risk are the Gordon Graham Model, the THIRA (Threat Hazard Identification and Risk Assessment) template from FEMA, and the Threat-Vulnerability-Consequence (TVC) model used in the public and private sectors. The TVC model, for example, is:



Fundamentally, high capability organizations are distinguished by being able to authoritatively answer the question “This is *why* – FHPD, for example, prioritizes enforcement through citation of these traffic safety scenarios” (e.g., high risk, inherently dangerous violations), which is one element of a triad that includes a focus on prioritizing “education,” e.g., warnings, and “engineering,” which may include temporary or permanent changes in routing/road design and technology such as automated enforcement.

APPENDICES

APPENDIX ONE: SOURCES AND CITATIONS

A. FHPD document production

- Sample Shift Summary Reports (Oct-Nov. 2022)
- Sample Major Incident Summaries
- FHPD General Order - Force Response to Resistance and Violence G.O.
Uniform and Dress Standards for Department Personnel G.O.
- FHPD General Order – Firearms Certification
- FHPD General Order - Guardian Tracking – Command Responsibilities
- FHPD General Order – Use of Force: Michigan Compiled Law 780.972 and
Graham v Connor 490 U.S. 386 (1989)
- FHPD General Order - Integrated Body-Worn and In-Car Camera System
- FHPD General Order - Command Responsibility
- 2022 National Commission on Accreditation of Law Enforcement Agencies
(CALEA) Standards Manual
- FHPD Dispatch Summaries (2019-2022 to date)
- Telecommunicator Training Program Manual – State of Michigan Dispatcher
Training Requirements Manual
- Agreement Between City of Farmington Hills, Michigan, and The Michigan
Fraternal Order of Police Labor Council - Effective July 1, 2017, to June 30,
2022
- 2019-2022 Spring and Fall Training Program Curricula via memoranda
- Technology used to document and track use of force – summary
memorandum and Policies
- FHPD Sample Blank and Completed Use of Force Reporting Forms
- FHPD Manual of Regulations
- FHPD FTO Training Manual
- FHPD Fair and Impartial Policing Curriculum and Power point
- FHPD Implicit Bias in Policing Curriculum and Power point

- FHPD Training Staff Qualifications/resumes (via memorandum)
- Michigan Law Enforcement Accreditation Process and Standards Manual
- Michigan Commission on Law Enforcement Standards Licensing Standards Manual
- 2019-2021 FHPD Complaint/Misconduct Investigation Summaries
- 2019-2021 FHPD Use of Force Investigation Summaries
- 2019-2021 FHPD Officer and Suspect/subject Injury Summaries
- Official Training/Certification Records of FHPD Training Staff (incl. Firearms and Defensive Tactics instructors (total of 30 records)
- FHPD General Order “Alternative Traffic Violations” [Replaces G.O. 17-007 “Impeded Traffic Violations] 11-10-2023

B. External Studies and Sources

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- National Consensus Policy on Use of Force, re: Traffic Stops and Enforcement, DOJ pub. 2020 [This report is a collaborative effort among 11 of the most significant law enforcement leadership and labor organizations in the United States The policy reflects the best thinking of all consensus organizations and is solely intended to serve as a template for law enforcement agencies to compare and enhance their existing policies]
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Appendix Two: Factors that Inhibit a Comparison of Police Department Statistical Information

Comparing FHPD policy, procedure and statistical information to another police department is complicated and will typically not provide accurate information for which a definitive or actionable statement can be made.

The primary requirement to make a definitive or actionable statement from a police department comparison assessment is to ensure the data from all departments involved in the assessment is the same regarding timeliness, accuracy, relevancy, comprehensive and objectiveness. For the most part, police departments do not collect data in a standardized, scientific, and systematic manner. Additionally, it is not easy to validate police department data reliability so it is difficult to state with confidence an “apples-to-apples” comparison has been completed.

Many factors inhibit the ability to complete a police department data comparison assessment that will result in actionable information. For example:

- Information may be segmented and does not show the complete and accurate picture.
- Crime statistics are socially constructed as society decides what is criminal therefore data collected by one community may be different from another community.
- Statistical information may be based on operational definitions, resulting in disagreement regarding the definition's accuracy and relevancy.
- Police departments may engage in administrative practices which result in statistics that are partial to their perspective of criminal activity.
- Counting rules used by police departments to categorize crimes change overtime so it is difficult to draw comparisons of crimes between different time periods

The following may be different for the jurisdictions/departments involved in the comparison assessment:

1. Jurisdiction strategic objectives
2. Police department strategic objectives
3. Jurisdiction population and demographics
4. Jurisdiction and police department political and organization structure
5. Jurisdiction and police department funding

6. Police department staffing
7. Allocation and deployment of police staff
8. Community and police department initiatives
9. Police department incorporation of public safety industry best practices, standards, and guidelines
10. Police department policies, procedures, and training curriculum
11. Police department technology systems
12. Types of data collected by the police department
13. Ability to generate reports and statistical analysis
14. Quality of police department data
15. Types and volume of crime, quality of life issues and traffic issues affecting the jurisdiction
16. Proximity to locations that impact types and volume of crime, quality of life issues and traffic issues affecting the jurisdiction

Additionally, many jurisdictions will not provide the comprehensive data needed to complete the comparison assessment for a variety of reasons including:

1. The jurisdiction does not want to participate in the comparison assessment.
2. The jurisdiction does not have the technology and business intelligence/analytics systems to produce the required data.
3. The jurisdiction does not collect the type of data needed to complete a comparison assessment.
4. The jurisdiction does not have personnel with the required knowledge, skills, and abilities to generate the required data.
5. The jurisdiction does not have personnel with the required bandwidth to generate the required data.
6. The jurisdiction does not have timely or accurate data.

Finally, the department requesting the data has no control over when departments receiving the request will deliver the data. This makes it very difficult to establish an actionable project plan and schedule.

The result is the level of effort to obtain other department data; evaluate the quality of the data; understand the data for relevancy and usability; and then defend any findings

and recommendations that are based on the data; is so time, cost, and labor intensive, there is no positive return on investment to complete this type of comparison assessment.

The most actionable comparison assessment process is to compare FHPD information against public safety industry best practices, standards, and guidelines.

APPENDIX THREE: STRATEGIC INITIATIVE – STEP

FROM: National Highway Traffic Safety Administration (NHTSA) Manual of Police Traffic Services Policies and Procedures, in collaboration with IACP, last revision 2022 www.nhtsa.gov

PURPOSE The purpose of this policy is to establish guidelines to manage properly the department's traffic patrol resources to assure the safe, orderly flow of traffic and to discourage criminal activity within the jurisdiction.

POLICY The department will design a detailed Selective Traffic Enforcement Program (STEP) for the assignment of traffic personnel to prevent traffic crashes, to reduce traffic violations, and to deter criminal behavior. III. DEFINITIONS Selective Traffic Enforcement Program (STEP): a systematic process of deploying sworn officers to meet public needs based on the analysis of traffic crash data, traffic enforcement activities, and criminal offense patterns within the community. IV. PROCEDURE A. In order to implement a valid STEP, the department will annually compile and analyze all traffic crash data, roadway traffic volume and conditions, traffic enforcement statistics, and criminal activity patterns. B. To achieve the benefits of a STEP, all officers will be adequately trained in the aspects of selective enforcement. Supervisors will consider available relevant data in making appropriate patrol assignments to reduce ultimately traffic crashes, to diminish traffic violations, and to prevent criminal activity. C. Traffic enforcement officers will be deployed in a manner consistent with the department's STEP. The enforcement effort will be routinely directed toward the times and locations where traffic crashes, violations, and criminal acts take place. D. The department will annually evaluate STEP. All information concerning enforcement, crashes, and criminal activity will be considered in determining whether or not Manual of Police Traffic Services Policies and Procedures Page 1 of 2 program modifications are required to address new circumstances or changing conditions. E. Supervisors who are responsible for the operation of STEP will be provided with the informational reports and data analysis. The department personnel who are assigned to conduct the compilation and analysis of data will be educated and experienced in statistical analysis.

PROCEDURES A. Training. Officers will receive initial and ongoing training in conducting professional traffic stops, emphasizing the need to respect the rights of all persons to be treated equally and to be free from unreasonable searches and seizures. The content of these programs will include officer safety; courtesy; cultural awareness, and language barriers; arrest, search, seizure, and other constitutional issues; and interpersonal communications skills. Manual of Police Traffic Services Policies and Procedures

APPENDIX FOUR – EXAMPLE OF PRETEXT STOP POLICY

LOS ANGELES POLICE DEPARTMENT MANUAL
VOLUME I
Established by Special Order No. . 2022

240. 06 POLICY - LIMITATION ON USE OF PRETEXTUAL STOPS

PREAMBLE. While the exercise of an officer's discretion in initiating a "stop" or conducting a detention is authorized under the law, it should reflect the necessary balance of the role of law enforcement in the prevention of crime and receiving and thereafter maintaining the community's trust that the officer's actions are fair and without bias. Conducting a vehicle or pedestrian stop and/or detention can promote public safety and the protection of the public from serious and sometimes violent crime. Such stops can also subject motorists and pedestrians to inconvenience, confusion, and anxiety, and strain relationships between law enforcement and the community because some members of the community perceive stops as biased, racially motivated, or unfair. To maintain public trust, the Department's use of pretext stops as a crime reduction strategy must be measured, in furtherance of achieving the necessary balance between the perception of fairness and identifying those engaged in serious criminal conduct.

Pretext Stops Defined. A pretextual or pretext stop is one where officers use reasonable suspicion or probable cause of a minor traffic or code violation (e.g., Municipal Code or Health and Safety Code) as a pretext to investigate another, more serious crime that is unrelated to that violation.

Policy.

Use of Traffic/Pedestrian Stops - General. Traffic or pedestrian stops made for the sole purpose of enforcing the Vehicle Code or other codes are intended to **protect public safety**. Therefore, officers should make stops for minor equipment violations or other infractions only when the officer believes that such a violation or infraction significantly interferes with public safety.

Note: The public safety reason for all traffic/pedestrian stops, citations and warnings should be articulated on body-worn video (BWV) and should include an officer's response to any questions posed by the individual stopped.

Pretext Stops - Restricted. It is the Department's policy that pretextual stops shall not be conducted **unless** officers are acting upon articulable information in addition to the traffic violation, which may or may not amount to reasonable suspicion, regarding a serious crime (i.e., a crime with potential for great bodily injury or death), such as a Part I violent crime, driving under the influence (DUI), reckless driving, street racing, street takeovers, hit and run, human or narcotics trafficking, gun violence, burglary, or another similarly serious crime. Such decisions should not be based on a mere hunch or on generalized characteristics such as a person's race, gender, age, homeless circumstance, or presence in a high-crime location.

PURPOSE: The purpose of this Order is to establish Department Manual Section 1/240.06, *Policy - Limitation on Use of Pretextual Stops*. The policy provides parameters and responsibilities for Department personnel when utilizing pretextual stops so that they remain in compliance with the 4th and 14th Amendments to the United States Constitution.

PROCEDURE:

- I. **POLICY - LIMITATION ON USE OF PRETEXTUAL STOPS- ESTABLISHED.** Department Manual Section 1/240.06, *Policy - Limitation on Use of Pretextual Stops*, has been established and is attached.

AMENDMENTS: This Order adds Section 1/240.06 to the Department Manual.

AUDIT RESPONSIBILITY: The Commanding Officer, Audit Division, shall review this directive and determine whether an audit or inspection shall be conducted in accordance with Department Manual Section 0/080.30.

If you have any questions, you may contact the Office of Constitutional Policing and Policy, at (213) 486-8730.


MICHELE R. MOORE

Chief of Police

AGENDA
CITY COUNCIL MEETING
JANUARY 8, 2024 – 7:30PM
CITY OF FARMINGTON HILLS
31555 W ELEVEN MILE ROAD
FARMINGTON HILLS, MICHIGAN
Telephone: 248-871-2410 Website: www.fhgov.com
Cable TV: Spectrum – Channel 203; AT&T – Channel 99
YouTube Channel: <https://www.youtube.com/user/FHChannel8>

REQUESTS TO SPEAK: Anyone requesting to speak before Council on any agenda item other than an advertised public hearing issue must complete and turn in to the City Clerk a blue, Public Participation Registration Form (located in the wall rack by the south door entering the council chambers).

REGULAR SESSION MEETING BEGINS AT 7:30P.M. IN THE CITY COUNCIL CHAMBER

STUDY SESSION (5:30P.M. Community Room– See Separate Agenda)

REGULAR SESSION MEETING

CALL REGULAR SESSION MEETING TO ORDER

PLEDGE OF ALLEGIANCE

ROLL CALL

1. Approval of regular session meeting agenda
2. Administration of Oath to Mayor Pro-Tem Michael Bridges
3. Proclamation Recognizing [Vicki Barnett](#) for her service to the citizens of Farmington Hills
4. Proclamation Recognizing [Mary Newlin](#) for her service to the citizens of Farmington Hills
5. Proclamation Recognizing [Ken Massey](#) for his service to the citizens of Farmington Hills

CORRESPONDENCE

CONSENT AGENDA - (See Items No. 9 - 15)

All items listed under Consent Agenda are considered routine, administrative, or non-controversial by the City Council and will be enacted by one motion. There will be no separate discussion of these items, unless a Council Member or citizen so requests, in which event the items may be removed from the Consent Agenda for consideration.

CONSENT AGENDA ITEMS FOR DISCUSSION

PUBLIC QUESTIONS AND COMMENTS

Limited to five (5) minutes for any item of City business not on the agenda.

COUNCIL MEMBERS COMMENTS AND ANNOUNCEMENTS

CITY MANAGER UPDATE

PUBLIC HEARING:

6. Public hearing and consideration of adoption of the Farmington Hills 2024-2028 Parks and Recreation Master Plan. [CMR 1-24-01](#)

7. Public hearing and consideration to permit the partial redevelopment of the Hunter's Square Shopping Center, [PUD Plan 2, 2023](#), including Site Plan 67-9-2023.

NEW BUSINESS:

8. Consideration of [appointments](#) to various boards and commissions.

CONSENT AGENDA:

9. Recommended approval of executing the Memorandum of Understanding and Mutual Non-Disclosure Agreement with Mitsubishi Electric Automotive America, Inc. [CMR 1-24-02](#)
10. Recommended approval of purchase of one John Deere TX Turf Gator with all terrain tires and electric lift and one John Deere TX Deluxe Cargo Gator with turf tires from Deere & Company in the total amount of \$19,724.93. [CMR 1-24-03](#)
11. Recommended approval of [rescheduling the regular City Council meeting of February 26, 2024](#) to February 5, 2024.
12. Recommended approval of a resolution for acceptance of the 2023 Transportation Asset Management Plan. [CMR 1-24-04](#)
13. Recommended approval of City Council [study session meeting minutes](#) of December 11, 2023.
14. Recommended approval of City Council [regular session meeting minutes](#) of December 11, 2023.
15. Recommended approval of City Council [special meeting minutes](#) of December 21, 2023.

ADDITIONS TO AGENDA

16. Attorney Report

ADJOURNMENT

Respectfully submitted,

Carly Lindahl, Interim City Clerk

Reviewed by:

Gary Mekjian, City Manager

NOTE: Anyone planning to attend the meeting who has need of special assistance under the Americans with Disabilities Act (ADA) is asked to contact the City Clerk's Office at 248-871-2410 at least two (2) business days prior to the meeting, wherein necessary arrangements/ accommodations will be made.



PROCLAMATION

Vicki Barnett

*In Recognition of Exemplary Service to the Citizens of Farmington Hills
January 2024*

- WHEREAS,** Vicki Barnett has represented the people of Farmington Hills with honor and distinction for nearly 30 years and is known as an effective, well-informed, honest leader who helped to make the City of Farmington Hills, Oakland County and the State of Michigan better places to live, work and do business; and,
- WHEREAS,** Vicki Barnett was first elected to the Farmington Hills City Council in 1995, where her strong vision, heartfelt advocacy and financial acumen served the City well during her tenure as a Council member, four terms as Mayor, three terms as a State Representative, and during her service with the Michigan Municipal League on the Board of Directors and as President; and,
- WHEREAS,** Vicki Barnett won decisive victories in each of her elections and served the people as a tireless advocate for strengthening the economy, creating jobs and supporting families, causing her to be named as one of metro Detroit's most influential women by Crain's Detroit Business and chosen as the Legislator of the Year by the Police Officers Association of Michigan during her time in Lansing; and,
- WHEREAS,** from the beginning of her distinguished political career and throughout her many years devoted to public service, Vicki Barnett has worked diligently to improve the quality of life for all those around her and has had a major impact on the people in Farmington Hills and throughout the State of Michigan.

NOW, THEREFORE, BE IT RESOLVED that I, Theresa Rich, Mayor of the City of Farmington Hills, on behalf of the City Council, do hereby honor and thank **Vicki Barnett** for her commitment and service to the people of our City, and encourage everyone to respect and recognize the work of this highly dedicated individual.

A handwritten signature in black ink, reading "Theresa Rich", is written over a horizontal line.

Theresa Rich, Mayor



PROCLAMATION

Mary Newlin

*In Recognition of Exemplary Service to the Citizens of Farmington Hills
January 2024*

- WHEREAS,** Mary Newlin has served the City of Farmington Hills and its residents with honor and distinction, and was regarded by her colleagues as a compassionate, outspoken and common-sense leader during her time as a member of the City Council from 2019 to 2023 and as Mayor Pro Tem in 2022; and,
- WHEREAS,** Mary Newlin has resided in the City of Farmington Hills for more than 30 years and, as a longtime resident, knows that the key to a strong community is ensuring that every resident has a voice; and,
- WHEREAS,** Mary Newlin leveraged first-hand medical expertise gained during her extensive career as a registered nurse with a bachelor's of science in nursing as council liaison to the Commission on Community Health, which is dedicated to supporting and encouraging an environment where residents have access to quality health services and wellness programs; and,
- WHEREAS,** Mary Newlin, a mother of three Farmington Public Schools graduates, showed unwavering commitment to every family in the City of Farmington Hills as Council liaison to the Commission on Children, Youth and Families, which promotes an environment where children and families are happy, healthy, educated, safe and encouraged to reach their full potential; and,
- WHEREAS,** Mary Newlin was a forward-thinking council member who not only worked on behalf of current Farmington Hills residents, but to make the City and its services better for future generations who will call this City home.

NOW, THEREFORE, BE IT RESOLVED that I, Theresa Rich, Mayor of the City of Farmington Hills, on behalf of the City Council, do hereby honor and thank **Mary Newlin** for her loyal service to the people of our City, and encourage everyone to respect and recognize her contributions to ensure that the City of Farmington Hills is a place where everyone is welcome.

A handwritten signature in black ink, appearing to read "Theresa Rich", is written over a horizontal line.

Theresa Rich, Mayor



PROCLAMATION

Dr. Kenneth Massey

*In Recognition of Exemplary Service to the Citizens of Farmington Hills
January 2024*

- WHEREAS,** Dr. Kenneth Massey has served the City of Farmington Hills and its residents with integrity for two decades, including four terms as a member of City Council, two terms as Mayor from 2015 to 2019, and as Mayor Pro Tem in 2006, 2010 and 2015; and,
- WHEREAS,** during his career in politics, Ken Massey has been involved in the Michigan Municipal League and the National League of Cities Public Safety Steering Committee, while remaining committed to his immediate community as vice president of his homeowners' association and as a member of the Council of Homeowner Associations; and,
- WHEREAS,** Ken Massey's insight and influence provided valuable contributions to the boards and commissions which he served as Council Liaison, including the Retirement Board of Trustees, the Emergency Preparedness Commission, of which he is a past chair, and the Economic Development Corporation as part of which he was instrumental in the establishment of The Hawk Innovation Center and Hatchery, a business incubator located inside the City's award-winning recreation center, The Hawk; and,
- WHEREAS,** Ken Massey's dedication to the City and its residents goes far beyond his service on City Council, as evidenced by his role in founding CARES of Farmington Hills in 2017, a non-profit that provides services to individuals and families in Farmington Hills and seven surrounding communities in Oakland and Wayne Counties, including a food pantry that supports more than 550 families monthly, and in establishing Farmington SAFE, a non-profit task force focused on facilitating conversations and providing resources around suicide prevention.

NOW, THEREFORE, BE IT RESOLVED that I, Theresa Rich, Mayor of the City of Farmington Hills, on behalf of the City Council, do hereby honor and thank **Dr. Kenneth Massey** for his tireless service to the people of our City, and encourage everyone to respect and recognize the contributions Ken has made to making the City of Farmington Hills even better.

A handwritten signature in black ink, appearing to read "Theresa Rich", written in a cursive style.

Theresa Rich, Mayor

REPORT FROM CITY MANAGER TO CITY COUNCIL
January 8, 2024

SUBJECT: Adoption of the Farmington Hills 2024-2028 Parks and Recreation Master Plan

AMINISTRATIVE SUMMARY:

- The City of Farmington Hills Department of Special Services has undertaken the process to prepare the five-year Parks and Recreation Master Plan. The plan is a road map for the parks, recreation, facilities, and programming decisions made in the next five years, as well as the projected future needs.
- As essential task in the planning process is to determine the needs of the community. This was accomplished by comparing the community to national guidelines, information gathered during the public input process and online surveys, consultation with the Department of Special Services and Parks & Recreation Commission, and by reviewing current offerings and programs.
- The Plan was also developed to meet the guidelines and grant requirements of the Michigan Department of Natural Resources. Since 1987, the City of Farmington Hills has received over \$4.7 million in grant funding, through various sources, to enhance parks and recreation opportunities for Farmington Hills residents.

RECOMMENDATION:

IT IS RESOLVED THAT the City Council adopts the City of Farmington Hills 2024-2028 Parks and Recreation Master Plan.

Prepared by:

Ellen Schnackel, Director of Special Services

Reviewed & Approved by:

Gary Mekjian, City Manager

Farmington Hills

5-Year Parks & Recreation Master Plan



January 8, 2024

Why is Farmington Hills updating their Parks & Recreation Master Plan?



To retain the ability to be eligible to receive grants from the Michigan Department of Natural Resources (MDNR).



The MDNR requires that community recreation plans be updated every five years to provide an opportunity to adapt to the changing needs and assets of a community.



To identify and evaluate any new or potential partnerships.



To create an Action Plan that can be aligned to an annual budget.

What is included in the Parks and Recreation Master Plan?

(As required by the MDNR)

Community Description

Administrative Structure

Recreation Inventory

Description of the Planning Process

Description of the Public Input Process

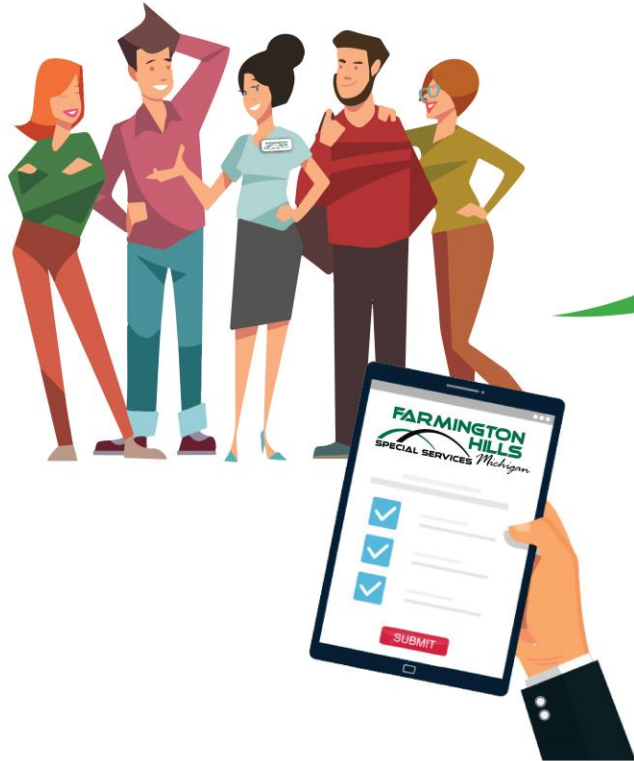
Goals and Objectives

Action Program

The Plan Development Process

First...

We gathered community input through a public input meeting and online opinion survey.



Then...

A draft plan was created which summarizes the needs, wants and options for funding.



After that...

The draft plan was made available to the public for a 30-day review period.



Finally...

We can begin to apply for grants through the MDNR!



And then...

Once we have a final approved plan, we will submit it to the MDNR to become grant eligible.



Results of Community Engagement



25 Aug. - 8 Nov. 2023

Online Survey
1,059 Total Responses



18 Oct. 2023

Public Input Session
36 Participants

Survey Responses

Question 1: Where do you reside?

- **10%** Farmington
- **78%** Farmington Hills
- **12%** Non-resident

Question 2: Which of the following three statements comes closest to how you feel about city-operated **PARKS**?

- **74.5%** Members of my household use city parks on a regular basis. I believe that these services are important to quality of life.
- **23.5%** Although members of my household do not use city parks on a regular basis, I believe that these services are important to quality of life.
- **0.1%** Visiting city parks on a regular basis is not important to quality of life.
- **3.17%** Other (Please Specify)

Question 3: Which of the following three statements comes closest to how you feel about city-operated **FACILITIES**?

- **69.4%** Members of my household use city facilities on a regular basis. I believe that these services are important to quality of life.
- **27.7%** Although members of my household do not use city facilities on a regular basis, I believe that these services are important to quality of life.
- **2%** Visiting city facilities on a regular basis is not important to quality of life.
- **1.9%** Other (Please Specify)

What is the condition of the existing park system?

How do you rate the condition of existing FACILITIES?

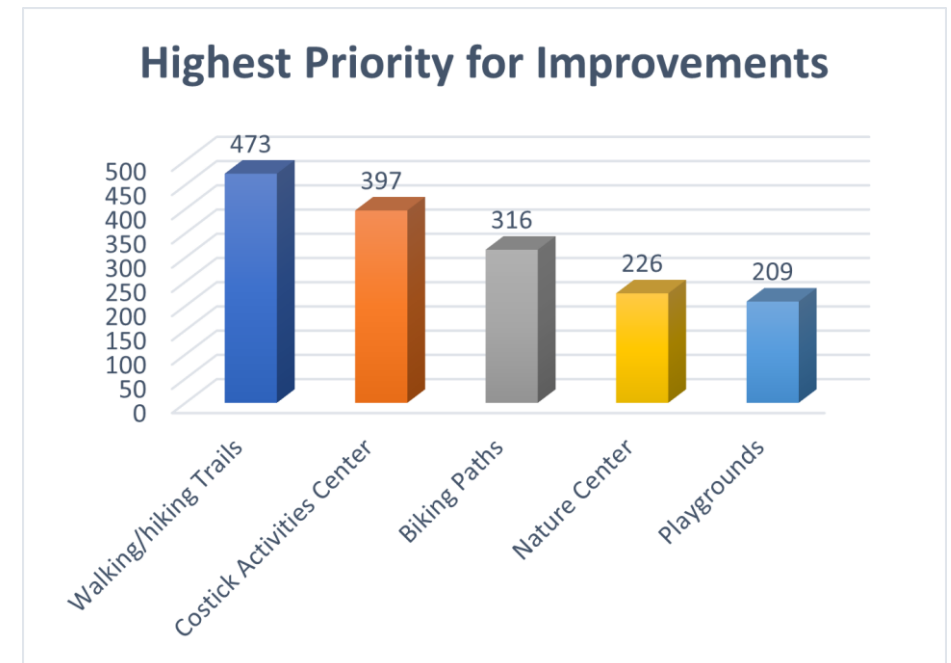
- An average of greater than 78% of respondents rated facilities “Good” to “Great”.
- The lowest rated facility was the Costick Activities Center with nearly 9% rating it as “Poor” and 31% rating it as “Adequate”

How do you rate the condition of existing PARKS & AMENITIES?

- An average of greater than 75% of respondents rated parks & amenities “Good” to “Great”.
- The lowest rated parks were Olde Town Park and William Grace Dog Park with apx. 33% of respondents rating them “Poor” to “Adequate”.

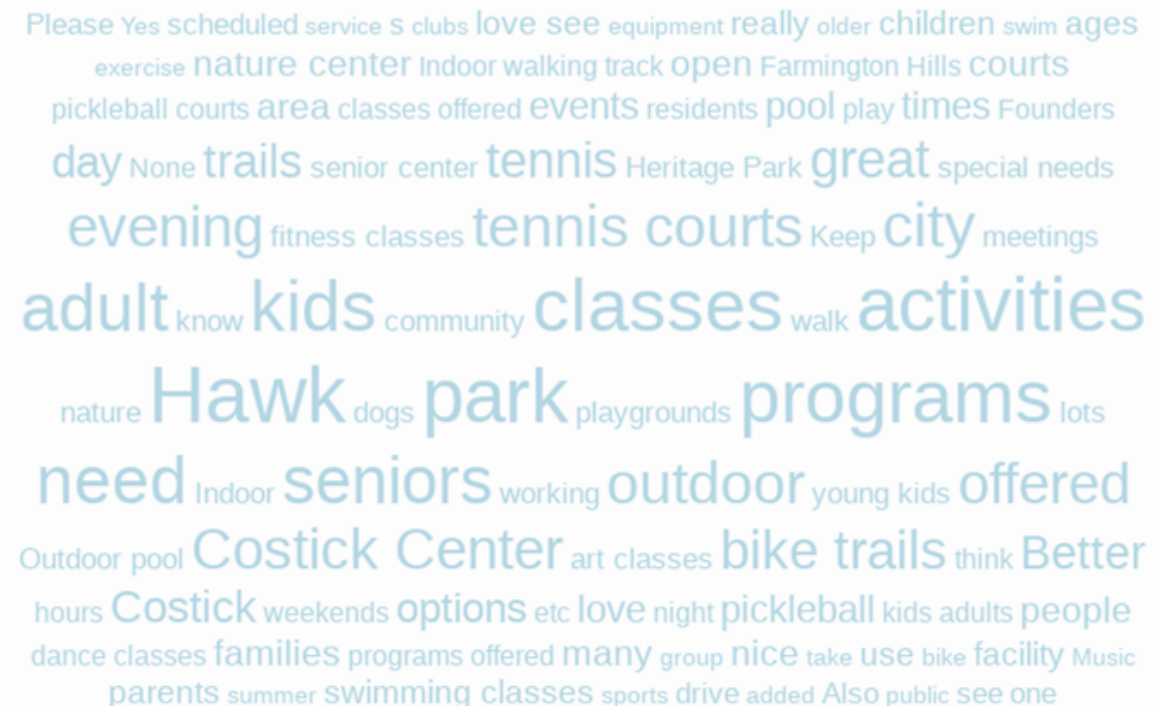
What are the priorities for improvement?

- When asked; Which of the following would be your priority(s) to receive improvements in the next five (5) years? Top 5 responses were:
 1. Walking/Hiking Trails
 2. Costick Activities Center
 3. Biking Paths
 4. Nature Center
 5. Playgrounds



What could be added?

- When asked; Are there any programs or facilities not currently offered by the Farmington Hills Special Services Department that you would like to see added? A few of the most mentioned items were:
 - More senior activities
 - More activities for teens
 - More activities for disabled individuals, particularly those who are neurodivergent or intellectually disabled
 - Additional tennis courts
 - Additional trails



Please Yes scheduled service S clubs love see equipment really older children swim ages exercise nature center Indoor walking track open Farmington Hills courts pickleball courts area classes offered events residents pool play times Founders day None trails senior center tennis Heritage Park great special needs evening fitness classes tennis courts Keep city meetings adult know kids community classes walk activities nature Hawk dogs park playgrounds programs lots need Indoor seniors working outdoor young kids offered Outdoor pool Costick Center art classes bike trails think Better hours Costick weekends options etc love night pickleball kids adults people dance classes families programs offered many group nice take use bike facility Music parents summer swimming classes sports drive added Also public see one

Public Input Session

- Participants were asked to provide input based on what concerns they may have or improvements they would like to see related to specific parks and facilities.
- A few of the most expressed comments included:
 - Improvements at the William Grace Dog Park
 - Paving the parking areas at Heritage and Founders Sports Parks
 - Improve and add to the pathways at several parks.



Goals and Objectives

Goal	Objective
Accessibility	Ensure accessibility for people of all abilities in all existing and future facilities.
Facilities Forward	Ensure the best course for facilities into the future.
Connectivity	Improve nonmotorized connectivity within the park system and surrounding communities.
Environmental Awareness and Preservation	Preserve and/or expand precious natural features.
Communication	Improve internal and external communication.
Community Relationships/Partnerships	Foster existing relationships as well as seek out new partnership opportunities.

Action Plan

- The Action Plan (*or Capital Improvement Plan*) serves as a guiding document for prioritizing projects and developing budgets for the next five years.
- Example projects included in the Action Plan for the next five years:
 - An adaptive playground and splashpad at Heritage Park
 - Resurfacing of cart paths at FH Golf Club
 - Baseball dugouts at Founders Sports Park

If the plan is approved...

- The plan will be referred to by MDNR when grant proposals are submitted.
- The action plan helps with park and recreation decision making regarding:
 - Programming
 - Land acquisition
 - Capital or minor improvements
 - Building ties with the community

Thank you!

Questions or
Comments?



Inter-Office Correspondence

DATE: January 8, 2023

TO: Gary Mekjian, City Manager

FROM: Charmaine Kettler-Schmult, Director of Planning and Community Development

SUBJECT: Planned Unit Development (“PUD”) 2, 2023, including Site Plan 67-9-2023 – Hunter’s Square Redevelopment

Applicant: Timothy Collier

Owner: RPT Realty, L.P.

Sidwell: 22-23-03-226-027 and 028

Zoning: B-2, Community Business District

Master Plan: Shopping Center Type Business

Location: 30825 and 31361 Orchard Lake Road

Description:

The applicant has submitted for City Council consideration proposed **PUD Plan 2, 2023, including Site Plan 67-9-2023**, which seeks to permit the partial redevelopment of the Hunter’s Square shopping center at the southwest intersection of Orchard Lake and Fourteen Mile Roads. The applications propose to demolish the existing northerly portion of the strip center and construct four (4) new major tenant spaces, the northernmost of which would have a drive-through pharmacy component.

Additionally, the applications propose to construct four (4) new outlot buildings along Orchard Lake Road, the northernmost of which would have a drive-through restaurant component. Finally, pedestrian circulation improvements, outdoor seating areas, public art, a future bus shelter, and improved landscaping are proposed, among other site improvements.

Please see Giffels Webster’s review [attached](#) for a detailed review of the plans.

Procedural Background:

- August 17, 2023 – Planning Commission unanimously passes motion qualifying application for PUD Option

- October 26, 2023 – Planning Commission unanimously passes motion setting application for public hearing for November 16, 2023
- November 16, 2023 – Following a public hearing, Planning Commission unanimously passes motion recommending *approval* of application to City Council

Summary of Zoning Deviations Sought:¹

Permit

- a front yard setback of 58.49 feet where 75 feet is the minimum setback required;
- a rear yard setback of 69.34 feet where 75 feet is the minimum setback required;
- the applicant to not provide a pedestrian connection between the existing sidewalk along Orchard Lake Road and proposed outlot “G” where connection is required;
- drive-in restaurant use where such use is not permitted within the B-2 District;
- drive-in pharmacy use where such use is not permitted within the B-2 District;
- sidewalk sales on a permanent basis where such use is permitted only on a seasonal basis within the B-2 District;
- indoor recreation facility use where such use is not permitted within the B-2 District;
- veterinary hospital or clinic where such use is not permitted within the B-2 District;
- stacking lane for proposed outlot “G” to be within a front yard where such lanes are prohibited in front yards;
- dumpster enclosures to be within front yard where such enclosures shall not be within front yards;
- fifteen (15) loading spaces where twenty (20) spaces are required; and
- five (5) signs on the front façade of major “E” where two (2) signs are the maximum number permitted on one (1) façade.

Possible Council Actions:

Suggested Resolution for APPROVAL:

Resolve that the application for approval of PUD Plan 2, 2023, including SP 67-9-2023, dated received December 7, 2023, is granted, subject to the following conditions:

- (1) *Any conditions and requirements stated the Planning Commission’s November 16, 2023, motion recommending approval of the PUD plan and Giffels Webster’s review shall be complied with or addressed to the satisfaction of the Planning and Community Development Department;*
- (2) *Any conditions and requirements stated in the reviews of the City Engineer and City Fire Marshal shall be complied with or addressed to the satisfaction of the City Engineer and Fire Marshal;*
- (3) ***[Insert additional conditions if appropriate.]***

And further resolve that the City Attorney prepare the appropriate PUD agreement stipulating the final PUD approval conditions and authorizing the identified zoning deviations for City Council consideration and final approval.

¹ In the event of a conflict between the deviations listed in this report and those listed in the planning consultant’s review, the deviations listed in this report shall govern.

Suggested Resolution for DENIAL:

Resolve that the application for approval of PUD Plan 2, 2023, including SP 67-9-2023, dated September 15, 2023, is denied, because it does not meet all provisions set forth in Section 34-3.20 of the Zoning Ordinance and the proposed development will adversely affect the public health, welfare, and safety for the following reasons: [Indicate reasons the PUD does not meet the requirements of Section 34-3.20 and/or will have the adverse effect described above.]

Department Authorization by: Charmaine Kettler-Schmult, Director of Planning and Community Development
Prepared by: Erik Perdonik, City Planner

Attachments:

- **PUD Plan 2, 2023, including Site Plan 67-9-2023 received December 7, 2023**
- **Giffels Webster’s review dated December 15, 2023**
- **Engineering Division review dated December 18, 2023**
- **Fire Department review dated December 12, 2023**
- **August 17, 2023, Planning Commission meeting minutes**
- **October 26, 2023, Planning Commission meeting minutes**
- **November 16, 2023, Planning Commission meeting minutes**
- **Public Notice**

PUD AND SITE PLAN SUBMITTAL FOR
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN

PEA GROUP
 T: 844.813.2949
 www.peagroup.com

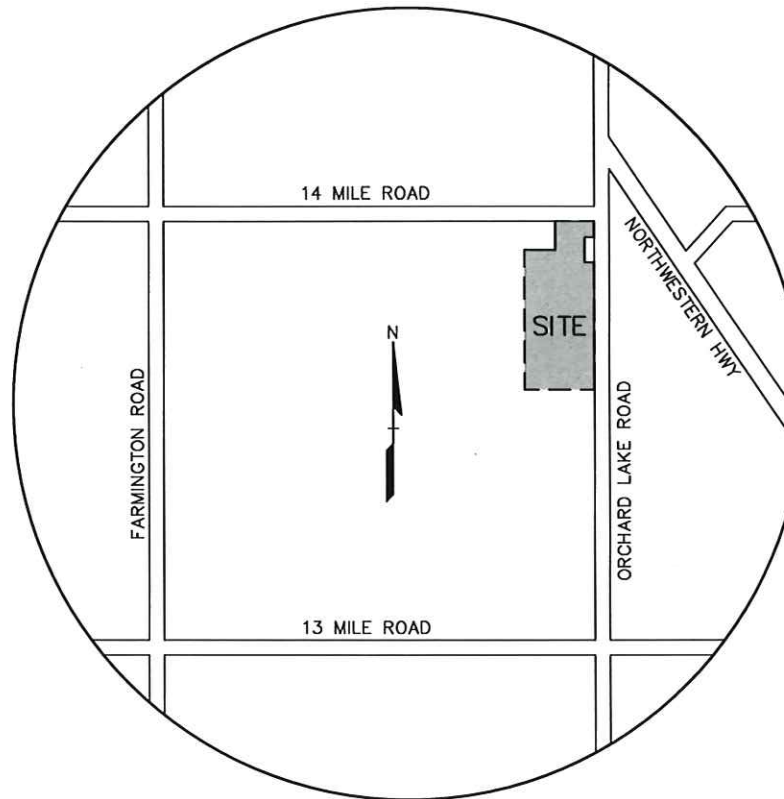


REV PUD 2, 2023

RECEIVED
 3P67-9-2023

DEC 07 2023

CITY OF FARMINGTON HILLS
 PLANNING DEPT.



LOCATION MAP
 NO SCALE

INDEX OF DRAWINGS:

- COVER SHEET
- C-1.0A OVERALL SITE PLAN
- C-1.0B OVERALL COMMUNITY BENEFIT PLAN
- C-1.0C SHEET REMOVED FROM THE SET
- C-1.0D TRAFFIC CIRCULATION PLAN
- C-2.1 TOPOGRAPHIC SURVEY - SHEET 1
- C-2.2 TOPOGRAPHIC SURVEY - SHEET 2
- C-2.3 TOPOGRAPHIC SURVEY - SHEET 3
- C-2.4 TOPOGRAPHIC SURVEY - SHEET 4
- C-2.5 TOPOGRAPHIC SURVEY - SHEET 5
- C-2.6 TOPOGRAPHIC SURVEY - SHEET 6
- C-2.7 EXISTING TREE LIST - SHEET 1
- C-2.8 EXISTING TREE LIST - SHEET 2
- C-4.1 DIMENSION AND PAVING PLAN - SHEET 1
- C-4.2 DIMENSION AND PAVING PLAN - SHEET 2
- C-4.3 DIMENSION AND PAVING PLAN - SHEET 3
- C-4.4 DIMENSION AND PAVING PLAN - SHEET 4
- C-4.5 DIMENSION AND PAVING PLAN - SHEET 5
- C-4.6 DIMENSION AND PAVING PLAN - SHEET 6
- C-7.0 PRELIMINARY OVERALL UTILITY PLAN
- C-7.1 PRELIMINARY GRADING AND UTILITY PLAN - SHEET 1
- C-7.2 PRELIMINARY GRADING AND UTILITY PLAN - SHEET 2
- C-7.3 PRELIMINARY GRADING AND UTILITY PLAN - SHEET 3
- C-7.4 PRELIMINARY GRADING AND UTILITY PLAN - SHEET 4
- C-7.5 PRELIMINARY GRADING AND UTILITY PLAN - SHEET 5
- C-7.6 PRELIMINARY GRADING AND UTILITY PLAN - SHEET 6
- C-11.1 NOTES AND DETAILS
- C-11.2 DETAILS
- C-12.0 OVERALL DRAINAGE MAP
- L-1.0 OVERALL LANDSCAPE PLAN
- L-1.1 LANDSCAPE PLAN - SHEET 1
- L-1.2 LANDSCAPE PLAN - SHEET 2
- L-1.3 LANDSCAPE PLAN - SHEET 3
- L-1.4 LANDSCAPE PLAN - SHEET 4
- L-1.5 LANDSCAPE PLAN - SHEET 5
- L-1.6 LANDSCAPE PLAN - SHEET 6
- L-2.0 LANDSCAPE DETAILS
- T-1.0 TREE PRESERVATION PLAN
- T-1.1 TREE PRESERVATION PLAN - SW
- T-1.2 TREE PRESERVATION PLAN - NW
- T-1.3 TREE PRESERVATION PLAN - SE
- T-1.4 TREE PRESERVATION PLAN - NE
- T-1.5 EXISTING TREE LIST
- PH-1 PHOTOMETRIC SITE PLAN
- PH-2 LIGHTING DETAILS
- ARCHITECTURAL PLANS:
- DET-1: ARCHITECTURAL SITE DETAILS
- A-1: MAJOR C, D-1, D-2 CONCEPTUAL FLOOR PLAN
- A-2: MAJOR C, D-1, D-2 CONCEPTUAL BUILDING ELEVATIONS
- A-3: MAJOR E FLOOR PLAN
- A-4: MAJOR E BUILDING ELEVATIONS AND 3D VIEWS
- A-4.1: MAJOR E ELEVATIONS AND SIGNAGE
- A-5: OUTLOT D CONCEPTUAL FLOOR PLAN
- A-6: OUTLOT D CONCEPTUAL BUILDING ELEVATIONS
- A-7: OUTLOT E CONCEPTUAL FLOOR PLAN
- A-8: OUTLOT E CONCEPTUAL BUILDING ELEVATIONS
- A-9: OUTLOT F CONCEPTUAL FLOOR PLAN
- A-10: OUTLOT F CONCEPTUAL BUILDING ELEVATIONS
- A-11: OUTLOT G CONCEPTUAL FLOOR PLAN
- A-12: OUTLOT G CONCEPTUAL BUILDING ELEVATIONS



CAUTION!!
 THE LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN ON THIS DRAWING FOR INFORMATION ONLY. NO GUARANTEE IS GIVEN AS TO THE ACCURACY THEREOF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS
 1 - SPAPUD RESUBMISSION 12/06/23

ORIGINAL ISSUE DATE
 SEPTEMBER 15, 2023

DRAWING TITLE
COVER SHEET - HUNTER'S SQUARE REDEVELOPMENT

PEA JOB NO. 22-0256
 P.M. RLS
 DN. KM
 DES. PB

DRAWING NUMBER
COVER

NOT FOR CONSTRUCTION

DEVELOPER:

RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN, 48076
 CONTACT: JOHN FREEL
 PHONE: (810) 569-0415
 EMAIL: JFREEL@RPTREALTY.COM

ARCHITECT:

COLLIERS ENGINEERING AND DESIGN
 20700 CIVIC CENTER DRIVE, SUITE #170
 SOUTHFIELD, MICHIGAN, 48076
 CONTACT: KRISTEN LARK
 PHONE: (248) 540-7700
 EMAIL: KRISTEN.LARK@COLLIERSENG.COM

CIVIL ENGINEER:

PEA GROUP
 58105 VAN DYKE ROAD
 WASHINGTON, MICHIGAN, 48094
 CONTACT: RACHEL L. SMITH, PE
 PHONE: (248) 821-9206
 EMAIL: RACHEL.SMITH@PEAGROUP.COM

LANDSCAPE ARCHITECT:

PEA GROUP
 7927 NEMCO WAY, SUITE #155
 BRIGHTON, MICHIGAN, 48116
 CONTACT: JANET EVANS, PLA
 PHONE: (517) 546-8583
 EMAIL: JEVANS@PEAGROUP.COM

5: \projects\2023\22-0256\22-0256.dwg, PLOT DATE: 12/15/23 10:14 AM, PLOT BY: RLS

BENCHMARKS:
(GPS DERIVED - NAVD83)

BM #300
DUMPLE ON ARROW ON HYDRANT AND 110' DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

BM #301
DUMPLE ON ARROW ON HYDRANT AND 137' DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL 1160' DIRECTLY SOUTH
FROM McDONALD'S RESTAURANT
ELEV. - 875.56

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN 'AREAS
DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE
FLOODPLAIN' (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 26125C0513P EFFECTIVE SEPTEMBER 28, 2006.

LEGAL DESCRIPTIONS:

PARCEL 23-03-228-028
A PARCEL OF LAND BEING SITUATED IN THE NORTHEAST 1/4 OF SECTION 3, TOWN 1 NORTH, RANGE 9 EAST, CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 3 AND PROCEEDING THENCE ALONG THE EAST LINE OF SAID SECTION 3, AND THE CENTERLINE OF ORCHARD LAKE ROAD (120 FEET WIDE) SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 258.00 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 214.00 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 130.35 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 152.65 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST, 154.00 FEET TO THE WEST LINE OF ORCHARD LAKE ROAD; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 1350.35 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 775.70 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 00 SECONDS EAST, 1242.48 FEET; THENCE NORTH 30 DEGREES 50 MINUTES 55 SECONDS WEST 155.35 FEET; THENCE NORTH 00 DEGREES 01 MINUTES 20 SECONDS WEST 127.47 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST 702.39 FEET TO THE POINT OF BEGINNING. CONTAINING 2.2650 ACRES

PARCEL 23-03-228-027
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GENERAL NOTES:
THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

- ALL CONSTRUCTION, WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH CURRENT OSHA, MDOT AND MUNICIPALITY STANDARDS AND REGULATIONS.
- THE CONTRACTOR SHALL NOTIFY THE CITY ENGINEER AND/OR THE AUTHORITY HAVING JURISDICTION 3 BUSINESS DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- THE CONTRACTOR SHALL CONTACT THE ENGINEER SHOULD THEY ENCOUNTER ANY DESIGN ISSUES DURING CONSTRUCTION. IF THE CONTRACTOR MAKES DESIGN MODIFICATIONS WITHOUT THE WRITTEN DIRECTION OF THE DESIGN ENGINEER, THE CONTRACTOR DOES SO AT HIS OWN RISK.
- ALL NECESSARY PERMITS, TESTING, BONDS AND INSURANCES ETC., SHALL BE PAID FOR BY THE CONTRACTOR. THE OWNER SHALL PAY FOR ALL CITY INSPECTION FEES.
- THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE 811/ONE CALL UTILITY LOCATING CENTER, THE CITY ENGINEER AND/OR THE AUTHORITY HAVING JURISDICTION 3 BUSINESS DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION. IF NO NOTIFICATION IS GIVEN AND DAMAGE RESULTS, SAID DAMAGE WILL BE REPAIRED AT SOLE EXPENSE OF THE CONTRACTOR. IF EXISTING UTILITY LINES ARE ENCOUNTERED THAT CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE DESIGN ENGINEER SO THAT THE CONFLICT MAY BE RESOLVED.
- CONTRACTOR SHALL VERIFY THAT THE PLANS AND SPECIFICATIONS ARE THE VERY LATEST PLANS AND SPECIFICATIONS AND FURTHERMORE, VERIFY THAT THESE PLANS AND SPECIFICATIONS HAVE BEEN APPROVED. ALL ITEMS CONSTRUCTED BY THE CONTRACTOR PRIOR TO RECEIVING FINAL APPROVAL, HAVING TO BE ADJUSTED OR RE-DONE, SHALL BE AT THE CONTRACTOR'S EXPENSE. SHOULD THE CONTRACTOR ENCOUNTER A CONFLICT BETWEEN THESE PLANS AND/OR SPECIFICATIONS, THEY SHALL SEEK CLARIFICATION IN WRITING FROM THE ENGINEER BEFORE COMMENCEMENT OF CONSTRUCTION. FAILURE TO DO SO SHALL BE AT SOLE EXPENSE TO THE CONTRACTOR.
- ANY WORK WITHIN THE STREET OR HIGHWAY RIGHTS-OF-WAY SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE AGENCIES HAVING JURISDICTION AND SHALL NOT BEGIN UNTIL ALL NECESSARY PERMITS HAVE BEEN ISSUED FOR THE WORK.
- ALL PROPERTIES OR FACILITIES IN THE SURROUNDING AREAS, PUBLIC OR PRIVATE, DESTROYED OR OTHERWISE DISTURBED DUE TO CONSTRUCTION, SHALL BE REPLACED AND/OR RESTORED TO THE ORIGINAL CONDITION BY THE CONTRACTOR.
- THE CONTRACTOR SHALL PROVIDE ALL NECESSARY BARRICADING, SIGNAGE, LIGHTS AND TRAFFIC CONTROL DEVICES TO PROTECT THE WORK AND SAFELY MAINTAIN TRAFFIC IN ACCORDANCE WITH LOCAL REQUIREMENTS AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (LATEST EDITION). THE DESIGN ENGINEER, OWNER, CITY AND STATE SHALL NOT BE HELD LIABLE FOR ANY CLAIMS RESULTING FROM ACCIDENTS OR DAMAGES CAUSED BY THE CONTRACTOR'S FAILURE TO COMPLY WITH TRAFFIC AND PUBLIC SAFETY REGULATIONS DURING THE CONSTRUCTION PERIOD.
- THE USE OF CRUSHED CONCRETE IS PROHIBITED ON THE PROJECT WITHIN 100 FEET OF ANY WATER COURSE (STREAM, RIVER, COUNTY DRAIN, ETC.) AND LAKE, REGARDLESS OF THE APPLICATION OR LOCATION OF THE WATER COURSE OR LAKE RELATIVE TO THE PROJECT LIMITS.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO ADJUST THE TOP OF ALL EXISTING AND PROPOSED STRUCTURES (MANHOLES, CATCH BASINS, BILETS, GATE WELLS ETC.) WITHIN GRADED AND/OR PAVED AREAS TO FINAL GRADES SHOWN ON THE PLANS. ALL SUCH ADJUSTMENTS SHALL BE INCIDENTAL TO THE JOB AND WILL NOT BE PAID FOR SEPARATELY.

ORDNANCE DEVIATIONS:

ALLOWED USE DEVIATIONS
• DRIVE-IN RESTAURANT
• PHARMACY PICK-UP
• OUTDOOR SIDEWALK SALES
• INDOOR RECREATIONAL ACTIVITIES
• VETERINARY HOSPITAL OR CLINIC

SETBACK DEVIATIONS
• REAR SETBACK DEVIATION FOR MAJOR BLDG (69.33' PROPOSED/75' REQUIRED)
• FRONT SETBACK DEVIATION FOR OUTLOT BLDG (58.49' PROPOSED/75' REQUIRED)
• SIDE SETBACK DEVIATION FOR EXISTING BUILDING (74.88' EXISTING/75' REQUIRED)
• PARKING SETBACK DEVIATION FOR EXISTING S.E. LOT (4.96' EXISTING, 10' REQUIRED)

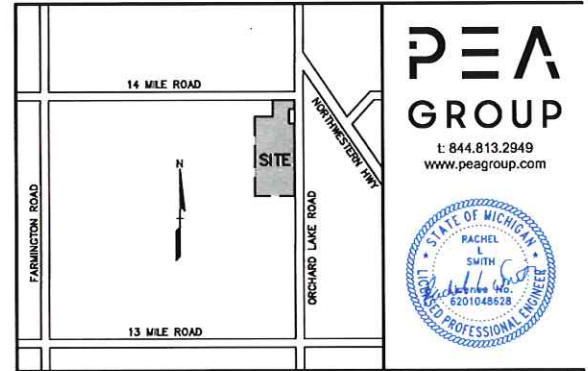
FRONT YARD DEVIATIONS
• OUTPOST ENCLOSURE IN FRONT YARD (OUTLOT 'G')
• VISUALLY SCREENED DRIVE THROUGH IN FRONT YARD WITH 10 STACKING SPACES TO MEET ORDINANCE (OUTLOT 'G')

SIGNAGE DEVIATIONS
• ADDITIONAL FACADE SIGNS ON MAJOR 'E' (5 PROPOSED/2 MAX ALLOWED)
• REUSE AND RELOCATE EXISTING NON-CONFORMING SIGNAGE

LOADING SPACE DEVIATIONS
• REDUCED LOADING SPACES BY 5 (15 PROPOSED/20 REQUIRED)

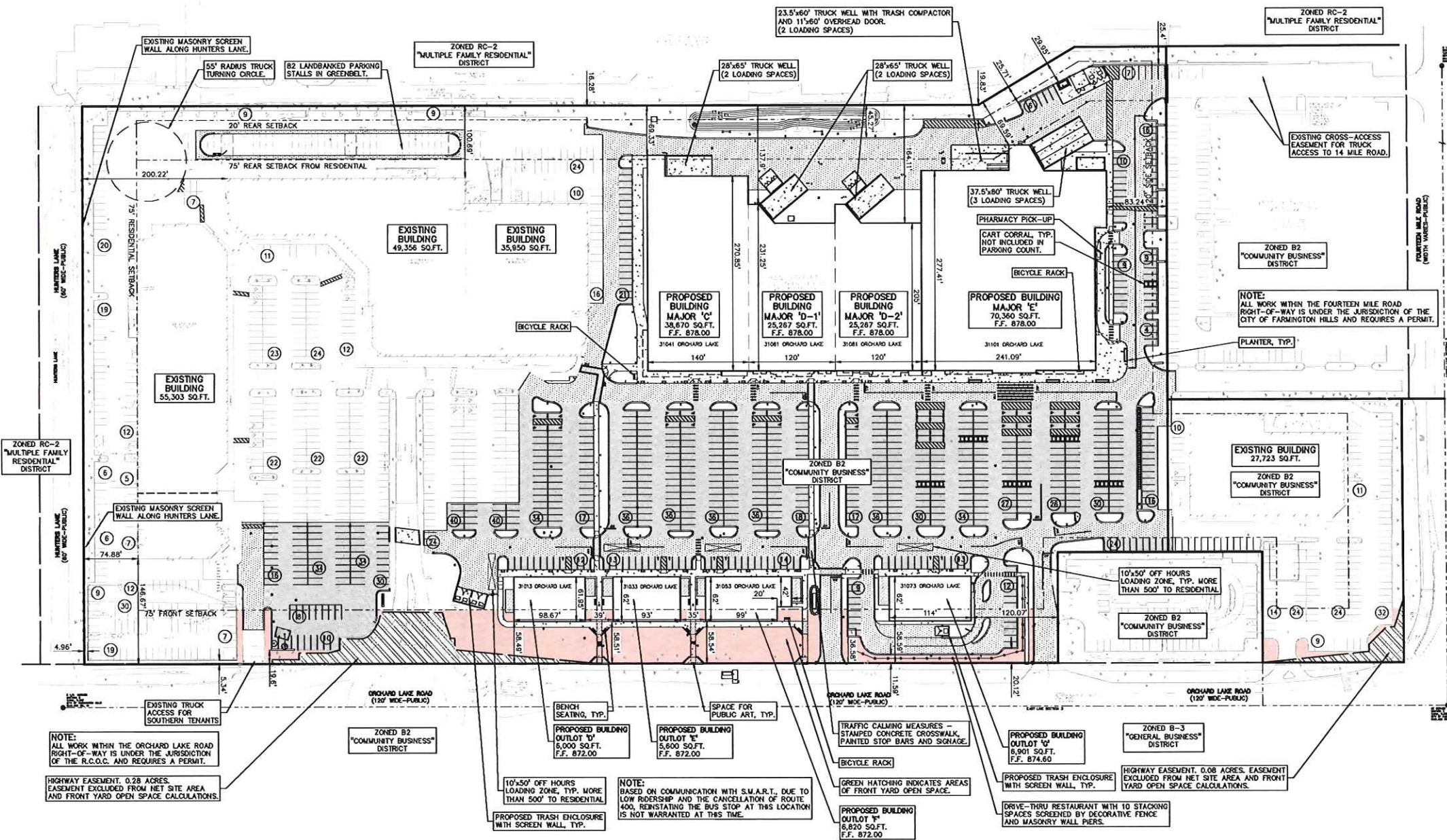
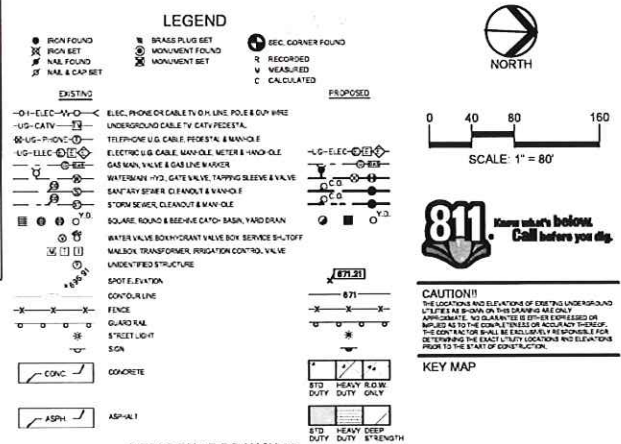
CITY OF FARMINGTON HILLS STANDARD NOTES:
THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

- NOTIFY THE CITY OF FARMINGTON HILLS ENGINEERING DIVISION AT 248.871.2560 A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
- ALL CONSTRUCTION MUST BE CONFORMING TO THE CURRENT STANDARDS AND SPECIFICATIONS ADOPTED BY THE CITY OF FARMINGTON HILLS.
- UTILITIES TO BE UNDERGROUND.
- ALL USE MISC DIO 1800.647.7344 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
- ALL SOIL EROSION AND SILT MUST BE CONTROLLED AND CONTAINED ON SITE.
- ALL EXCAVATION UNDER OR WITHIN 3 FEET OF PUBLIC PAVEMENT, EXISTING OR PROPOSED, SHALL BE BACKFILLED AND COMPACTED WITH SAND (CLASS II MDOT).
- THE CONTRACTOR IS RESPONSIBLE FOR ALL DAMAGE TO EXISTING UTILITIES.
- PRIOR TO THE ISSUANCE OF AN OCCUPANCY PERMIT, ENGINEERING SITE INSPECTION IS REQUIRED.



GENERAL NOTES:
THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

- ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, FACE OF SIDEWALK, OUTSIDE FACE OF BUILDING, PROPERTY LINE, CENTER OF MANHOLE/CATCH BASIN OR CENTERLINE OF PIPE UNLESS OTHERWISE NOTED.
- 'NO PARKING-FIRE LANE' SIGNS SHALL BE POSTED ALONG ALL FIRE LANES AS DIRECTED BY THE FIRE OFFICIAL.
- REFER TO NOTES AND DETAILS SHEET FOR ON-SITE PAVING DETAILS.
- REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.



SITE DATA TABLE:

SITE AREA: 28.85 ACRES (1,256,706 S.F.) GROSS
28.48 ACRES (1,241,024 S.F.) NET

ZONING: B-2 (COMMUNITY BUSINESS DISTRICT) PROPOSED PUD
PROPOSED USE: COMMERCIAL SHOPPING CENTER (1,241,024 S.F.)
PROPOSED ADDITIONAL USES: DRIVE-IN RESTAURANT, INDOOR RECREATIONAL FACILITIES, SIDEWALK SALES, PHARMACY PICK-UP, ELECTRIC VEHICLE INFRASTRUCTURE, VETERINARY HOSPITAL OR CLINIC

BUILDING INFORMATION:
MAXIMUM ALLOWABLE BUILDING HEIGHT = 50 FEET (3 STORIES)
PROPOSED BUILDING HEIGHT = 1 STORY
BUILDING FOOTPRINT AREA = 353,217 S.F.
BUILDING LOT COVERAGE = 353,217 S.F. / 1,241,024 S.F. = 28.5%

SETBACK REQUIREMENTS:

REQUIREMENT	REQUIRED	PROPOSED
FRONT (EAST)	75'	58.49'
SIDE (SOUTH)	75'	74.88' (EXISTING BUILDING)
SIDE (NORTH)	20'	83.24'
REAR (WEST)	75'	69.33' (EXISTING 60.78')

PARKING CALCULATIONS:
USABLE FLOOR AREA (75% OF GROSS FLOOR AREA) = 353,217 S.F. x 0.75 = 264,913 S.F.
COMMERCIAL SHOPPING CENTER
1 SPACE PER 150 S.F. USABLE AREA UP TO 15,000 S.F. = 15,000 S.F. / 150 S.F. = 100 SPACES
1 SPACE PER 175 S.F. USABLE AREA FROM 15,001 S.F. TO 100,000 S.F. USABLE AREA = 85,000 S.F. / 175 S.F. = 485.7 = 486 SPACES
1 SPACE PER 200 S.F. USABLE AREA IN EXCESS OF 100,001 S.F. = (264,913 S.F. - 100,001 S.F.) / 200 S.F. = 164,913 S.F. / 200 S.F. = 824.6 = 825 SPACES
TOTAL REQUIRED PARKING = 100 + 486 + 825 = 1,411 SPACES
TOTAL LAND-BANKED PARKING SPACES = 92 SPACES
TOTAL AVAILABLE PARKING SPACES = 1,413 SPACES (NOT INC. CART CORRALS)

PARKING RATIO = 1,413 SPACES/353,217 GROSS FLOOR AREA = 4.0 SPACES/1000 S.F.

LOADING CALCULATIONS:
LOADING REQUIRED = 10 S.F. OF LOADING AREA PER FOOT OF BUILDING FRONTAGE
1 LOADING SPACE IS 500 S.F.

BUILDING FRONTAGE:

BUILDING	FRONTAGE	FORMULA	REQUIRED	PROVIDED
MAJOR 'C'	140'	(140x10)/500	= 2.8 (2)	2
MAJOR 'D-1'	120'	(120x10)/500	= 2.4 (2)	2
MAJOR 'D-2'	120'	(120x10)/500	= 2.4 (2)	2
MAJOR 'E'	241.09'	(241.09x10)/500	= 4.8 (5)	5
OUTLOT 'D'	98.67'	(98.67x10)/500	= 2.0 (2)	1
OUTLOT 'E'	93'	(93x10)/500	= 1.9 (2)	1
OUTLOT 'F'	99'	(99x10)/500	= 2.0 (2)	1
OUTLOT 'G'	114'	(114x10)/500	= 2.3 (2)	1
TOTAL			= 20	15

OPEN SPACE:
MINIMUM FRONT YARD OPEN SPACE REQUIRED = 20%
PROVIDED FRONT YARD OPEN SPACE = 37,570 S.F. / 100,333 S.F. = 37%

SITE SOILS INFORMATION:
ACCORDING TO THE USDA NATURAL RESOURCES CONSERVATION SERVICE WEB SOIL SURVEY FOR OAKLAND COUNTY DATED AUGUST 22, 2023, THE SITE CONSISTS OF THE FOLLOWING SOIL TYPES:
SANDY LOAM, LOAMY SAND, LOAMS, FINE SAND, UDIPSAMMENTS, URBAN LAND

REVISIONS

NO.	DESCRIPTION	DATE
1	1-SPAPUD RESUBMISSION	12-08-23

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
OVERALL SITE PLAN

PEA JOB NO. 22-0256

P.M. RLS

D.N. KM

DES. PB

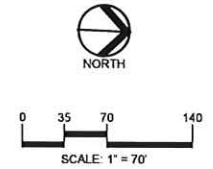
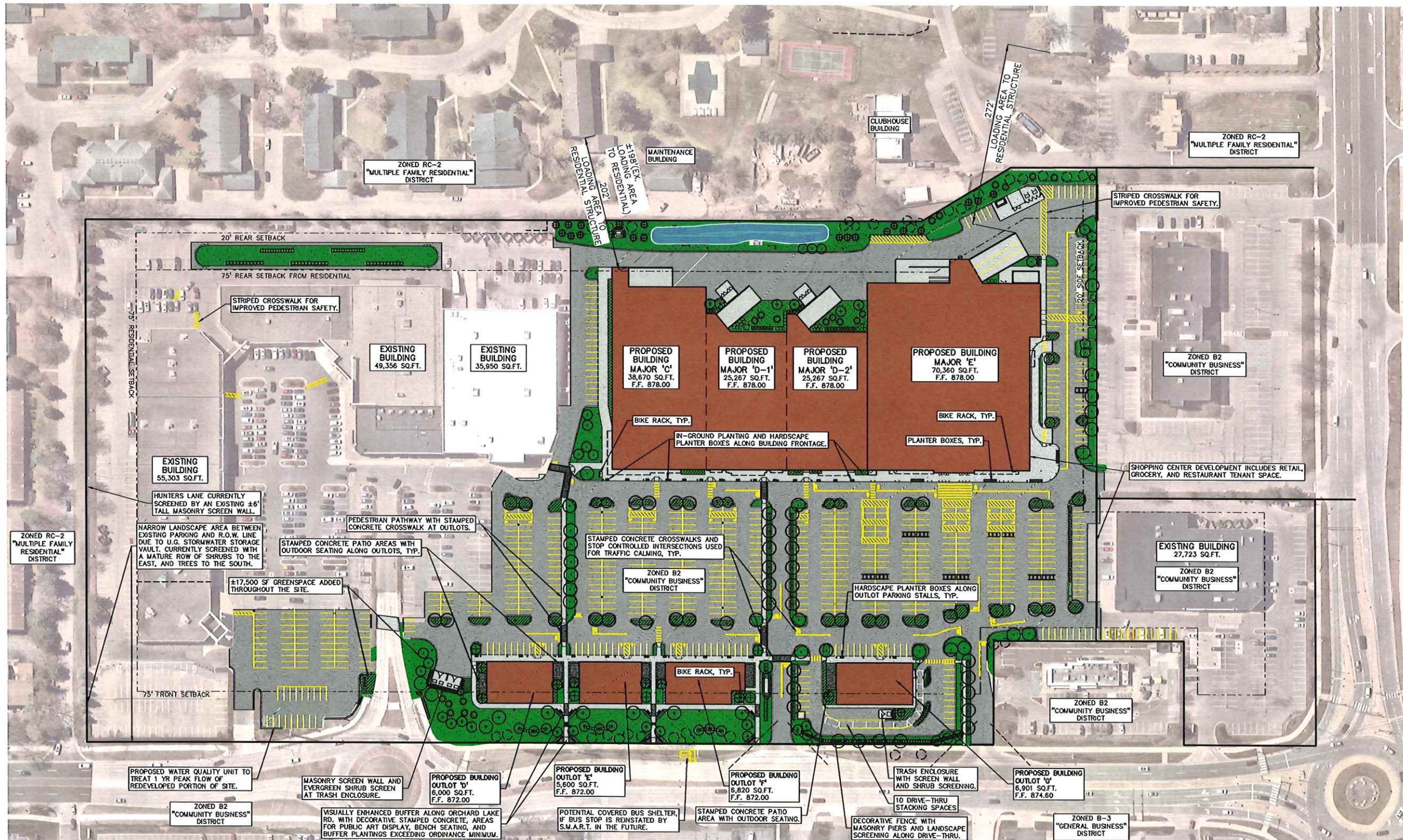
DRAWING NUMBER
C-1.0A

NOT FOR CONSTRUCTION

- PROPOSED COMMUNITY BENEFITS LIST:**
1. SPACES FOR PUBLIC ARTWORK
 2. REDUCTION OF ON SITE PAVED AREAS
 3. ADDITION OF PEDESTRIAN PATHWAYS
 4. TRAFFIC CALMING MEASURES TO INCREASE PEDESTRIAN SAFETY
 5. DIVERSITY OF ON-SITE USES INCLUDING RETAIL, GROCERY, RESTAURANTS, OFFICE SPACE AND ENTERTAINMENT
 6. ENHANCEMENT OF EXISTING LANDSCAPE BUFFER ADJACENT TO RESIDENTIAL PROPERTY TO THE WEST.
 7. ENHANCEMENT OF LANDSCAPE BUFFER ALONG ORCHARD LAKE ROAD
 8. ADDITIONAL SHRUBBERY AND PERENNIAL LANDSCAPE ENHANCEMENTS IN PARKING LOT
 9. OUTDOOR PLAZA AREAS WITH BENCH SEATING
 10. ADDITIONAL STORMWATER QUALITY MEASURES AND DETENTION STORAGE CAPACITY.
 11. DEFERRED/FUTURE COVERED BUS STOP IF S.M.A.R.T. REINSTATES THE BUS LINE.
 12. A REDUCTION OF 424 PARKING SPACES, AND THE RESULTANT LOSS OF PAVEMENT.

PEA GROUP
 t: 844.813.2949
 www.peagroup.com

STATE OF MICHIGAN
 RACHEL L. SMITH
 LICENSE NO. 6201048528
 PROFESSIONAL LANDSCAPE ARCHITECT



811 Know what's below. Call before you dig.

CAUTION!!
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KEY MAP

CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 BENTLEY, MICHIGAN 48724

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS	
1 - SPAPUD RESUBMISSION	12/08/23

ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
OVERALL COMMUNITY BENEFIT PLAN

PEA JOB NO.	22-0256
P.M.	RLS
DN.	KM
DES.	PB
DRAWING NUMBER	

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(GPS DERIVED - NAVD83)

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FLOODPLAIN' (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 26125C0513F EFFECTIVE SEPTEMBER 29, 2006.

LEGAL DESCRIPTIONS:

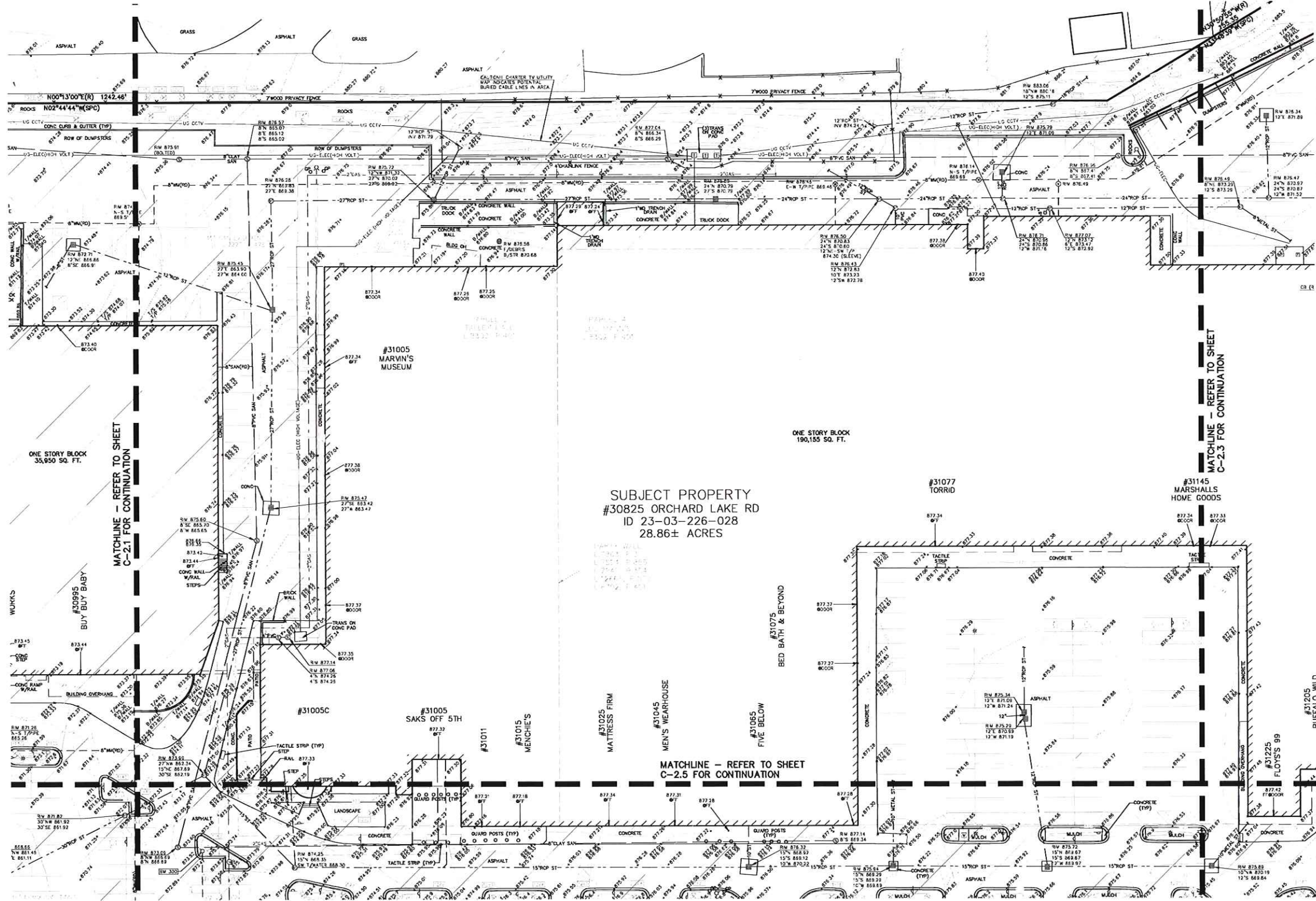
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PEA GROUP
t: 844.813.2949
www.peagroup.com

STATE OF MICHIGAN
TOOD D. SHELLY
PROFESSIONAL SURVEYOR
NO. 4001041111

Tood D. Shelly



LEGEND

● IRON FOUND	⊗ BRASS PIVOT SET	⊙ REC CORNER FOUND
⊗ IRON SET	⊗ MONUMENT FOUND	⊕ RECORDED
⊗ NAIL FOUND	⊗ MONUMENT SET	⊖ MEASURED
⊗ NAIL & CAP SET		⊘ CALCULATED

EXISTING

- ELEC—W—O— ELEC. PHONE OR CABLE TV OR LINE, POLE & CON. WIRE
- ELEC—C—E— UNDERGROUND CABLE TV OR CABLE
- ELEC—P—H—O— TELEPHONE OR CABLE, FIBER OPTIC & WANDS
- ELEC—G—E— ELECTRIC OR GAS CABLE, METER & WANDS
- ELEC—G—E— GAS MAIN, VALVE & GAS LINE WANDS
- ELEC—G—E— WATER MAIN, VALVE, TAPPING, BURNER & WANDS
- ELEC—G—E— SANITARY SEWER, CLEANOUT & WANDS
- ELEC—G—E— STORM SEWER, CLEANOUT & WANDS
- ELEC—G—E— SQUARE ROAD & BEEHIVE CAT-DRAIN, YARD DRAIN
- ELEC—G—E— WATER VALVE, BOILER, VALVE BOX, SERVICE SHUT-OFF
- ELEC—G—E— METER BOX, TRANSFORMER, RIGIDATION CONTROL VALVE
- ELEC—G—E— UNDERVISED STRUCTURE
- SPOT ELEVATION
- CONTOUR LINE
- FENCE
- GUARD RAIL
- STREET LIGHT
- SIGN

CONC CONCRETE

ASPH ASPHALT

SCALE
0 15 30 60
SCALE: 1" = 30'

811 Know what's below. Call before you dig.

CAUTION!!
THE LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES SHOWN ON THIS DRAWING ARE BASED ON RECORD DRAWINGS AND FIELD SURVEY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION AND ELEVATION PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

REFERENCE DRAWINGS

WATER MAIN	TRANSMISSION MAP BY G.W.A. APRIL 2018
WATER MAIN	TRANSMISSION MAP BY G.W.A. MARCH 2020
ELECTRIC	LOCATION MAP 28234, DATE JUNE 2023
GAS	LOCATION MAP 28234, DATE JUNE 2023
OTHER	HUNTER SQUARE LAND TITLE B.L.S. PEAGROUP #31-0252, MARCH 2021
OTHER	LOG-HOUSE WITH HUNTER SQUARE AS B.L.T.S. ORCHARD, FARM, IL172 & WILLOW, INC. AUGUST 1981, UPDATED 1983
OTHER	ORCHARD PLACE AS B.L.T.S. CHOWARD, FARM, IL172 & WILLOW, INC. ISSUED FEBRUARY 1983, UPDATED MAY 2025
OTHER	HUNTER SQUARE FARMING LOT AND DRIVEWAY RENOVATIONS AS B.L.T.S. CALDWELL, H.L.T. & MICHIGAN, INC. ISSUED SEPTEMBER 1983
OTHER	UPDATED MAY 2025
OTHER	ORCHARD PLACE RESUBDIVISION PROJECT AS B.L.T.S. OHN CONSULTING ENGINEERS, ISSUED OCTOBER 1985, UPDATED DECEMBER 2006
OTHER	HUNTER SQUARE B.L.T.S. OHN CONSULTING ENGINEERS, DECEMBER 2004
OTHER	ORCHARD LAKE ROAD RESUBDIVISION 13 MILE ROAD TO 14 MILE ROAD, H.L.T.C. OCTOBER 2014

SITE SHEET KEY MAP

CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1-SPADUP RESUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
TOPOGRAPHIC SURVEY - SHEET 2

PEA JOB NO. 22-0256
P.M. RLS
DN. KM
DES. PB
DRAWING NUMBER

C-2.2

NOT FOR CONSTRUCTION

BENCHMARKS
(GPS DERIVED - NAVD83)

BM #300
DAPPLE ON ARROW ON HYDRANT AND #109 DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

BM #301
DAPPLE ON ARROW ON HYDRANT AND #39 DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL #180 DIRECTLY SOUTH
FROM MCDONALD'S RESTAURANT
ELEV. - 875.55

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN AREAS
DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE
FLOODPLAIN (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 12505131 EFFECTIVE SEPTEMBER 28, 2006.

LEGAL DESCRIPTIONS:

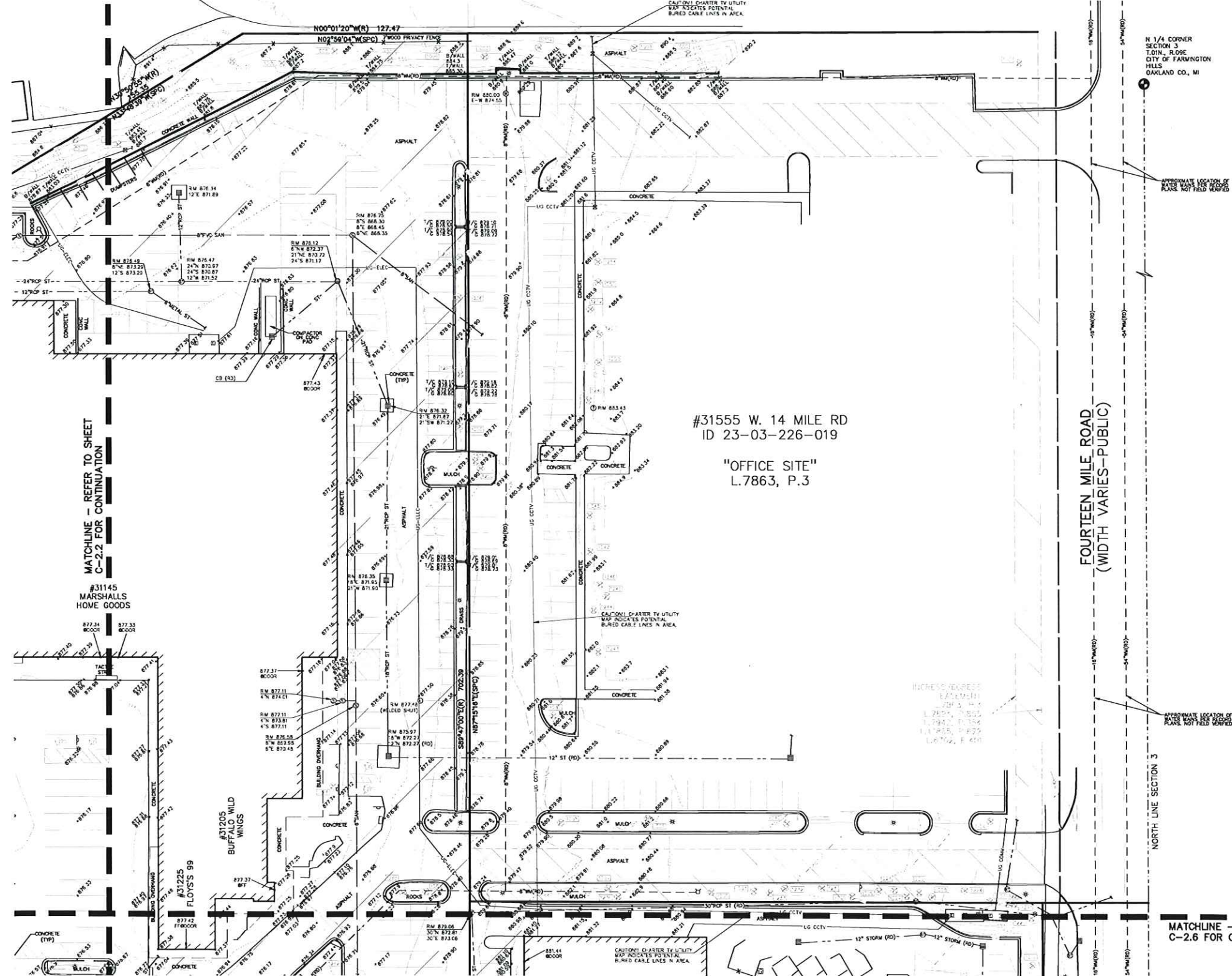
PARCEL 23-03-226-028
A PARCEL OF LAND BEING SITUATED IN THE NORTHEAST 1/4 OF SECTION 3, TOWN 1 NORTH, RANGE 9 EAST, CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 3 AND PROCEEDING THENCE ALONG THE EAST LINE OF SAID SECTION 3, AND THE CENTERLINE OF ORCHARD LAKE ROAD (120 FEET WIDE) SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 256.00 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 214.00 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 130.35 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 152.65 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST, 154.00 FEET TO THE WEST LINE OF ORCHARD LAKE ROAD; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 1350.35 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 775.70 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 00 SECONDS EAST, 1242.48 FEET; THENCE NORTH 30 DEGREES 50 MINUTES 55 SECONDS WEST 155.35 FEET; THENCE NORTH 00 DEGREES 01 MINUTES 20 SECONDS WEST 127.47 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST 702.39 FEET TO THE POINT OF BEGINNING, CONTAINING 4.28.58 ACRES.

PARCEL 23-03-226-027
A PARCEL OF LAND BEING SITUATED IN THE NORTHEAST 1/4 OF SECTION 3, TOWN 1 NORTH, RANGE 9 EAST, CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 3 AND PROCEEDING THENCE ALONG THE EAST LINE OF SAID SECTION 3, AND THE CENTERLINE OF ORCHARD LAKE ROAD (120 FEET WIDE) SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 256.00 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST 600.00 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST 154.00 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 130.35 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST 211.99 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 00 SECONDS EAST 324.57 FEET TO THE SOUTH LINE OF FOURTEEN MILE ROAD (WIDTH VARIES); THENCE ALONG SAID SOUTH LINE NORTH 89 DEGREES 58 MINUTES 40 SECONDS EAST 366.00 FEET TO THE WEST LINE OF SAID ORCHARD LAKE ROAD; THENCE ALONG SAID WEST LINE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 195.75 FEET TO THE POINT OF BEGINNING, CONTAINING 4.22.27 ACRES.

PEA GROUP
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TODD D. SHELLY
PROFESSIONAL SURVEYOR
NO. 400104111

Eric D. Shelly



LEGEND

● IRON FOUND	⊗ BRASS FLAG SET	⊙ REC. CORNER FOUND
⊗ IRON SET	⊗ MONUMENT FOUND	⊗ RECORDED
⊗ NAIL FOUND	⊗ MONUMENT SET	⊗ MEASURED
⊗ NAIL & CAP SET		⊗ CALCULATED

EXISTING

—ELEC—	ELEC. PHONE OR CABLE TV ON LINE, POLE & SPA WIRE
—ELEC—	UNDERGROUND CABLE TV, CATV, FIBER, ETC.
—ELEC—	TELEPHONE U.S. CABLE, FIBER, ETC. & WIRE
—ELEC—	ELECTRIC U.S. CABLE, WIRE, METER & WOOD POLE
—ELEC—	GAS MAIN, VALVE & GAS LINE & WIRE
—ELEC—	WATER MAIN, VALVE, TAPPING SLEEVES & WIRE
—ELEC—	SAFETY SPARK GUARDING & WIRE
—ELEC—	STORM SEWER, CLEAN OUT & WIRE
—ELEC—	SQUARE, ROUND & REEFER CATD-MAN, VANDERBILT
—ELEC—	WATER VALVE, REPAIR VALVE, SOX SERVICE & TOTT
—ELEC—	MILBROK TRANSFORMER, REGULATION CONTROL, VALVE
—ELEC—	UNDEVELOPED STRUCTURE
—ELEC—	SPOT ELEVATION
—ELEC—	CONTOUR LINE
—ELEC—	FENCE
—ELEC—	GUARD RAIL
—ELEC—	SPEED LIGHT
—ELEC—	SIGN
—ELEC—	CONCRETE
—ELEC—	ASPH.

0 15 30 60
SCALE: 1" = 30'

811
Know what's below.
Call before you dig.

CAUTION!
THE LOCATION AND DEPTHS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN ON THIS DRAWING AS BEST AS IS KNOWN BY THE SURVEYOR. THE USER OF THIS DRAWING SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTHS OF EXISTING UTILITIES PRIOR TO THE START OF CONSTRUCTION.



REFERENCE DRAWINGS

WATER MAIN TRANSMISSION MAP BY OLVA, APRIL 2018

WATER MAIN TRANSMISSION MAP BY OLVA, MARCH 2007

ELECTRIC LOCATION MAP 2021 SITE, JUNE 2021

TRANSMISSION MAP, CONSUMERS ENERGY, E-18-023-1

HUNTER SQUARE LAND TITLE & SURVEY, PEA GROUP #210202, MARCH 2021

LOD-MANN #2 AT HUNTER SQUARE AS BUILT, GRO-ARD, PAPER, HLTZ & WOLMONT, INC., JULY 1991, UPDATED 1999

OTHER GRO-ARD PLAZA AS BUILT, GRO-ARD, PAPER, HLTZ & WOLMONT, INC., ISSUED FEBRUARY 1991, UPDATED MAY 2008

OTHER GRO-ARD PLAZA PARKING LOT AND DRIVEWAY RENOVATIONS AS BUILT, GRO-ARD, HLTZ & WOLMONT, INC., ISSUED SEPTEMBER 1991

OTHER GRO-ARD PLAZA RENOVATION PROJECT AS BUILT, D-W CONSTRUCTION ENGINEERS, ISSUED OCTOBER 1988, UPDATED DECEMBER 2002

OTHER HUNTER SQUARE AS BUILT, D-W CONSTRUCTION ENGINEERS, DECEMBER 2004

OTHER GRO-ARD LAKE ROAD REHABILITATION 13 MILE ROAD TO 14 MILE ROAD, HRC INC., OCTOBER 2019

CLIENT

RPT REALTY
20750 CRYG CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN, 48034

PROJECT TITLE

HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN, 48334

REVISIONS

1 - SP4PUD RE-SUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE

TOPOGRAPHIC SURVEY - SHEET 3

PEA JOB NO. 22-0256
P.M. RLS
DN. KM
DES. PB
DRAWING NUMBER

C-2.3

NOT FOR CONSTRUCTION

BENCHMARKS
(GPS DERIVED - NAVD83)

BM #300
DUMPLE ON ARROW ON HYDRANT AND 1109' DIRECTLY NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE SIGN BUBBY.

BM #301
DUMPLE ON ARROW ON HYDRANT AND 139' DIRECTLY WEST FROM SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF SITE.

BM #302
LIGHT POLE BASE IN SHOPPING MALL 1150' DIRECTLY SOUTH FROM MCDONALD'S RESTAURANT.

LEGAL DESCRIPTIONS:

PARCEL 23-03-226-028
A PARCEL OF LAND BEING SITUATED IN THE NORTHEAST 1/4 OF SECTION 3, TOWN 1 NORTH, RANGE 9 EAST, CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 3 AND PROCEEDING THENCE ALONG THE EAST LINE OF SAID SECTION 3, AND THE CENTERLINE OF ORCHARD LAKE ROAD (120 FEET WIDE) SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 256.00 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 214.00 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 130.35 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 152.85 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST, 154.00 FEET TO THE WEST LINE OF ORCHARD LAKE ROAD; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 1350.35 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 775.70 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 00 SECONDS EAST, 1242.48 FEET; THENCE NORTH 30 DEGREES 50 MINUTES 55 SECONDS WEST 155.35 FEET; THENCE NORTH 00 DEGREES 01 MINUTES 20 SECONDS WEST 127.47 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST 702.39 FEET TO THE POINT OF BEGINNING. CONTAINING 2.26.58 ACRES.

PARCEL 23-03-226-027
A PARCEL OF LAND BEING SITUATED IN THE NORTHEAST 1/4 OF SECTION 3, TOWN 1 NORTH, RANGE 9 EAST, CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 3 AND PROCEEDING THENCE ALONG THE EAST LINE OF SAID SECTION 3, AND THE CENTERLINE OF ORCHARD LAKE ROAD (120 FEET WIDE) SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 256.00 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST 60.00 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST 154.00 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 130.35 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST 211.99 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 00 SECONDS EAST 324.57 FEET TO THE SOUTH LINE OF FOURTEEN MILE ROAD (WIDTH VARIES); THENCE ALONG SAID SOUTH LINE NORTH 89 DEGREES 58 MINUTES 40 SECONDS EAST 368.00 FEET TO THE WEST LINE OF SAID ORCHARD LAKE ROAD; THENCE ALONG SAID WEST LINE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 195.75 FEET TO THE POINT OF BEGINNING. CONTAINING 2.27 ACRES.

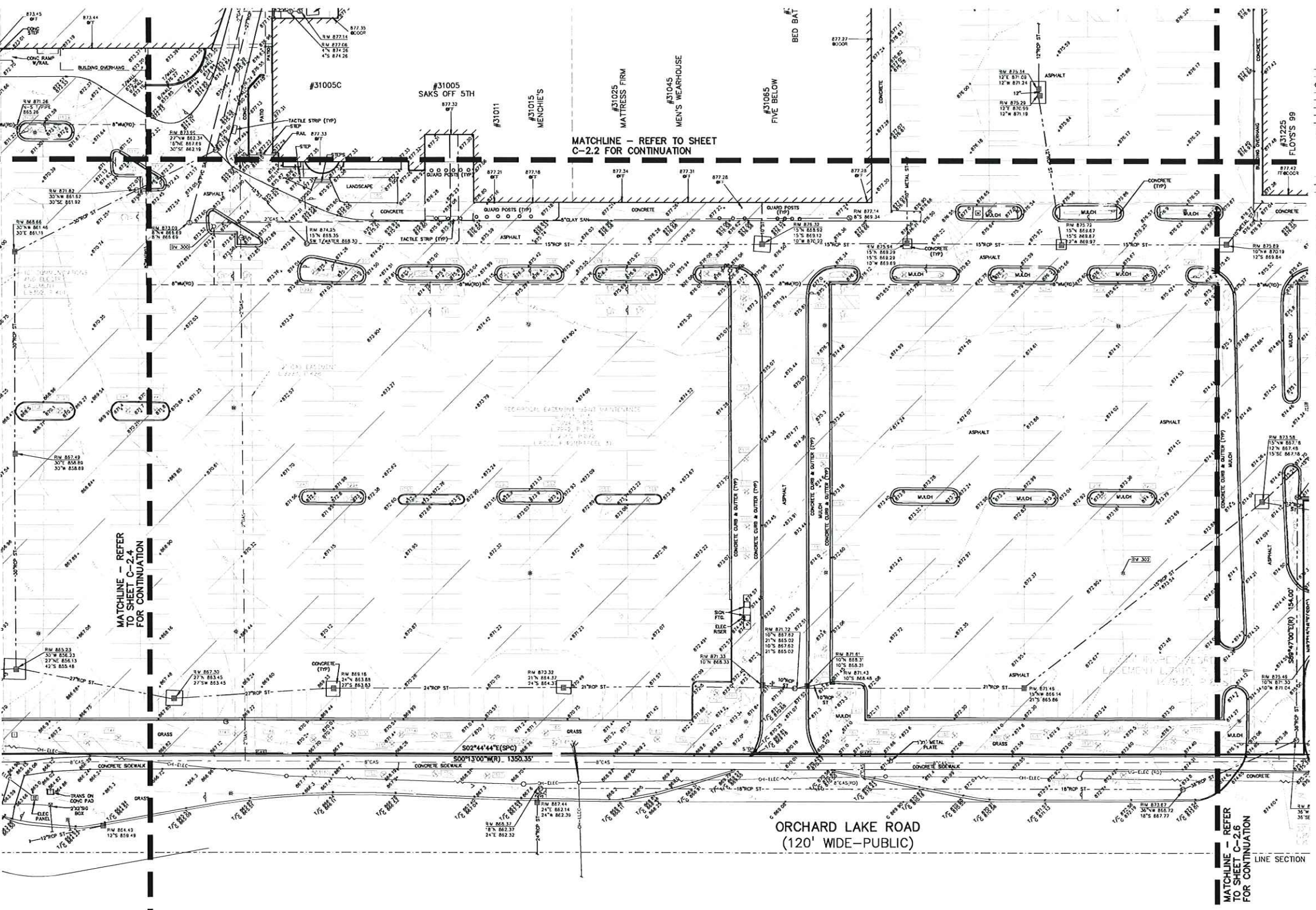
FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP PANEL NUMBER 1250513F EFFECTIVE SEPTEMBER 28, 2006.

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TODD D. SHELLY
PROFESSIONAL SURVEYOR
NO. 400194111

14 MILE ROAD
13 MILE ROAD
FARMINGTON ROAD
ORCHARD LAKE ROAD
NORTHWESTERN HWY

SITE



LEGEND

● IRON FOUND
○ IRON SET
✕ NAIL FOUND
✕ NAIL & CAP SET

● BRASS FLAG SET
○ MONUMENT FOUND
✕ MONUMENT SET

● REC. CORNER FOUND
○ RECORDED
✕ MEASURED
○ CALCULATED

EXISTING

○-ELEC-W-O- ELEC. PHONE CABLE ON LINE POLE & DWP WIRE
○-ELEC-CATV- CATV FEEDER
○-ELEC-P-CO-CO- TELEPHONE US CABLE FIBER & BUNDLED
○-ELEC-U-G-C-E- ELECTRIC U.S. CABLE, W/WHOLE METER & W/O OLE
○-ELEC-G-V-G-E- GAS MAIN, VALVE & GAS LINE & W/WHOLE
○-ELEC-W-V-G-E- WATER MAIN, VALVE, TAPPING, SERVICE & W/WHOLE
○-ELEC-S-W-C-E- SANITARY SEWER, CLEANOUT & W/WHOLE
○-ELEC-S-W-C-E- STORM SEWER, CLEANOUT & W/WHOLE
○-ELEC-S-W-C-E- SQUARE, ROAD & REELECTRIFIED CATCH-BASIN, VARDRAIN
○-ELEC-W-V-G-E- WATER VALVE, BOND, SHUT-OFF, VALVE BOX, SERVICE & W/WHOLE
○-ELEC-T-R-F- TRANSFORMER, REGULATOR, CONTROL, VALVE
○-ELEC-U-S- UNIDENTIFIED STRUCTURE
○-ELEC-F-E- SPLIT E. FEEDER
○-ELEC-S-E- SPLIT E. FEEDER

○ CONTOUR LINE
○ FENCE
○ CLAND. DIAL
○ STREET LIGHT
○ SIGN

○ CONC. CONCRETE
○ ASPH. ASPHALT

0 15 30 60
SCALE: 1" = 30'

811 Know what's below. Call before you dig.

CAUTION!
THE LOCATION AND DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS GIVEN OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE USER OF THIS DRAWING SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY UTILITY INFORMATION PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

REFERENCE DRAWINGS

WATER MAIN
WATER MAIN
ELECTRIC
GAS
OTHER
OTHER
OTHER
OTHER
OTHER
OTHER

TRANSMISSION MAP #1, OLIVE, APRIL 2018
TRANSMISSION MAP #1, OLIVE, MARCH 2007
LOCATION MAP #2018, SITE, JUNE 2018
TRANSMISSION MAP, CONSUMERS ENERGY, E-15-02-1
HUNTER SQUARE LAND TITLE SURVEY, PEABODY GROUP #200205, MARCH 2011
LOFT-MANN'S AT HUNTER SQUARE AS BLDG. ORCHARD PARK, HLTZ & WOLMONT, INC., AUGUST 1991, UNDATED 1991
ORCHARD PLACE AS BLDG. ORCHARD PARK, HLTZ & WOLMONT, INC., BEDES FLOOR PLAN UNDATED MAY 2005
HUNTER SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS BLDG. ORCHARD #18-12 & WOLMONT, INC., 18-03-03, SEPTEMBER 1992, UNDATED MAY 2005
ORCHARD PLACE REHABILITATION PROJECT AS BLDG. ORCHARD #18-12 & WOLMONT, INC., BEDES FLOOR PLAN, OCTOBER 1995, UNDATED DECEMBER 2002
HUNTER SQUARE LAND TITLE SURVEY, PEABODY GROUP #200205, MARCH 2011
ORCHARD LAKE ROAD REHABILITATION 13 MILE ROAD TO 14 MILE ROAD, HRC, INC., OCTOBER 2019

CLIENT

RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
BOUTTEFIELD, MICHIGAN, 48314

PROJECT TITLE

HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN, 48334

REVISIONS

1 - SP4PUD RESUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE

TOPOGRAPHIC SURVEY - SHEET 5

PEA JOB NO. 22-0256

P.M. RLS
DN. KM
DES. PB

DRAWING NUMBER

C-2.5

NOT FOR CONSTRUCTION

BENCHMARKS
(GPS DERIVED - NAVD83)

BM #300
EMPLE ON ARROW ON HYDRANT #109 DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

BM #301
EMPLE ON ARROW ON HYDRANT #139 DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL #167 DIRECTLY SOUTH
FROM McDONALDS RESTAURANT
ELEV. - 875.56

LEGAL DESCRIPTIONS:

PARCEL 23-03-226-028
A PARCEL OF LAND BEING SITUATED IN THE NORTHEAST 1/4 OF SECTION 3, TOWN 1 NORTH, RANGE 9 EAST, CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 3 AND PROCEEDING THENCE ALONG THE EAST LINE OF SAID SECTION 3, AND THE CENTERLINE OF ORCHARD LAKE ROAD (120 FEET WIDE) SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 258.00 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 214.00 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 130.35 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 152.85 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST, 154.00 FEET TO THE WEST LINE OF ORCHARD LAKE ROAD; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 1350.35 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 725.70 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 00 SECONDS EAST, 1242.46 FEET; THENCE NORTH 30 DEGREES 50 MINUTES 55 SECONDS WEST 155.35 FEET; THENCE NORTH 00 DEGREES 01 MINUTES 20 SECONDS WEST 127.47 FEET; THENCE SOUTH 89 DEGREES 47 MINUTES 00 SECONDS EAST 702.39 FEET TO THE POINT OF BEGINNING. CONTAINING ±26.58 ACRES

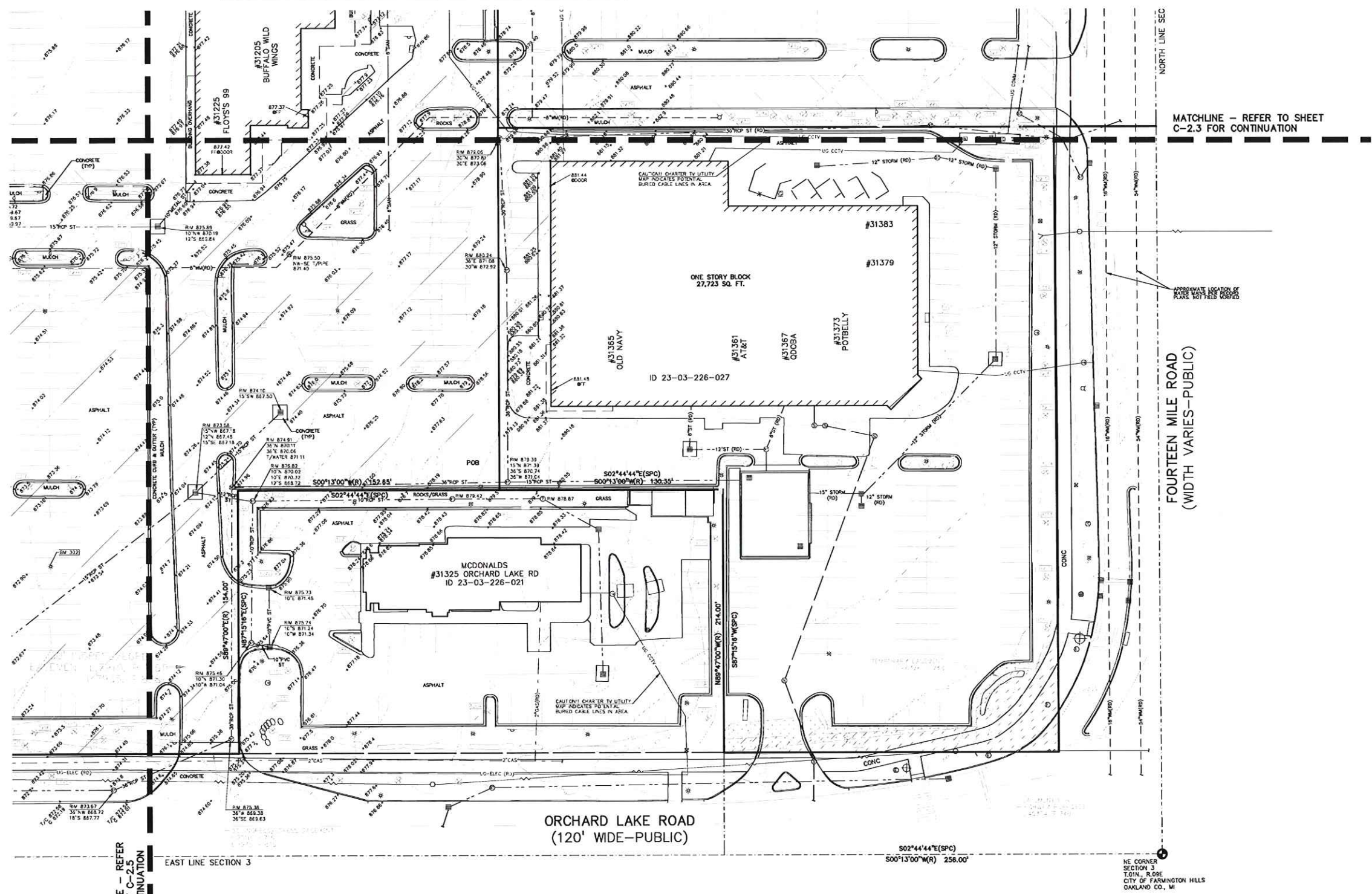
PARCEL 23-03-226-027
A PARCEL OF LAND BEING SITUATED IN THE NORTHEAST 1/4 OF SECTION 3, TOWN 1 NORTH, RANGE 9 EAST, CITY OF FARMINGTON HILLS, OAKLAND COUNTY, MICHIGAN, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCING AT THE NORTHEAST CORNER OF SAID SECTION 3 AND PROCEEDING THENCE ALONG THE EAST LINE OF SAID SECTION 3, AND THE CENTERLINE OF ORCHARD LAKE ROAD (120 FEET WIDE) SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 258.00 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST 60.00 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 154.00 FEET; THENCE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST, 130.35 FEET; THENCE NORTH 89 DEGREES 47 MINUTES 00 SECONDS WEST, 211.99 FEET; THENCE NORTH 00 DEGREES 13 MINUTES 00 SECONDS EAST 324.57 FEET TO THE SOUTH LINE OF FOURTEEN MILE ROAD (WIDTH VARIES); THENCE ALONG SAID SOUTH LINE NORTH 89 DEGREES 58 MINUTES 40 SECONDS EAST 368.00 FEET TO THE WEST LINE OF SAID ORCHARD LAKE ROAD; THENCE ALONG SAID WEST LINE SOUTH 00 DEGREES 13 MINUTES 00 SECONDS WEST 195.75 FEET TO THE POINT OF BEGINNING. CONTAINING ±2.27 ACRES

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP PANEL NUMBER 26125C0131 EFFECTIVE SEPTEMBER 29, 2006.

PEA GROUP
t: 844.813.2949
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STATE OF MICHIGAN
TOOD D. SHELLY
PROFESSIONAL SURVEYOR
NO. 4001041111

East D. Shelly



LEGEND

0 15 30 60
SCALE: 1" = 30'

811 Know what's below. Call before you dig.

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KEY MAP

CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MI 48334

REVISIONS

1 - SPAPUD RESUBMISSION	12/06/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
TOPOGRAPHIC SURVEY - SHEET 6

PEA JOB NO. 22-0256

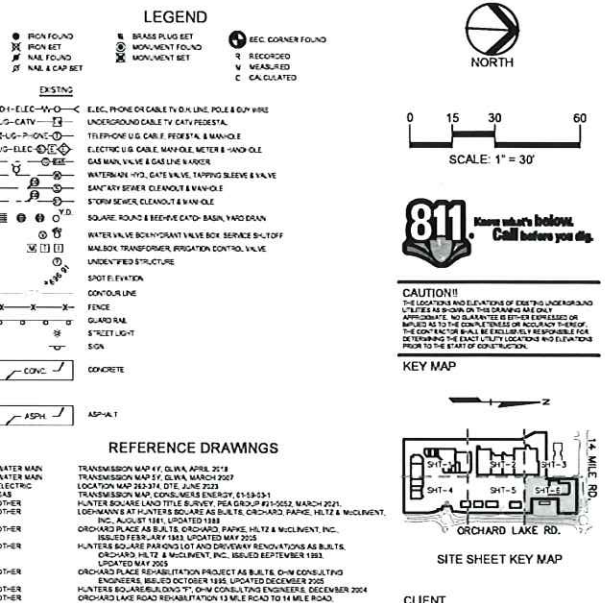
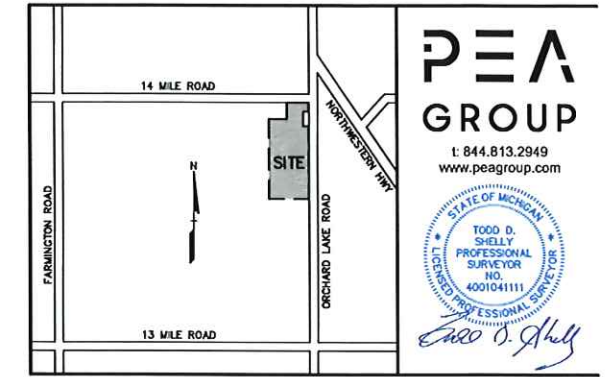
P.M.	RLS
DN.	KM
DES.	PB

DRAWING NUMBER

NOT FOR CONSTRUCTION **C-2.6**

Table with 11 columns: TAG NO., CODE, DBH, COMMON NAME, LATIN NAME, NOTES/Extra Trunk DBH, CONDITION/FACTOR (trunk, growth rate, structure, insects/diseases, crown dev, life exp), and OVERALL RANK. Lists various tree species like Norway Maple, Sugar Maple, and Red Cedar with their respective measurements and overall health ratings.

Table with 11 columns: TAG NO., CODE, DBH, COMMON NAME, LATIN NAME, NOTES/Extra Trunk DBH, CONDITION/FACTOR (trunk, growth rate, structure, insects/diseases, crown dev, life exp), and OVERALL RANK. Lists various tree species like Blue Spruce, Crab Apple, and Japanese zelkova with their respective measurements and overall health ratings.



CLIENT: RPT REALTY 2075 CIVIC CENTER DRIVE, SUITE #310, SOUTHFIELD, MI 48034

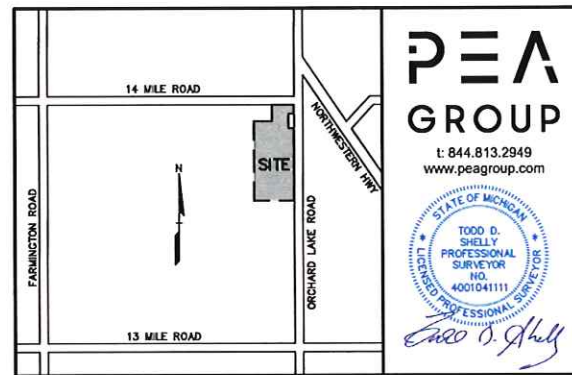
PROJECT TITLE: HUNTER'S SQUARE REDEVELOPMENT, ORCHARD LAKE ROAD AT 14 MILE ROAD, FARMINGTON HILLS, MI 48334

REVISIONS: 1 - SPAPUD RESUBMISSION, 12/06/23

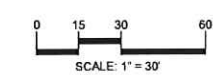
EXISTING TREE LIST - SHEET 1

PEA JOB NO. 22-0256, P.M. RLS, D.N. KM, DES. PB, DRAWING NUMBER: C-2.7

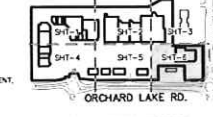
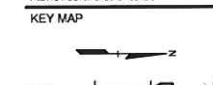
TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	NOTES/Extra Trunk DBH	CONDITION/FACTOR						OVERALL RANK
						trunk	growth rate	structure	insects/diseases	crown dev	life exp	
1200	CA	4	Crab Apple	Malus coronaria		2	2	2	4	2	3	15
1201	CA	7	Crab Apple	Malus coronaria		5	5	5	5	5	3	28
1202	NM	12	Norway Maple	Acer platanoides		5	5	5	5	5	3	28
1203	CA	6	Crab Apple	Malus coronaria		4	4	4	5	4	3	24
1204	NM	13	Norway Maple	Acer platanoides		5	5	5	5	5	3	28
1205	NS	12	Norway Spruce	Picea abies		4	5	4	4	3	3	23
1206	LL	10	Littleleaf Linden	Tilia cordata		5	5	4	5	4	3	26
1207	LL	9	Littleleaf Linden	Tilia cordata		5	5	5	5	4	3	27
1208	LL	7	Littleleaf Linden	Tilia cordata		5	5	5	5	4	3	27
1209	LL	9	Littleleaf Linden	Tilia cordata		5	4	4	5	4	3	25
1210	BP	5	Bradford Pear	Pyrus calleryana		5	4	4	5	4	3	25
1211	RM	5	Red Maple	Acer rubrum		5	5	5	5	4	3	27
1212	RM	6	Red Maple	Acer rubrum		5	5	5	5	5	3	28
1213	RM	5	Red Maple	Acer rubrum		4	5	5	5	4	3	26
1214	RM	8	Red Maple	Acer rubrum		5	5	5	5	5	3	28
1215	RM	4	Red Maple	Acer rubrum		5	5	5	5	4	3	27
1216	RM	5	Red Maple	Acer rubrum		5	5	5	5	4	3	27
1217	RM	5	Red Maple	Acer rubrum		5	5	5	5	5	3	28
1218	RM	4	Red Maple	Acer rubrum		5	5	5	5	5	3	28
1219	RM	6	Red Maple	Acer rubrum		5	5	5	5	5	3	28
1220	RC	5	Red Cedar	Juniperus virginiana	x3 4, 2, 1	5	5	5	5	5	3	28
1221	RC	4	Red Cedar	Juniperus virginiana	x2 1, 1	5	5	5	5	5	3	28
1222	RC	4	Red Cedar	Juniperus virginiana	x2 2, 2	5	5	5	5	5	3	28
1223	RC	3	Red Cedar	Juniperus virginiana	x1 2	5	5	5	5	5	3	28
1224	RP	14	Red Pine	Pinus resinosa		3	4	4	2	3	3	19
1225	RP	12	Red Pine	Pinus resinosa		5	5	5	5	5	3	28
1226	RP	12	Red Pine	Pinus resinosa		5	5	5	5	4	3	27
1227	RP	11	Red Pine	Pinus resinosa		5	5	4	5	4	3	26
1228	BP	5	Bradford Pear	Pyrus calleryana	x1 4	4	5	4	5	4	3	25
1229	BP	10	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1230	RP	9	Red Pine	Pinus resinosa		5	5	4	5	4	3	26
1231	RP	9	Red Pine	Pinus resinosa		5	5	5	5	5	3	28
1232	BP	11	Bradford Pear	Pyrus calleryana		4	5	5	5	4	3	26
1233	BP	10	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1234	BP	12	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1235	RP	10	Red Pine	Pinus resinosa		5	4	5	4	4	3	25
1236	BP	11	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1237	BP	13	Bradford Pear	Pyrus calleryana		4	5	4	5	4	3	25
1238	HL	4	Honeylocust	Gleditsia triacanthos		5	5	5	5	5	3	28
1239	HL	4	Honeylocust	Gleditsia triacanthos		5	5	5	5	4	3	27
1240	HL	4	Honeylocust	Gleditsia triacanthos		5	5	5	5	5	3	28
1241	NM	9	Norway Maple	Acer platanoides		5	5	5	5	5	3	28
1242	CA	7	Crab Apple	Malus coronaria	x2 7.5	5	5	5	5	5	4	29
1243	RB	5	River Birch	Betula nigra	x2 5.3	5	5	5	5	5	5	25
1244	SB	2	Service Berry	Amenlancher arborea	x9 2.2, 2.2, 1.1, 1.1	5	5	5	5	5	5	30
1245	SB	3	Service Berry	Amenlancher arborea	x10 2.2, 1.1, 1.1, 1.1, 1.1	4	5	5	5	5	5	29
1246	SB	2	Service Berry	Amenlancher arborea	x6 2.2, 2.2, 1.1	5	5	4	5	5	3	27
1247	RB	3	River Birch	Betula nigra	x2 3.2	5	5	5	5	5	3	28
1248	NM	16	Norway Maple	Acer platanoides		4	5	5	5	4	3	26
1249	NM	13	Norway Maple	Acer platanoides		5	5	5	5	5	3	28
1250	NM	13	Norway Maple	Acer platanoides		5	5	5	5	5	3	28
1251	RB	3	River Birch	Betula nigra	x2 2.2	5	5	5	5	5	3	28
1252	RB	3	River Birch	Betula nigra	x2 3.2	5	5	5	5	5	3	28
1253	RB	4	River Birch	Betula nigra	x1 3	5	5	5	5	5	5	30
1254	NM	17	Norway Maple	Acer platanoides		5	5	5	5	5	3	28
1255	RM	5	Red Maple	Acer rubrum	DEAD							
1256	RM	5	Red Maple	Acer rubrum	DEAD							
1257	RM	5	Red Maple	Acer rubrum	DEAD							
1258	RM	3	Red Maple	Acer rubrum	DEAD							
1259	RP	10	Red Pine	Pinus resinosa		5	4	4	5	3	3	24
1260	RP	10	Red Pine	Pinus resinosa		5	5	5	5	4	3	27
1261	HL	6	Honeylocust	Gleditsia triacanthos		5	5	5	5	4	3	27
1262	HL	6	Honeylocust	Gleditsia triacanthos		5	5	5	5	5	3	28
1263	NM	8	Norway Maple	Acer platanoides		5	5	5	5	4	3	27
1264	HL	7	Honeylocust	Gleditsia triacanthos		5	5	5	5	5	3	28
1265	RB	2	River Birch	Betula nigra	x1	5	5	5	5	5	3	28
1266	RB	3	River Birch	Betula nigra	x4 2.2, 2.1	5	5	5	5	5	3	28
1267	BP	4	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1268	BP	4	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1269	BP	4	Bradford Pear	Pyrus calleryana		4	4	4	4	3	3	23
1270	BP	4	Bradford Pear	Pyrus calleryana		5	5	4	5	4	3	26
1271	CA	6	Crab Apple	Malus coronaria		4	4	4	4	4	3	23
1272	NM	13	Norway Maple	Acer platanoides		5	5	5	5	5	3	28
1273	WS	9	White Spruce	Picea glauca		4	4	4	5	3	3	23
1274	NM	12	Norway Maple	Acer platanoides		4	4	5	5	4	3	25
1275	HL	6	Honeylocust	Gleditsia triacanthos		3	3	3	3	3	3	18
1276	HL	5	Honeylocust	Gleditsia triacanthos		3	3	3	4	3	3	19
1277	BP	4	Bradford Pear	Pyrus calleryana		5	5	4	5	4	3	26
1278	BP	5	Bradford Pear	Pyrus calleryana		5	5	4	5	4	3	26
1279	BP	4	Bradford Pear	Pyrus calleryana		5	5	5	4	4	3	26
1280	BP	4	Bradford Pear	Pyrus calleryana		4	4	4	5	5	3	25
1281	BP	3	Bradford Pear	Pyrus calleryana		5	4	4	5	4	3	25
1282	BP	4	Bradford Pear	Pyrus calleryana		5	4	4	4	4	3	24
1283	BP	4	Bradford Pear	Pyrus calleryana		5	4	4	5	4	3	25
1284	BP	4	Bradford Pear	Pyrus calleryana		5	5	4	5	4	3	26
1285	BP	4	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1286	BP	5	Bradford Pear	Pyrus calleryana		5	4	4	5	4	3	25
1287	BP	4	Bradford Pear	Pyrus calleryana		4	5	4	5	4	3	25
1288	BP	4	Bradford Pear	Pyrus calleryana	x1 4	5	5	5	5	4	3	27
1289	BP	3	Bradford Pear	Pyrus calleryana		5	4	4	5	3	3	24
1290	CA	4	Crab Apple	Malus coronaria		5	5	5	5	5	3	28
1291	BP	5	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1292	BP	4	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1293	BP	2	Bradford Pear	Pyrus calleryana		5	4	4	5	4	3	25
1294	BP	3	Bradford Pear	Pyrus calleryana		5	5	5	5	4	3	27
1295	BP	3	Bradford Pear	Pyrus calleryana		5	4	4	5	3	3	24
1296	BP	3	Bradford Pear	Pyrus calleryana		5	4	4	5	4	3	25
1297	BP	3	Bradford Pear	Pyrus calleryana		5	5	4	5	4	3	26
1298	BP	2	Bradford Pear	Pyrus calleryana	DEAD							
1299	BP	3	Bradford Pear	Pyrus calleryana	DEAD							



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CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48074

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS	
1 - SPA/PLD RESUBMISSION	12/08/23

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
EXISTING TREE LIST - SHEET 2

PEA JOB NO. 22-0256
P.M. RLS
DN. KM
DES. PB
DRAWING NUMBER

GENERAL NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

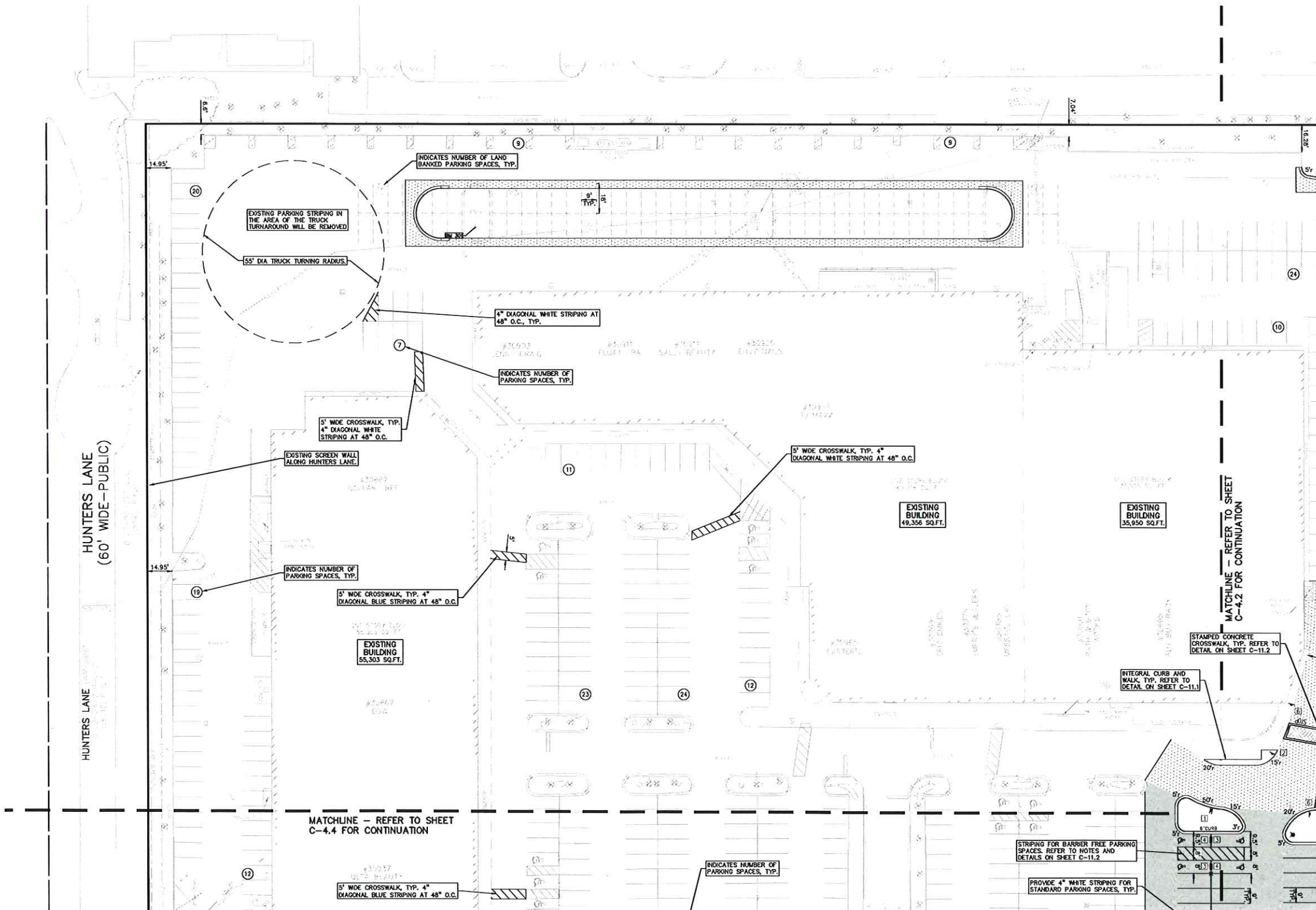
1. ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, FACE OF SIDEWALK, OUTSIDE FACE OF BUILDING, PROPERTY LINE, CENTER OF MANHOLE/CATCH BASIN OR CENTERLINE OF PIPE UNLESS OTHERWISE NOTED.
2. "NO PARKING-FIRE LANE" SIGNS SHALL BE POSTED ALONG ALL FIRE LANES AT 125' FOOT INTERVALS OR AS DIRECTED BY THE FIRE OFFICIAL.
3. REFER TO NOTES & DETAILS SHEET FOR ON-SITE PAVING DETAILS.
4. REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

CITY OF FARMINGTON HILLS STANDARD NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

1. NOTIFY THE CITY OF FARMINGTON HILLS ENGINEERING DIVISION AT 248.871.2560 A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
2. ALL CONSTRUCTION MUST BE CONFORMING TO THE CURRENT STANDARDS AND SPECIFICATIONS ADOPTED BY THE CITY OF FARMINGTON HILLS.
3. UTILITIES TO BE UNDERGROUND.
4. CALL MISS DIG 1.800.647.7344 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
5. ALL SOIL EROSION AND SILT MUST BE CONTROLLED AND CONTAINED ON SITE.
6. ALL EXCAVATION UNDER OR WITHIN 3 FEET OF PUBLIC PAVEMENT, EXISTING OR PROPOSED, SHALL BE BACKFILLED AND COMPACTED WITH SAND (CLASS II MDOT).
7. THE CONTRACTOR IS RESPONSIBLE FOR ALL DAMAGE TO EXISTING UTILITIES.
8. PRIOR TO THE ISSUANCE OF AN OCCUPANCY PERMIT, ENGINEERING SITE INSPECTION IS REQUIRED.

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 www.peagroup.com

STATE OF MICHIGAN
 RACHEL SMITH
 LICENSE NO. 5201048828
 PROFESSIONAL ENGINEER



LEGEND

● IRON FOUND	⊗ BRASS PLUG SET	⊙ REC. CORNER FOUND
⊗ IRON SET	⊗ MONUMENT FOUND	⊙ RECORDED
⊗ NAIL FOUND	⊗ MONUMENT SET	⊙ MEASURED
⊗ NAIL & CAP SET		⊙ CALCULATED

EXISTING

- 01-ELEC-W-C-ELEC. PHONE CABLE TO OH LINE POLE & O.P. WIRE
- 02-CATV-CATV UNDERGROUND CABLE TV CATV FEEDST. A
- 03-FC-FC TELEPHONE CABLE FEEDER & BUNDLE
- 04-ELEC-ELEC. ELECTRIC CABLE MANHOLE METER & W/O METER
- 05-GAS-GAS GAS MAN. VALVE & GAS LINE W/ METER
- 06-WATER-WATER WATER METER, GATE VALVE, TAPPING SLEEVE & VALVE
- 07-SAN-SAN SANITARY SEWER, CLEANOUT & MAN-OF
- 08-FIRE-FIRE 4" FIRE MAIN, CLEANOUT & MANHOLE
- 09-SQ-SQ SQUARE, ROUND & REEFER CATD. MANHOLE, VANDERBILT
- 10-WATER-WATER WATER VALVE, ROOM, COAST VALVE BOX, SERVICE, SHUT-OFF
- 11-MBX-MBX MAILBOX, TRANSFORMER, REGULATION CONTROL VALVE
- 12-UNDE-UNDE UNDEVELOPED STRUCTURE
- 13-SPOT-SPOT SPOT ELEVATION
- 14-CONC-CONC CONTOUR LINE
- 15-FENCE-FENCE FENCE
- 16-GLD-GLD GLASS GULCH
- 17-5' LIGHT-5' LIGHT 5' LIGHT
- 18-SON-SON SON
- 19-CONC-CONC CONCRETE
- 20-ASPH-ASPH ASPHALT

PROPOSED

- 01-ELEC-ELEC. ELECTRIC CABLE TO OH LINE POLE & O.P. WIRE
- 02-CATV-CATV UNDERGROUND CABLE TV CATV FEEDST. A
- 03-FC-FC TELEPHONE CABLE FEEDER & BUNDLE
- 04-ELEC-ELEC. ELECTRIC CABLE MANHOLE METER & W/O METER
- 05-GAS-GAS GAS MAN. VALVE & GAS LINE W/ METER
- 06-WATER-WATER WATER METER, GATE VALVE, TAPPING SLEEVE & VALVE
- 07-SAN-SAN SANITARY SEWER, CLEANOUT & MAN-OF
- 08-FIRE-FIRE 4" FIRE MAIN, CLEANOUT & MANHOLE
- 09-SQ-SQ SQUARE, ROUND & REEFER CATD. MANHOLE, VANDERBILT
- 10-WATER-WATER WATER VALVE, ROOM, COAST VALVE BOX, SERVICE, SHUT-OFF
- 11-MBX-MBX MAILBOX, TRANSFORMER, REGULATION CONTROL VALVE
- 12-UNDE-UNDE UNDEVELOPED STRUCTURE
- 13-SPOT-SPOT SPOT ELEVATION
- 14-CONC-CONC CONTOUR LINE
- 15-FENCE-FENCE FENCE
- 16-GLD-GLD GLASS GULCH
- 17-5' LIGHT-5' LIGHT 5' LIGHT
- 18-SON-SON SON
- 19-CONC-CONC CONCRETE
- 20-ASPH-ASPH ASPHALT

REFERENCE DRAWINGS

- WATER MAIN TRANSMISSION MAP #1, OLGA, APRIL 2018
- WATER MAIN TRANSMISSION MAP #1, OLGA, MARCH 2007
- GAS TRANSMISSION MAP, COVINGTON ENERGY, 11/18/2011
- LOCATION MAP #20314, DATE: 1/24/2011
- OTHER HUNTERS SQUARE LAND TITLE SURVEY, PEA GROUP #12/2023, MARCH 2021
- OTHER LUD-MANN AT HUNTERS SQUARE AS B.L.T.S. ORCHARD PARK, HL72 & WILMONT, INC. AUGUST 1981, UPDATED MAY 2008
- OTHER ORCHARD PLACE AS B.L.T.S. ORCHARD PARK, HL72 & WILMONT, INC. AUGUST 1981, UPDATED MAY 2008
- OTHER HUNTERS SQUARE PARKING LOT AND DRIVEWAY RECONSTRUCTIONS AS B.L.T.S. ORCHARD PLACE & WILMONT, INC. ISSUED SEPTEMBER 1981, UPDATED MAY 2005
- OTHER ORCHARD PLACE RECONSTRUCTION PROJECT AS B.L.T.S. ORCHARD PLACE & WILMONT, INC. ENGINEERS, ISSUED OCTOBER 1985, UPDATED DECEMBER 2005
- OTHER HUNTERS SQUARE BUILDING "D" ON CORNER THIS ENGINEER, DECEMBER 2004
- OTHER ORCHARD LAKE ROAD RECONSTRUCTION 13 MILE ROAD TO 14 MILE ROAD, HMC INC, OCTOBER 2019

KEY MAP

CAUTION!
 THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS GIVEN OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND ELEVATIONS OF EXISTING UTILITIES PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

SITE SHEET KEY MAP

- SIDEWALK RAMP LEGEND:**
- SIDEWALK RAMP "TYPE D" (1)
 - SIDEWALK RAMP "TYPE F" (2)
 - SIDEWALK RAMP "TYPE P" (3)
 - SIDEWALK RAMP "TYPE R" (4)
 - CURB DROP ONLY (5)
- REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.
- SIGN LEGEND:**
- "NO PARKING FIRE LANE" SIGN (1)
 - "STOP" SIGN (2)
 - "BARRIER FREE PARKING" SIGN (3)
 - "VAN ACCESSIBLE" SIGN (4)
 - "ELECTRIC VEHICLE PARKING" SIGN (5)
 - "CROSSWALK" SIGN (6)
 - "DO NOT ENTER" SIGN (7)
 - "ONE WAY" SIGN (8)
 - "PICK UP" SIGNS (NUMBERED) (9)
- REFER TO DETAIL SHEET FOR SIGN DETAILS.

CLIENT
RPT REALTY
 20750 CYMG CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN, 48033

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SP4PLUD RESUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
DIMENSION AND PAVING PLAN - SHEET 1

PEA JOB NO.	22-0256
P.M.	RLS
DN.	KM
DES.	PB
DRAWING NUMBER	

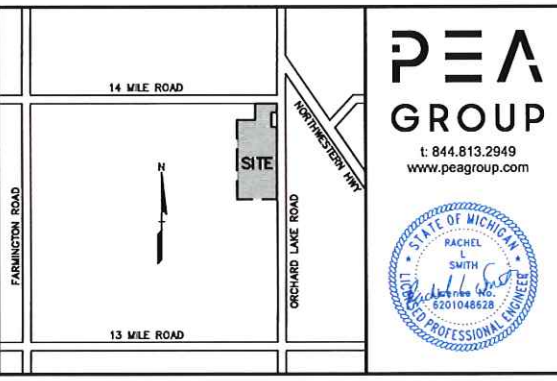
NOT FOR CONSTRUCTION **C-4.1**

GENERAL NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

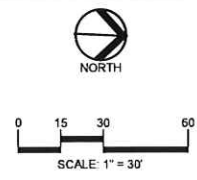
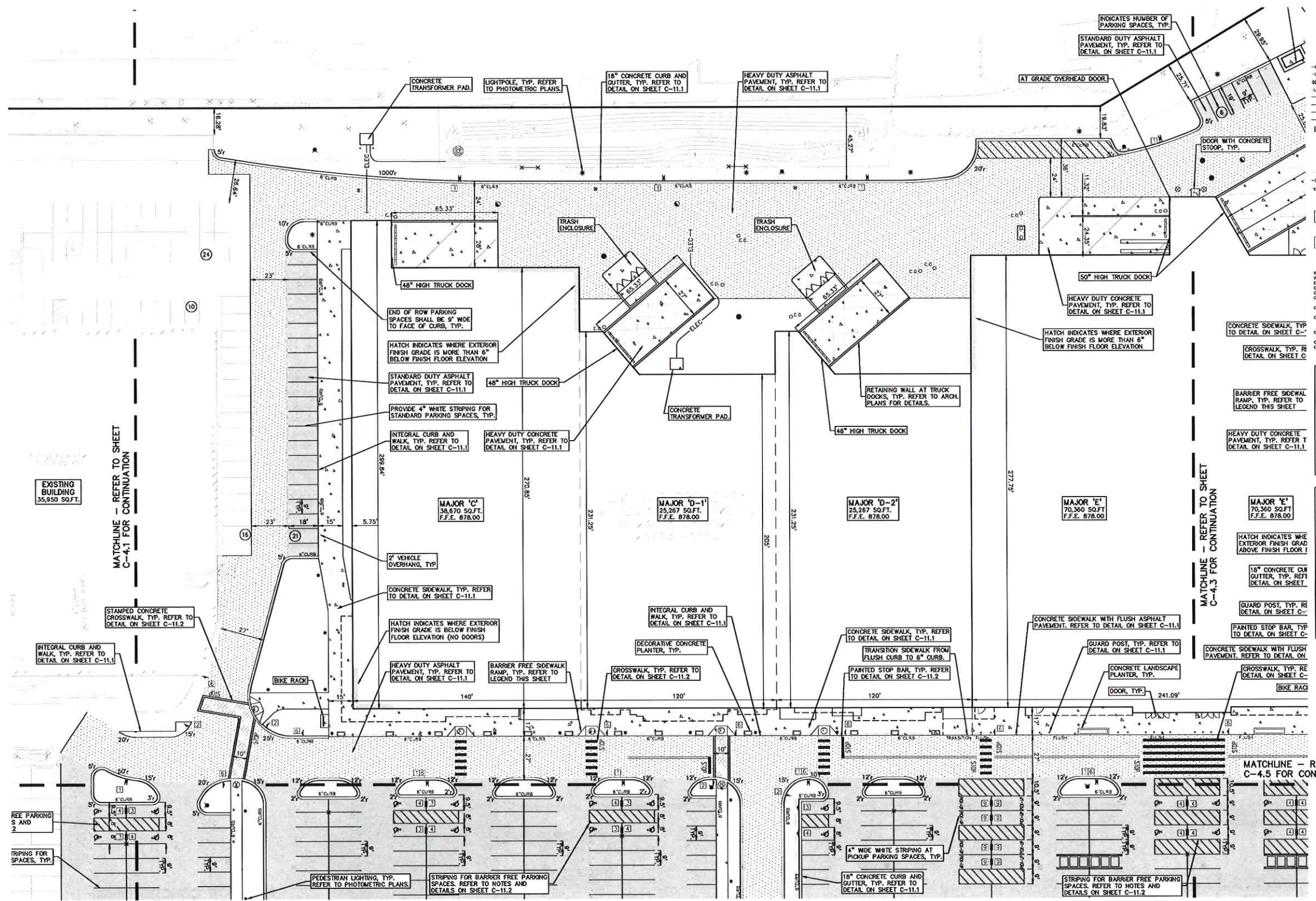
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- REFERENCE DRAWINGS:**
- WATER MAIN TRANSMISSION MAP BY G.W.A. APRIL 2018
 - SEWER MAIN TRANSMISSION MAP BY G.W.A. MARCH 2005
 - ELECTRIC LOCATION MAP BY S.P.A. DTE JUNE 2023
 - OTHER GARDEN CITY CENTER DEVELOPMENT #1818201
 - OTHER HUNTER'S SQUARE LAND TITLE & SURVEY, PEA GROUP #11-0052, MARCH 2011.
 - OTHER HOUGHMAN HILL CENTER DEVELOPMENT #1818201
 - OTHER INC. AUGUST 1981, UPDATED 1988
 - OTHER ORCHARD LAKE ROAD REHABILITATION PROJECT AS BUILT, G.W.A. & WIGLINT, INC. ISSUED FEBRUARY 1981, UPDATED MAY 2016
 - OTHER ORCHARD LAKE ROAD REHABILITATION PROJECT AS BUILT, G.W.A. & WIGLINT, INC. ISSUED SEPTEMBER 1981, UPDATED MAY 2016
 - OTHER ORCHARD LAKE ROAD REHABILITATION PROJECT AS BUILT, G.W.A. & WIGLINT, INC. ISSUED SEPTEMBER 1981, UPDATED MAY 2016
 - OTHER HUNTER'S SQUARE PHASE 1B & 1C, G.W.A. & WIGLINT, INC. ISSUED DECEMBER 2014
 - OTHER HUNTER'S SQUARE PHASE 1B & 1C, G.W.A. & WIGLINT, INC. ISSUED DECEMBER 2014
 - OTHER ORCHARD LAKE ROAD REHABILITATION 13 MILE ROAD TO 14 MILE ROAD, HRC, INC. OCTOBER 2019

- SIDEWALK RAMP LEGEND:**
- SIDEWALK RAMP 'TYPE D'
 - SIDEWALK RAMP 'TYPE F'
 - SIDEWALK RAMP 'TYPE P'
 - SIDEWALK RAMP 'TYPE R'
 - CURB DROP ONLY
- REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

- SIGN LEGEND:**
- 'NO PARKING FIRE LANE' SIGN
 - 'STOP' SIGN
 - 'BARRIER FREE PARKING' SIGN
 - 'VAN ACCESSIBLE' SIGN
 - 'ELECTRIC VEHICLE PARKING' SIGN
 - 'CROSSWALK' SIGN
 - 'DO NOT ENTER' SIGN
 - 'ONE WAY' SIGN
 - 'PICK UP' SIGNS (NUMBERED)
- REFER TO DETAIL SHEET FOR SIGN DETAILS.

CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN, 48064

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

NO.	REVISION	DATE
1-	SPAPUD RESUBMISSION	12/08/23

ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
DIMENSION AND PAVING PLAN - SHEET 2

PEA JOB NO. 22-0256

DESIGNER	DATE
P.M.	RLS
DN.	KM
DES.	PB

DRAWING NUMBER

NOT FOR CONSTRUCTION **C-4.2**

GENERAL NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

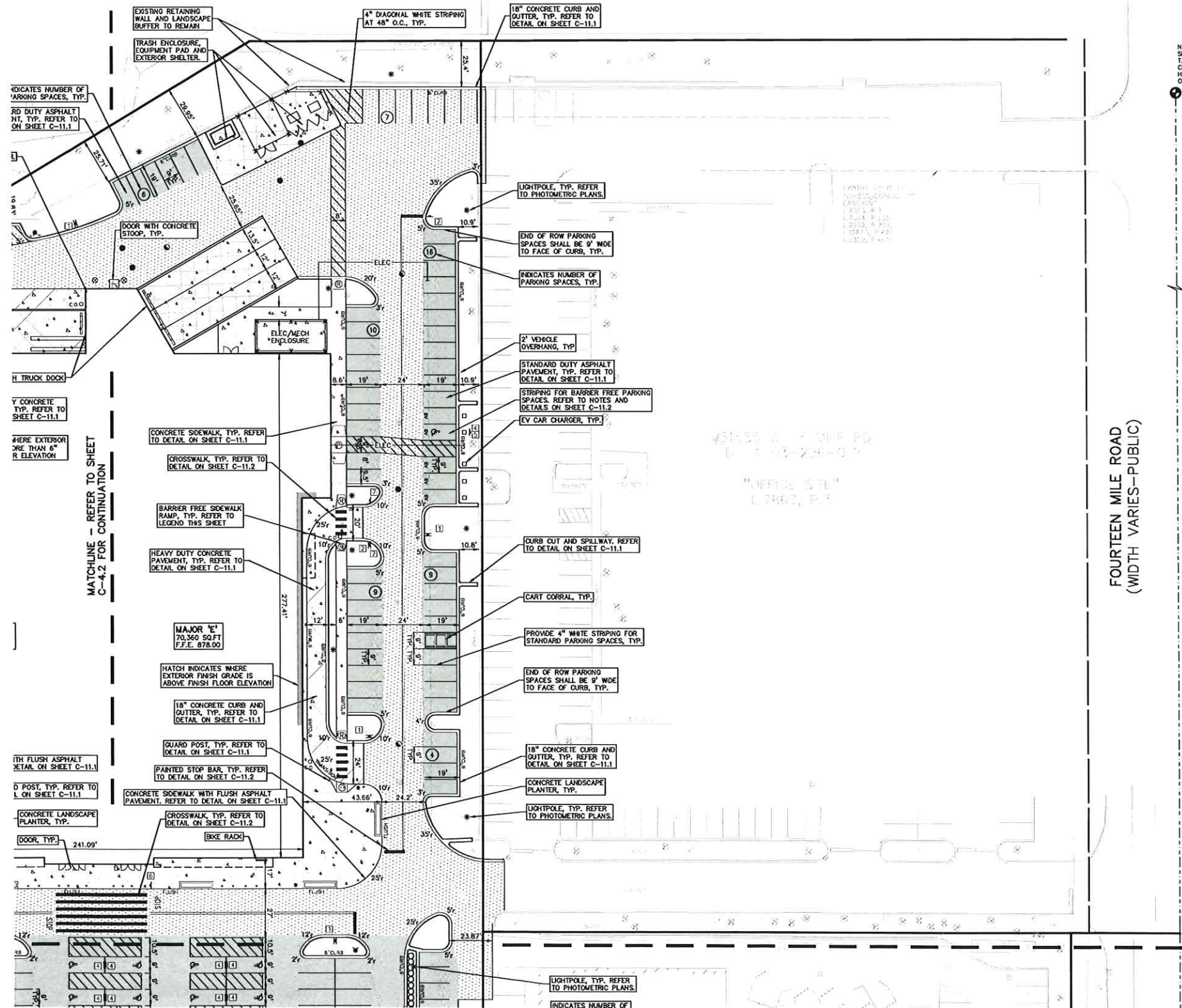
1. ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, FACE OF SIDEWALK, OUTSIDE FACE OF BUILDING, PROPERTY LINE, CENTER OF MANHOLE/CATCH BASIN OR CENTERLINE OF PIPE UNLESS OTHERWISE NOTED.
2. 'NO PARKING-FIRE LANE' SIGNS SHALL BE POSTED ALONG ALL FIRE LANES AT 125' FOOT INTERVALS OR AS DIRECTED BY THE FIRE OFFICIAL.
3. REFER TO NOTES & DETAILS SHEET FOR ON-SITE PAVING DETAILS.
4. REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

CITY OF FARMINGTON HILLS STANDARD NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

1. NOTIFY THE CITY OF FARMINGTON HILLS ENGINEERING DIVISION AT 248.871.2560 A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
2. ALL CONSTRUCTION MUST BE CONFORMING TO THE CURRENT STANDARDS AND SPECIFICATIONS ADOPTED BY THE CITY OF FARMINGTON HILLS.
3. UTILITIES TO BE UNDERGROUND.
4. CALL MISS DIO 1.800.647.7344 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
5. ALL SOIL EROSION AND SILT MUST BE CONTROLLED AND CONTAINED ON SITE.
6. ALL EXCAVATION UNDER OR WITHIN 3 FEET OF PUBLIC PAVEMENT, EXISTING OR PROPOSED, SHALL BE BACKFILLED AND COMPACTED WITH SAND (CLASS II MDOT).
7. THE CONTRACTOR IS RESPONSIBLE FOR ALL DAMAGE TO EXISTING UTILITIES.
8. PRIOR TO THE ISSUANCE OF AN OCCUPANCY PERMIT, ENGINEERING SITE INSPECTION IS REQUIRED.

PEA GROUP
 t: 844.813.2949
 www.peagroup.com

STATE OF MICHIGAN
 RACHEL SMITH
 LICENSE NO. 6201048528
 PROFESSIONAL ENGINEER



N 1/4 CORNER
 SECTION 3
 T.10N., R.09E
 CITY OF FARMINGTON
 HILLS
 OAKLAND CO., MI

FOURTEEN MILE ROAD
 (WIDTH VARIES-PUBLIC)

NORTH LINE SECTION 3

MATCHLINE - REFER TO SHEET
 C-4.6 FOR CONTINUATION

LEGEND

EXISTING	PROPOSED
● IRON FOUND	● CONC. CORNER FOUND
○ IRON SET	○ MONUMENT FOUND
■ NAIL FOUND	■ RECORDED
▲ NAIL & CAP SET	▲ MEASURED
	▲ CALCULATED

REFERENCE DRAWINGS

WATER MAIN	TRANSMISSION MAP BY G.W.A. APRIL 2018
WATER MAIN	TRANSMISSION MAP BY G.W.A. MARCH 2007
ELECTRIC	LOCATION MAP 2018 BY DTE, JUNE 2018
GAS	TRANSMISSION MAP CONSUMERS ENERGY, E158251
OTHER	HUNTER SQUARE LAND TITLE SURVEY PRO GROUP #10223 MARCH 2017
OTHER	LOE-MANN'S AT HUNTER SQUARE AS B.L.T.E. ORCHARD PARK, HLTZ & WOLMONT, INC. 07/18/2018
OTHER	ORCHARD PLACE AS B.L.T.E. ORCHARD PARK, HLTZ & WOLMONT, INC. 07/18/2018
OTHER	HUNTER SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS B.L.T.E. ORCHARD PLACE, HLTZ & WOLMONT, INC. 08/02/2018
OTHER	UPDATED MAY 2025
OTHER	ORCHARD PLACE REHABILITATION PROJECT AS B.L.T.E. ORCHARD PLACE, HLTZ & WOLMONT, INC. 08/02/2018
OTHER	ENGINEER, ISSUED OCTOBER 1988, UPDATED DECEMBER 2005
OTHER	HUNTER SQUARE LAND TITLE SURVEY PRO GROUP #10223 MARCH 2017
OTHER	ORCHARD LAKE ROAD REHABILITATION 14 MILE ROAD TO 14 MILE ROAD, HMC INC. OCTOBER 2019

KEY MAP

CAUTION!
 THE LOCATION AND DEPTHS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS GIVEN OR IMPLIED AS TO THE CORRECTNESS OR ACCURACY THEREOF. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND VERIFYING THE LOCATION AND DEPTHS OF EXISTING UTILITIES PRIOR TO THE START OF CONSTRUCTION.

811 Know what's below. Call before you dig.

KEY MAP

CLIENT
RPT REALTY
 20750 CYG CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN, 48075

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN, 48334

REVISIONS

NO.	DESCRIPTION	DATE
1	SPAFUD RESUBMISSION	12/08/23

ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
DIMENSION AND PAVING PLAN - SHEET 3

PEA JOB NO. 22-0256
P.M. RLS
DN. KM
DES. PB
DRAWING NUMBER

C-4.3

NOT FOR CONSTRUCTION

GENERAL NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

1. ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, FACE OF SIDEWALK, OUTSIDE FACE OF BUILDING, PROPERTY LINE, CENTER OF MANHOLE/CATCH BASIN OR CENTERLINE OF PIPE UNLESS OTHERWISE NOTED.
2. "NO PARKING-FIRE LANE" SIGNS SHALL BE POSTED ALONG ALL FIRE LANES AT 125' FOOT INTERVALS OR AS DIRECTED BY THE FIRE OFFICIAL.
3. REFER TO NOTES & DETAILS SHEET FOR ON-SITE PAVING DETAILS.
4. REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

CITY OF FARMINGTON HILLS STANDARD NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

1. NOTIFY THE CITY OF FARMINGTON HILLS ENGINEERING DIVISION AT 248.871.2560 A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
2. ALL CONSTRUCTION MUST BE CONFORMING TO THE CURRENT STANDARDS AND SPECIFICATIONS ADOPTED BY THE CITY OF FARMINGTON HILLS.
3. UTILITIES TO BE UNDERGROUND.
4. CALL MISS DIG 1.800.647.7344 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
5. ALL SOIL EROSION AND SILT MUST BE CONTROLLED AND CONTAINED ON SITE.
6. ALL EXCAVATION UNDER OR WITHIN 3 FEET OF PUBLIC PAVEMENT, EXISTING OR PROPOSED, SHALL BE BACKFILLED AND COMPACTED WITH SAND (CLASS II MDOT).
7. THE CONTRACTOR IS RESPONSIBLE FOR ALL DAMAGE TO EXISTING UTILITIES.
8. PRIOR TO THE ISSUANCE OF AN OCCUPANCY PERMIT, ENGINEERING SITE INSPECTION IS REQUIRED.

14 MILE ROAD
 13 MILE ROAD
 FARMINGTON ROAD
 ORCHARD LAKE ROAD
 NORTHWESTERN HWY

PEA GROUP
 t: 844.813.2949
 www.peagroup.com

STATE OF MICHIGAN
 RACHEL L. SMITH
 LICENSED PROFESSIONAL ENGINEER
 5201048628

0 15 30 60
 SCALE: 1" = 30'

811 Know what's below. Call before you dig.

CAUTION!!
 THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN FOR INFORMATION ONLY. THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 BLOOMFIELD HILLS, MICHIGAN 48304

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

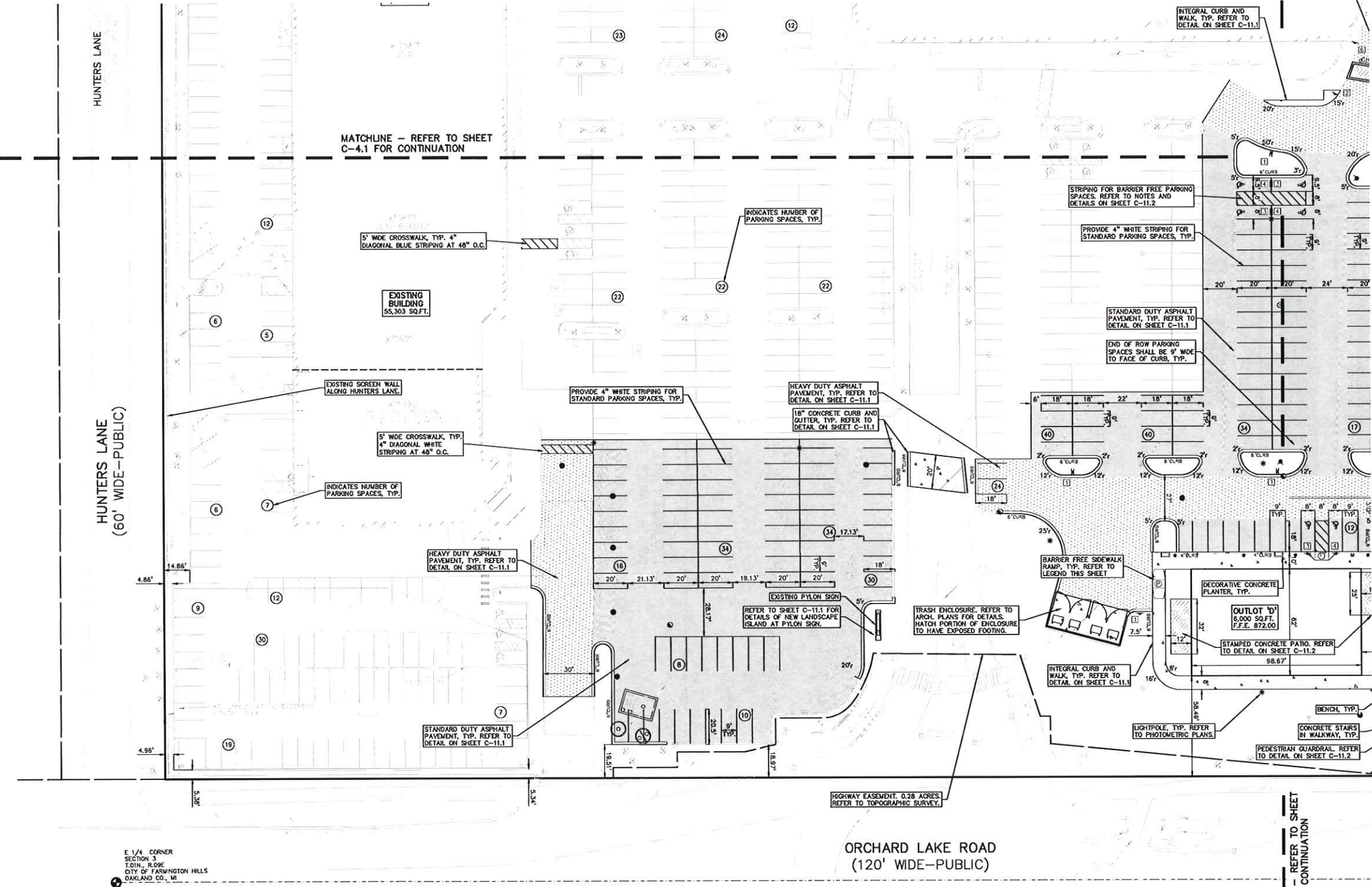
1- SPAPUD RESUBMISSION	12/03/23
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ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
DIMENSION AND PAVING PLAN - SHEET 4

PEA JOB NO. 22-0256
 P.M. RLS
 DN. KM
 DS. PB

DRAWING NUMBER
C-4.4



NOT FOR CONSTRUCTION

GENERAL NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

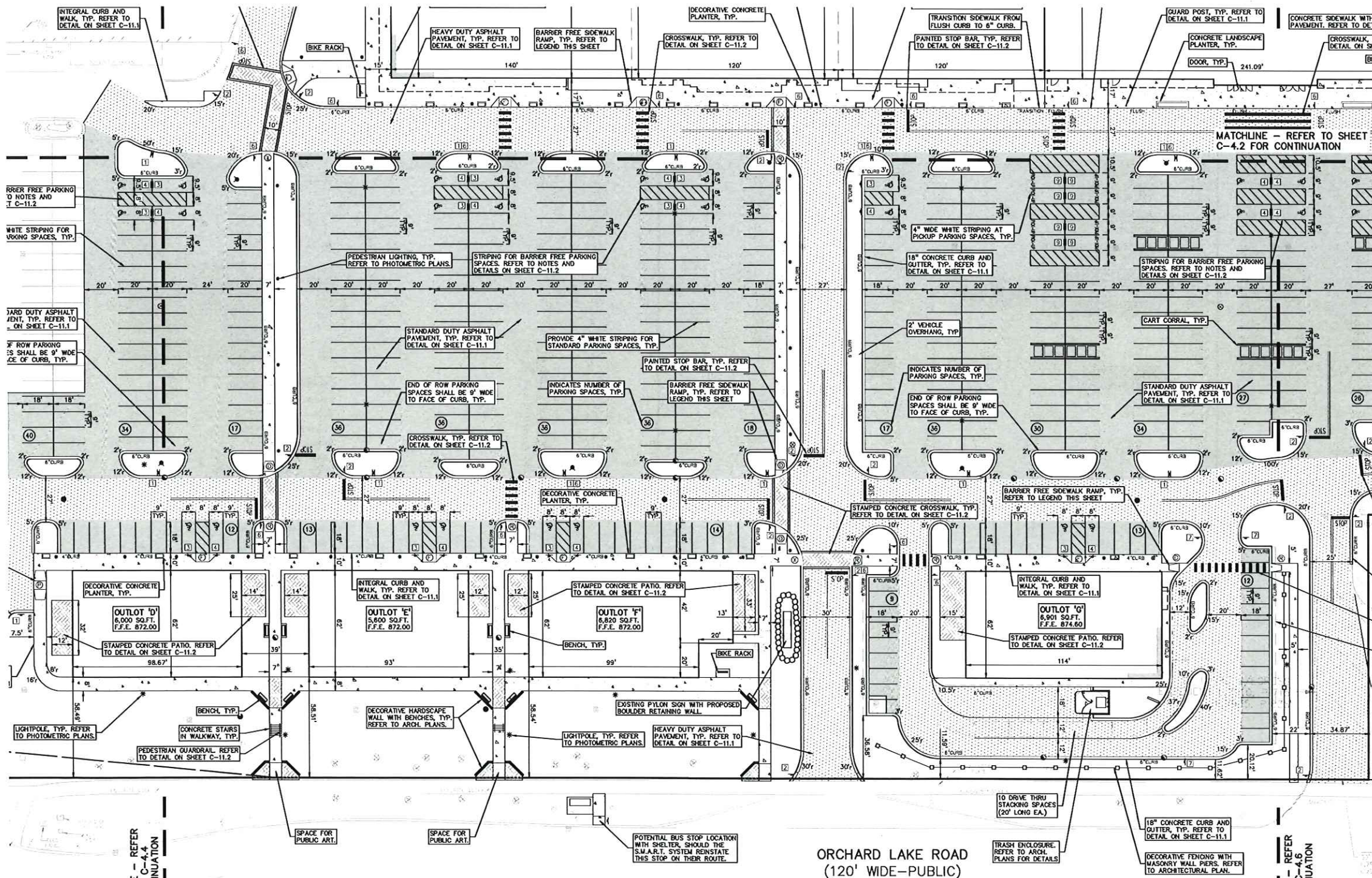
1. ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, FACE OF SIDEWALK, OUTSIDE FACE OF BUILDING, PROPERTY LINE, CENTER OF MANHOLE/CATCH BASIN OR CENTERLINE OF PIPE UNLESS OTHERWISE NOTED.
2. 'NO PARKING-FIRE LANE' SIGNS SHALL BE POSTED ALONG ALL FIRE LANES AT 125' FOOT INTERVALS OR AS DIRECTED BY THE FIRE OFFICIAL.
3. REFER TO NOTES & DETAILS SHEET FOR ON-SITE PAVING DETAILS.
4. REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

CITY OF FARMINGTON HILLS STANDARD NOTES:
 THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

1. NOTIFY THE CITY OF FARMINGTON HILLS ENGINEERING DIVISION AT 248.871.2560 A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
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PEA GROUP
 t: 844.813.2949
 www.peagroup.com

STATE OF MICHIGAN
 RACHEL L. SMITH
 LICENSE NO. 9231048828
 PROFESSIONAL ENGINEER



LEGEND

EXISTING	PROPOSED
● RGA FOUND	● CONC CORNER FOUND
○ RGA SET	○ RECEIVED
■ MAIL FOUND	■ MEASURED
■ MAIL & CAP SET	■ CALCULATED
— ELEC	— ELEC. PHONE CABLE TV OR LINE POLE & DUCT WIRE
— CATV	— UNDERGROUND CABLE TV CATV/DSL/STPL
— P-CO	— TELEPHONE CABLE FIBER OPT. & MANHOLE
— ELEC	— ELECTRIC CABLE MANHOLE METER & MANHOLE
— GAS	— GAS MAIN VALVE & GAS LINE MANHOLE
— WTR	— WATER MAIN 12\"/>

REFERENCE DRAWINGS

- TRANSMISSION MAP BY GLWA, APRIL 2018
- TRANSMISSION MAP BY GLWA, MARCH 2007
- LOCATION MAP 18341.DWG, JUNE 2022
- TRANSMISSION MAP, CONSUMERS ENERGY, EN-1843-1
- PLASTER SQUARE LAND TITLE & SURVEY, PEABODY GROUP/PEABODY MARCH 2021
- LOOMISVILLE AT HUNTERS SQUARE AS BLDG. ORCHARD PARK, HL72 & MICHIGAN, INC. AUGUST 1981 LOCATED 1981
- ORCHARD PLACE AS BLDG. ORCHARD PARK, HL72 & MICHIGAN, INC. REDESIGNED BY HL72 LOCATED 1981
- HUNTERS SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS BLDG. ORCHARD HL72 & MICHIGAN, INC. 185-03 SEPTEMBER 1982
- ORCHARD PLACE RENOVATION PROJECT AS BLDG. CH-CONSULTING ENGINEERS, INC. 08-05 OCTOBER 1988, UPDATED DECEMBER 2008
- HUNTERS SQUARE 185-03 CH-CONSULTING ENGINEERS, DECEMBER 2008
- ORCHARD LAKE ROAD REHABILITATION 13 MILE ROAD TO 14 MILE ROAD, HRC INC. OCTOBER 2018

SIDEWALK RAMP LEGEND:

- SIDEWALK RAMP 'TYPE D'
- SIDEWALK RAMP 'TYPE F'
- SIDEWALK RAMP 'TYPE P'
- SIDEWALK RAMP 'TYPE R'
- CURB DROP ONLY

REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

SIGN LEGEND:

- 'NO PARKING FIRE LANE' SIGN
- 'STOP' SIGN
- 'BARRIER FREE PARKING' SIGN
- 'VAN ACCESSIBLE' SIGN
- 'ELECTRIC VEHICLE PARKING' SIGN
- 'CROSSWALK' SIGN
- 'DO NOT ENTER' SIGN
- 'ONE WAY' SIGN
- 'PICK UP' SIGNS (NUMBERED)

REFER TO DETAIL SHEET FOR SIGN DETAILS

0 15 30 60
 SCALE: 1" = 30'

811 Know what's below. Call before you dig.

CAUTION!
 THE EXISTING ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS GIVEN FOR THE ACCURACY OF THESE ELEVATIONS. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR VERIFYING THE EXISTING UTILITIES AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.



CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN 48075

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SP4PUD RESUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
DIMENSION AND PAVING PLAN - SHEET 5

PEA JOB NO.	22-0258
P.M.	RLS
DN.	KM
DES.	PB
DRAWING NUMBER	

NOT FOR CONSTRUCTION **C-4.5**

GENERAL NOTES:
THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

1. ALL DIMENSIONS SHOWN ARE TO BACK OF CURB, FACE OF SIDEWALK, OUTSIDE FACE OF BUILDING, PROPERTY LINE, CENTER OF MANHOLE/CATCH BASIN OR CENTERLINE OF PIPE UNLESS OTHERWISE NOTED.
2. "NO PARKING-FIRE LANE" SIGNS SHALL BE POSTED ALONG ALL FIRE LANES AT 125' FOOT INTERVALS OR AS DIRECTED BY THE FIRE OFFICIAL.
3. REFER TO NOTES & DETAILS SHEET FOR ON-SITE PAVING DETAILS.
4. REFER TO LATEST EDITION OF MDOIT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

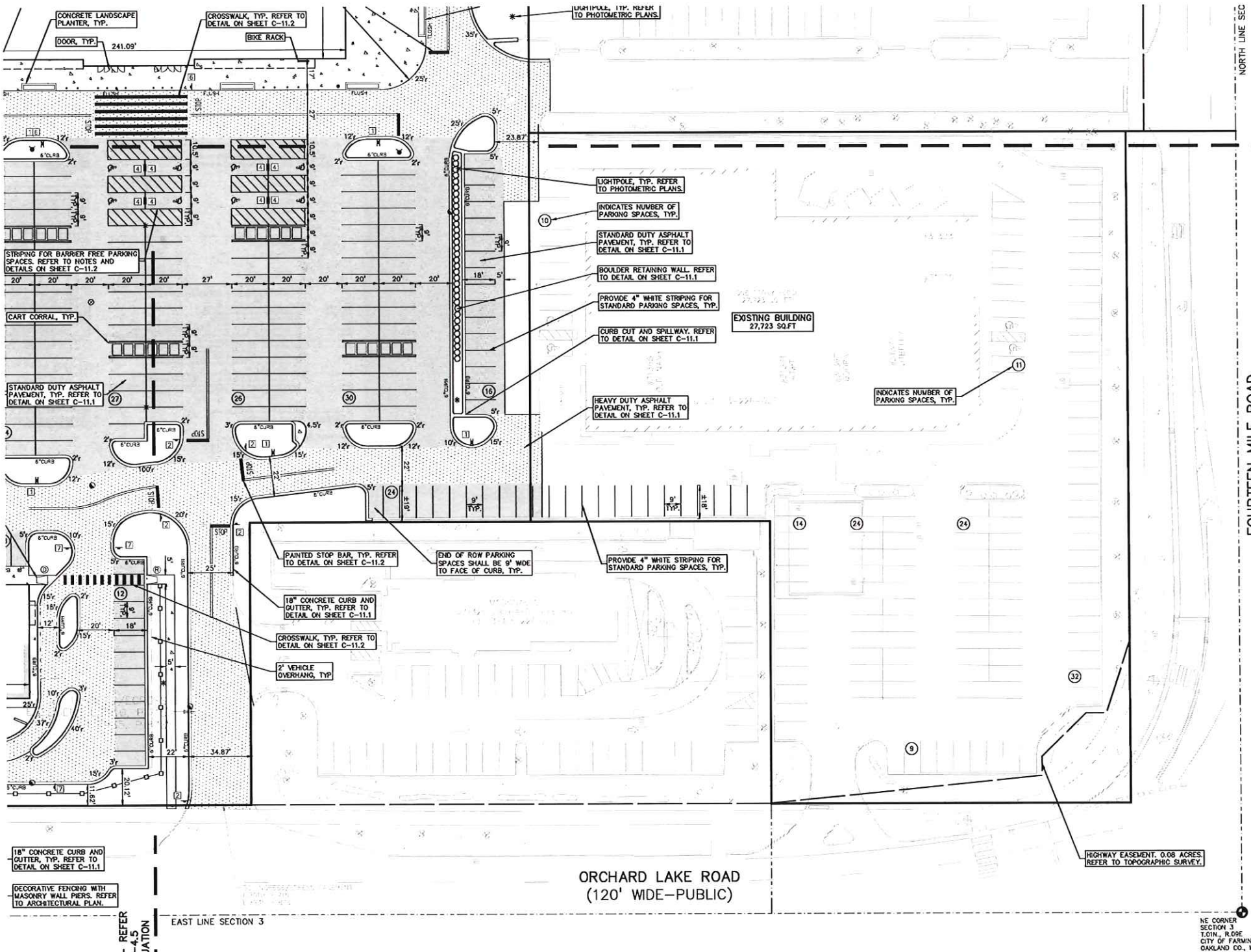
CITY OF FARMINGTON HILLS STANDARD NOTES:
THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT.

1. NOTIFY THE CITY OF FARMINGTON HILLS ENGINEERING DIVISION AT 248.871.2560 A MINIMUM OF 48 HOURS PRIOR TO THE START OF CONSTRUCTION.
2. ALL CONSTRUCTION MUST BE CONFORMING TO THE CURRENT STANDARDS AND SPECIFICATIONS ADOPTED BY THE CITY OF FARMINGTON HILLS.
3. UTILITIES TO BE UNDERGROUND.
4. CALL MISS DIG 1.800.647.7344 A MINIMUM OF 72 HOURS PRIOR TO THE START OF CONSTRUCTION.
5. ALL SOIL EROSION AND SILT MUST BE CONTROLLED AND CONTAINED ON SITE.
6. ALL EXCAVATION UNDER OR WITHIN 3 FEET OF PUBLIC PAVEMENT, EXISTING OR PROPOSED, SHALL BE BACKFILLED AND COMPACTED WITH SAND (CLASS II MDOIT).
7. THE CONTRACTOR IS RESPONSIBLE FOR ALL DAMAGE TO EXISTING UTILITIES.
8. PRIOR TO THE ISSUANCE OF AN OCCUPANCY PERMIT, ENGINEERING SITE INSPECTION IS REQUIRED.

14 MILE ROAD
13 MILE ROAD
FARMINGTON ROAD
ORCHARD LAKE ROAD
NORTHWESTERN TPK
SITE

PEA GROUP
t: 844.813.2949
www.peagroup.com

RACHEL L. SMITH
PROFESSIONAL ENGINEER
STATE OF MICHIGAN
6201048628



LEGEND

EXISTING

- PICA FOUND
- PIGA SET
- MANHOLE FOUND
- MANHOLE SET
- VALVE & CAP SET
- ELEC. FROM OVERHEAD TO ON LINE POLE OR WIRE
- UNDERGROUND CABLE TV CATV/FECS/TA
- TELEPHONE UG CABLE FIBER/OPT. & MANHOLE
- ELECTRIC UG CABLE, MANHOLE, METER & MANHOLE
- GAS MAIN, VALVE & GAS LINE MANHOLE
- WATER MAIN, VALVE, CATCH BASIN, TAPPING BLEND & VALVE
- SANITARY SEWER, CLEANOUT, & MANHOLE
- STORM SEWER, CLEANOUT, & MANHOLE
- SQUARE, ROUND & BEEHIVE CATCH BASIN, VALVE & MANHOLE
- WATER VALVE, BOLLARD, VALVE BOX, SERVICE SHUT-OFF
- MALIBOX TRANSFORMER, IRRIGATION CONTROL VALVE
- UNIDENTIFIED STRUCTURE
- SPOT ELEVATION
- CENTER LINE
- FENCE
- GUARD RAIL
- STREET LIGHT
- SIGN
- CONCR. CONCRETE
- ASPHL. ASPHALT

PROPOSED

- SEC CORNER FOUND
- RECORDED
- MEASURED
- CALCULATED
- ELEC. FROM OVERHEAD TO ON LINE POLE OR WIRE
- UNDERGROUND CABLE TV CATV/FECS/TA
- TELEPHONE UG CABLE FIBER/OPT. & MANHOLE
- ELECTRIC UG CABLE, MANHOLE, METER & MANHOLE
- GAS MAIN, VALVE & GAS LINE MANHOLE
- WATER MAIN, VALVE, CATCH BASIN, TAPPING BLEND & VALVE
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- SQUARE, ROUND & BEEHIVE CATCH BASIN, VALVE & MANHOLE
- WATER VALVE, BOLLARD, VALVE BOX, SERVICE SHUT-OFF
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- UNIDENTIFIED STRUCTURE
- SPOT ELEVATION
- CENTER LINE
- FENCE
- GUARD RAIL
- STREET LIGHT
- SIGN
- CONCR. CONCRETE
- ASPHL. ASPHALT

REFERENCE DRAWINGS

- WATER MAIN TRANSMISSION MAP BY GERRA, APRIL 2018
- ELECTRIC TRANSMISSION MAP BY GERRA, MARCH 2019
- LOCATION MAP 282374, DATE JUNE 2023
- TRANSMISSION MAP BY GERRA, MARCH 2019
- HUNTER SQUARE LAND TITLE SURVEY, PEAGROUP #17-0022, MARCH 2021
- LOCATIONS OF PLATTER SQUARES AS BATTLE CROWN PARK, HILTI & MICHELMONT, INC., AUGUST 1981, UPDATED 1988
- ORCHARD LAKE SQUARE TO CHICAGO PARK, HILTI & MICHELMONT, INC., FEBRUARY 1981, UPDATED MAY 2018
- HUNTER SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS BATTLE CROWN PARK, HILTI & MICHELMONT, INC., DECEMBER 1983, UPDATED MAY 2018
- ORCHARD PLACE RENOVATION PROJECT AS BATTLE CROWN PARK, HILTI & MICHELMONT, INC., DECEMBER 1983, UPDATED MAY 2018
- HUNTER SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS BATTLE CROWN PARK, HILTI & MICHELMONT, INC., DECEMBER 2004
- ORCHARD LAKE ROAD RENOVATION 13 MILE ROAD TO 14 MILE ROAD, HILTI & MICHELMONT, INC., OCTOBER 2019

SIDEWALK RAMP LEGEND:

- SIDEWALK RAMP "TYPE D"
- SIDEWALK RAMP "TYPE F"
- SIDEWALK RAMP "TYPE P"
- SIDEWALK RAMP "TYPE R"
- CURB DROP ONLY

REFER TO LATEST EDITION OF MDOIT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

SIGN LEGEND:

- 1 "NO PARKING FIRE LANE" SIGN
- 2 "STOP" SIGN
- 3 "BARRIER FREE PARKING" SIGN
- 4 "VAN ACCESSIBLE" SIGN
- 5 "ELECTRIC VEHICLE PARKING" SIGN
- 6 "CROSSWALK" SIGN
- 7 "DO NOT ENTER" SIGN
- 8 "ONE WAY" SIGN
- 9 "PICK UP" SIGNS (NUMBERED)

REFER TO DETAIL SHEET FOR SIGN DETAILS.

SCALE: 1" = 30'

KEY MAP: SITE SHEET KEY MAP

MATCHLINE - REFER TO SHEET C-4.5 FOR CONTINUATION

MATCHLINE - REFER TO SHEET C-4.3 FOR CONTINUATION

CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

NO.	DESCRIPTION	DATE
1	SPAPUD RESUBMISSION	12/06/23

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
DIMENSION AND PAVING PLAN - SHEET 6

PEA JOB NO. 22-0256

DESIGNER	DATE
P.M.	RLS
D.N.	KM
D.S.	PB

DRAWING NUMBER

BENCHMARKS
(GPS DERIVED - NAVD83)

BM #300
DUMPLE ON ARROW ON HYDRANT AND #109 DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

BM #301
DUMPLE ON ARROW ON HYDRANT AND #39 DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL #187 DIRECTLY SOUTH
FROM MCDONALDS RESTAURANT
ELEV. - 875.56

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN "AREAS
DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE
FLOODPLAIN" (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 25020181E EFFECTIVE SEPTEMBER 29, 2006.

GENERAL UTILITY NOTES:

- ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF FARMINGTON HILLS AND OAKLAND COUNTY.
- ALL TRENCHES UNDER OR WITHIN THREE (3) FEET OR THE FORTY-FIVE (45) DEGREE ZONE OF INFLUENCE LINE OF EXISTING AND/OR PROPOSED PAVEMENT, BUILDING PAD OR DRIVE APPROACH SHALL BE BACKFILLED WITH SAND COMPACTED TO AT LEAST NINETY-FIVE (95) PERCENT OF MAXIMUM UNIT WEIGHT (ASTM D-1557). ALL OTHER TRENCHES TO BE COMPACTED TO 90% OR BETTER.
- WHERE EXISTING MANHOLES OR SEWER PIPE ARE TO BE TAPPED, DRILL HOLES 4" CENTER TO CENTER, AROUND PERIPHERY OF OPENING TO CREATE A PLANE OF WEAKNESS JOINT BEFORE BREAKING SECTION OUT.
- THE LOCATIONS AND DIMENSIONS SHOWN ON THE PLANS FOR EXISTING UTILITIES ARE IN ACCORDANCE WITH AVAILABLE INFORMATION WITHOUT UNCOVERING AND MEASURING. THE DESIGN ENGINEER DOES NOT GUARANTEE THE ACCURACY OF THIS INFORMATION OR THAT ALL EXISTING UNDERGROUND FACILITIES ARE SHOWN. CONTRACTOR SHALL FIELD VERIFY UTILITIES.
- THE CONTRACTOR SHALL COORDINATE TO ENSURE ALL REQUIRED PIPES, CONDUITS, CABLES AND SLEEVES ARE PROPERLY PLACED FOR THE INSTALLATION OF GAS, ELECTRIC, PHONE, CABLE, IRRIGATION, ETC. IN SUCH A MANNER THAT WILL FACILITATE THEIR PROPER INSTALLATION PRIOR TO THE PLACEMENT OF THE PROPOSED PAVEMENT AND LANDSCAPING.
- PIPE LENGTHS INDICATED ARE FROM CENTER OF STRUCTURE AND TO END OF FLARED END SECTION UNLESS NOTED OTHERWISE.
- CONTRACTOR SHALL INSPECT ALL EXISTING PUBLIC STORM SEWER, SANITARY SEWER AND WATER MAIN STRUCTURES WITHIN THE LIMITS OF CONSTRUCTION AND WITH THE GOVERNING AGENCY INSPECTOR PRIOR TO ESTABLISHING FINAL GRADE. NOTIFY THE ENGINEER, OWNER/DEVELOPER, AND GOVERNING AGENCY IF STRUCTURE IS DEEMED TO BE STRUCTURALLY UNSOUND AND/OR IN NEED OF REPAIR.

STORM SEWER NOTES:

- ALL STORM SEWER 12" DIAMETER OR LARGER SHALL BE EITHER REINFORCED CONCRETE PIPE (RCP C-75) CLASS IV WITH MODIFIED TONGUE AND GROOVE JOINT WITH RUBBER GASKETS, POLYPROPYLENE DUAL-WALL CORRUGATED SMOOTH-LINED PIPE (A.D.S. HIGH-PERFORMANCE STORM PIPE), AS NOTED ON THE PLANS UNLESS SPECIFIED OTHERWISE (ASTM C-443). ALL JOINTS FOR HP-STORM PIPE SHALL BE EXTENDED BELL AND SPOOT MEETING ASTM D-3212.
- ALL STORM SEWER LEADS SHALL BE CONSTRUCTED AT 1.00% MINIMUM SLOPE.
- ALL STORM SEWER 10" OR LESS AND/OR LEADS SHALL BE SDR 28.
- JOINTS FOR P.V.C. PIPE SHALL BE ELASTOMERIC (RUBBER GASKET) AS SPECIFIED IN A.S.T.M. DESIGNATION D-3212.

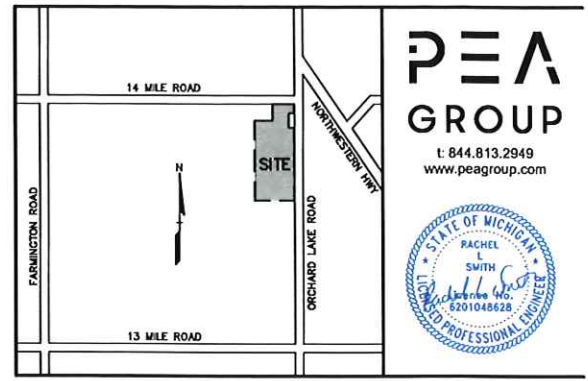
WATER MAIN NOTES:

- ALL WATER MAIN SHALL BE INSTALLED WITH A MINIMUM COVER OF 5.5' BELOW FINISH GRADE. WHEN WATER MAINS MUST DIP TO PASS UNDER A STORM SEWER OR SANITARY SEWER, THE SECTIONS WHICH ARE DEEPER THAN NORMAL SHALL BE KEPT TO A MINIMUM LENGTH BY THE USE OF VERTICAL ELEVATION AND A QUARTER (11.25) DEGREE BENDS, PROPERLY ANCHORED.
- ALL TEES, BENDS, CONNECTIONS, ETC. ARE CONSIDERED INCIDENTAL TO THE JOB.
- PHYSICAL CONNECTIONS SHALL NOT BE MADE BETWEEN EXISTING AND NEW WATER MAINS UNTIL REQUIRED TESTING IS SATISFACTORILY COMPLETED.
- MAINTAIN 10' HORIZONTAL CLEARANCE BETWEEN OUTER EDGE OF WATERMAIN AND ANY SANITARY/STORM SEWER OR STRUCTURE.

- NO PHYSICAL CONNECTION TO THE EXISTING WATER MAIN CAN BE MADE UNTIL ALL NEW WATER MAIN PASSES PRESSURE AND BACTERIOLOGICAL TESTS TO THE SATISFACTION OF THE CITY.
- ALL WATER MAIN AND FITTINGS (3" DIAMETER AND LARGER) SHALL BE DUCTILE IRON, CLASS 54.
- WATER MAIN SERVICE LEADS SHALL BE TYPE "K" ANNEALED SEAMLESS COPPER WITH FLARED FITTINGS, UNLESS OTHERWISE NOTED.
- ALL FIRE HYDRANTS SHALL BE E.I.W #58R MODEL #250 PER CITY STANDARDS.
- ALL HYDRANTS TO BE A MINIMUM OF 5' FROM BACK OF CURB, TYP.
- ALL NECESSARY FITTINGS, THRUST BLOCKS, RESTRAINING GLANDS, BLOW OFFS, ETC. FOR WATER MAIN ARE CONSIDERED INCIDENTAL TO THIS PROJECT. THE CONTRACTOR SHALL INSTALL THESE ITEMS AS NECESSARY AND AS REQUIRED BY THE CITY.
- THE WATER MAIN CONTRACTOR SHALL NOTIFY THE INSPECTION DEPARTMENT OF THE CITY AT LEAST THREE WORKING DAYS IN ADVANCE OF STARTING CONSTRUCTION.

SANITARY SEWER NOTES:

- DOWNSPOUTS, WEAP TILE, FOOTING DRAINS OR ANY CONDUIT THAT CARRIES STORM OR GROUND WATER SHALL NOT BE ALLOWED TO DISCHARGE INTO A SANITARY SEWER.
- ALL SANITARY LEADS SHALL BE CONSTRUCTED AT 1.00% MINIMUM SLOPE.
- ALL SANITARY SEWER 8" OR LARGER SHALL BE P.V.C. TRUSS PIPE (ASTM D2880) AND FITTINGS, WITH ELASTOMERIC GASKET JOINTS PER ASTM D3212 UNLESS OTHERWISE NOTED.
- ALL SANITARY SEWER LEADS SHALL BE POLYVINYL CHLORIDE (PVC) SDR 23.5 PIPE AND FITTINGS. ALL JOINTS TO BE ELASTOMERIC GASKET JOINTS PER ASTM D3212 UNLESS OTHERWISE NOTED.
- SANITARY LEADS SHALL BE PROVIDED WITH CLEANOUTS EVERY 100 FEET AND AT EVERY BEND AS SHOWN. ALL CLEANOUTS TO BE PROVIDED WITH E.I.W. #1565 BOX OR EQUAL.



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STATE OF MICHIGAN
RACHEL L. SMITH
LICENSED PROFESSIONAL ENGINEER
5210148528

LEGEND

● IRON FOUND	■ BRASS PLUG SET	○ SEC. CORNER FOUND
✕ IRON SET	■ MONUMENT FOUND	⊗ RECORDED
✕ NAIL FOUND	✕ NAIL & CAP SET	⊕ MEASURED
		⊖ CALCULATED

EXISTING

- 0-11 ELEC - W-D - UNDERGROUND CABLE TV CABLE PERMITS
- 0-11 ELEC - W-D - TELEPHONE U.S. CABLE PERMITS & MANHOLE
- 0-11 ELEC - W-D - ELECTRIC U.S. CABLE PERMITS & MANHOLE
- 0-11 ELEC - W-D - GAS MAIN VALVE & GAS LINE W/VALVE
- 0-11 ELEC - W-D - WATER MAIN 1/2" GATE VALVE TAPPING SLEEVE & VALVE
- 0-11 ELEC - W-D - SANITARY SEWER CLEANOUT & MANHOLE
- 0-11 ELEC - W-D - STORM SEWER CLEANOUT & MANHOLE
- 0-11 ELEC - W-D - SQUARE ROAD & BEE-HIVE CAT-DRAIN YARD DRAIN
- 0-11 ELEC - W-D - WATER VALVE RECONSTRUCT VALVE BOX SERVICE SHUTOFF
- 0-11 ELEC - W-D - MAIL BOX TRANSFORMER, IRRIGATION CONTROL VALVE
- 0-11 ELEC - W-D - UNDEVELOPED STRUCTURE
- 0-11 ELEC - W-D - SPOT ELEVATION
- 0-11 ELEC - W-D - CONTOUR LINE
- 0-11 ELEC - W-D - FENCE
- 0-11 ELEC - W-D - GLASS RAIL
- 0-11 ELEC - W-D - STREET LIGHT
- 0-11 ELEC - W-D - SIGN
- 0-11 ELEC - W-D - CONCRETE
- 0-11 ELEC - W-D - ASPHALT

PROPOSED

- 0-11 ELEC - W-D - 12" WATER MAIN EASEMENT, TYP.
- 0-11 ELEC - W-D - 20" SANITARY SEWER EASEMENT, TYP.
- 0-11 ELEC - W-D - RE-GRADED WEST DETENTION BASIN WITH NEW OUTLET CONDUIT TO MAXIMIZE STORMWATER STORAGE
- 0-11 ELEC - W-D - GAS LINE, TYP.
- 0-11 ELEC - W-D - MECHANICAL AND ELECTRICAL PAD
- 0-11 ELEC - W-D - GREASE TRAP
- 0-11 ELEC - W-D - SAN LEAD, TYP.
- 0-11 ELEC - W-D - STORM LEAD, TYP.
- 0-11 ELEC - W-D - EXISTING STORM SEWER TO REMAIN
- 0-11 ELEC - W-D - UG-ELECTRICAL, TYP.
- 0-11 ELEC - W-D - 12" WATER MAIN EASEMENT, TYP.
- 0-11 ELEC - W-D - EXISTING BUILDING
- 0-11 ELEC - W-D - STORM SEWER, TYP.
- 0-11 ELEC - W-D - SOUTH DETENTION VAULT
- 0-11 ELEC - W-D - PROPOSED EXPANSION OF U.G. DETENTION USING 60" DIA. PIPES
- 0-11 ELEC - W-D - EXISTING STORM SEWER TO REMAIN
- 0-11 ELEC - W-D - PROPOSED BUILDING OUTLET 10" F.F. 872.00
- 0-11 ELEC - W-D - PROPOSED BUILDING OUTLET 10" F.F. 872.00
- 0-11 ELEC - W-D - PROPOSED BUILDING OUTLET 10" F.F. 872.00
- 0-11 ELEC - W-D - PROPOSED BUILDING OUTLET 10" F.F. 874.60
- 0-11 ELEC - W-D - NORTH DETENTION VAULT

REFERENCE DRAWINGS

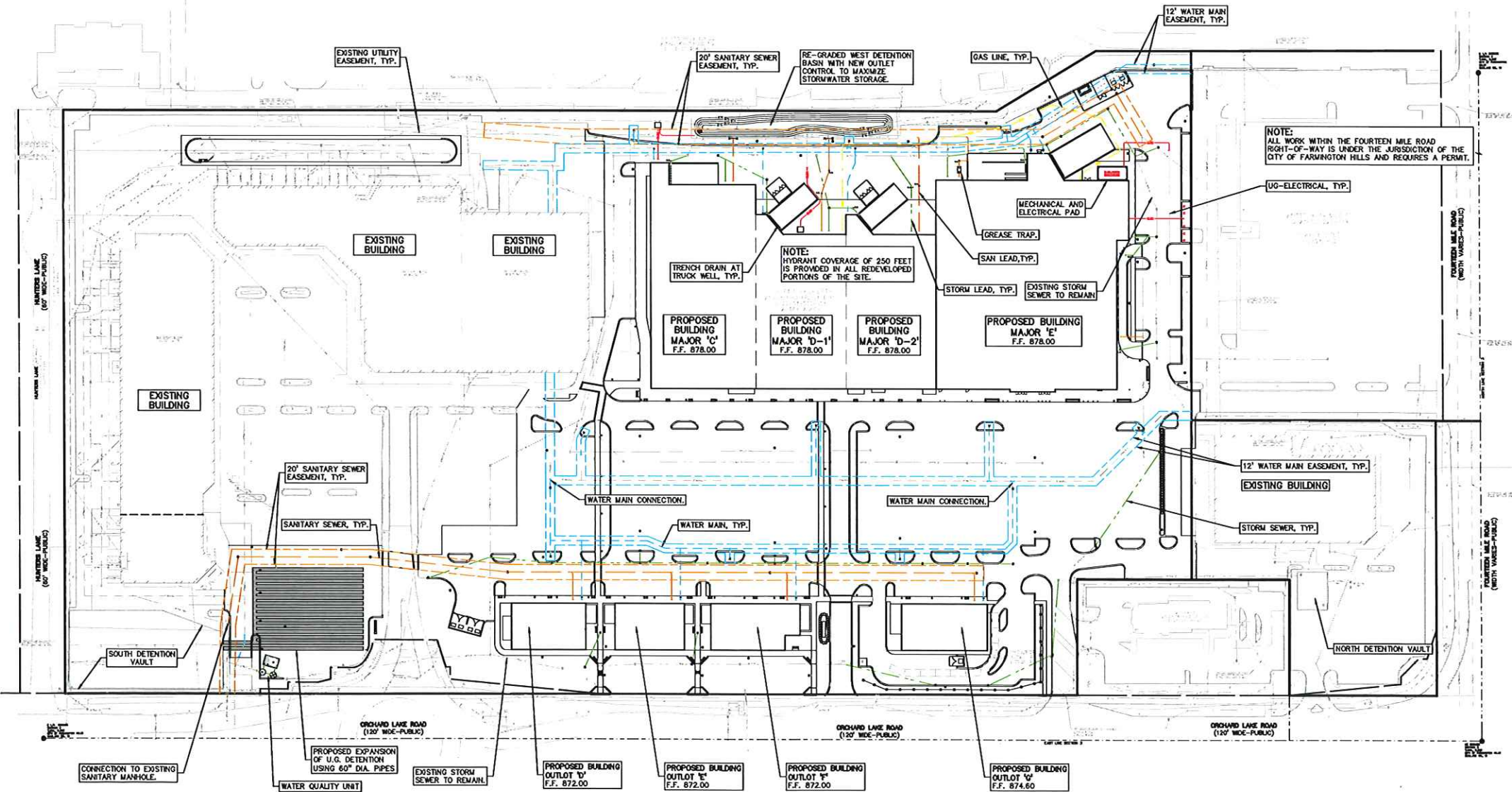
- WATER MAIN TRANSMISSION MAP #1, D.I.R.A. APRIL 2019
- ELECTRIC TRANSMISSION MAP #1, D.I.R.A. MARCH 2021
- ELECTRIC LOCATION MAP #2014, D.T.E. JUNE 2023
- GAS TRANSMISSION MAP, CONSUMER ENERGY, 01/18/23
- OTHER HUNTER SQUARE LAND TITLE & PLAT, PEA GROUP, 03/2022, MARCH 2021
- OTHER LOC. MAN. AT HUNTER SQUARE AS BUILT, ORCHARD PARK, HLTZ & MGMT, INC., AUGUST 1981, LOCATED 1981
- OTHER ORCHARD PLACE AS BUILT, ORCHARD PARK, HLTZ & MGMT, INC., REDEVELOPMENT PLAN, LOCATED MAY 2022
- OTHER HUNTER SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS BUILT, ORCHARD, HLTZ & MGMT, INC., ISSUED SEPTEMBER 1981
- OTHER ORCHARD PLACE REHABILITATION PROJECT AS BUILT, OH CONSULTING ENGINEERS, RE-OPENED OCTOBER 1985, LOCATED DECEMBER 2005
- OTHER HUNTER SQUARE REHABILITATION PROJECT AS BUILT, OH CONSULTING ENGINEERS, RE-OPENED OCTOBER 1985, LOCATED DECEMBER 2005
- OTHER ORCHARD LAKE ROAD REHABILITATION 13 MILE ROAD TO 14 MILE ROAD, H&C INC., OCTOBER 2019

SCALE: 1" = 80'

811 Know what's below. Call before you dig.

CAUTION!
THE LOCATION AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS GIVEN EXPRESSLY OR IMPLIED AS TO THE ACCURACY OF THESE UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

KEY MAP



CLIENT
RPT REALTY
20750 ORCHARD CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN, 48074

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SP/PAUD RESUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
PRELIMINARY OVERALL UTILITY PLAN

PEA JOB NO.	22-0256
P.M.	RLS
DN.	KM
DES.	PB
DRAWING NUMBER	C-7.0

NOT FOR CONSTRUCTION

BENCHMARKS:
(GPS DERIVED - NAVD83)

BM #300
DUMPLE ON ARROW ON HYDRANT AND 109' DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

BM #301
DUMPLE ON ARROW ON HYDRANT AND 137' DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL 1197' DIRECTLY SOUTH
FROM McDONALD'S RESTAURANT
ELEV. - 875.56

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN "AREAS
DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE
FLOODPLAIN" (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 28125C0513F EFFECTIVE SEPTEMBER 29, 2006.

GENERAL UTILITY NOTES:

1. ALL WORKMANSHIP AND MATERIALS SHALL BE IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF FARMINGTON HILLS AND OAKLAND COUNTY.
2. ALL TRENCHES UNDER OR WITHIN THREE (3) FEET OR THE FORTY-FIVE (45) DEGREE ZONE OF INFLUENCE LINE OF EXISTING AND/OR PROPOSED PAVEMENT, BUILDING PAD OR DRIVE APPROACH SHALL BE BACKFILLED WITH SAND COMPACTED TO AT LEAST NINETY-FIVE (95) PERCENT OF MAXIMUM UNIT WEIGHT (ASTM D-1557), ALL OTHER TRENCHES TO BE COMPACTED TO 90% OR BETTER.
3. WHERE EXISTING MANHOLES OR SEWER PIPE ARE TO BE TAPPED, DRILL HOLES 4" CENTER TO CENTER, AROUND PERIPHERY OF OPENING TO CREATE A PLANE OF WEAKNESS JOINT BEFORE BREAKING SECTION OUT.
4. THE LOCATIONS AND DIMENSIONS SHOWN ON THE PLANS FOR EXISTING UTILITIES ARE IN ACCORDANCE WITH AVAILABLE INFORMATION WITHOUT UNCOVERING AND MEASURING. THE DESIGN ENGINEER DOES NOT GUARANTEE THE ACCURACY OF THIS INFORMATION OR THAT ALL EXISTING UNDERGROUND FACILITIES ARE SHOWN. CONTRACTOR SHALL FIELD VERIFY UTILITIES.
5. THE CONTRACTOR SHALL COORDINATE TO ENSURE ALL REQUIRED PIPES, CONDUITS, CABLES AND SLEEVES ARE PROPERLY PLACED FOR THE INSTALLATION OF GAS, ELECTRIC, PHONE, CABLE, IRRIGATION, ETC. IN SUCH A MANNER THAT WILL FACILITATE THEIR PROPER INSTALLATION PRIOR TO THE PLACEMENT OF THE PROPOSED PAVEMENT AND LANDSCAPING.
6. PIPE LENGTHS INDICATED ARE FROM CENTER OF STRUCTURE AND TO END OF FLARED END SECTION UNLESS NOTED OTHERWISE.
7. CONTRACTOR SHALL INSPECT ALL EXISTING PUBLIC STORM SEWER, SANITARY SEWER AND WATER MAIN STRUCTURES WITHIN THE LIMITS OF CONSTRUCTION AND WITH THE GOVERNING AGENCY INSPECTOR PRIOR TO ESTABLISHING FINAL GRADE. NOTIFY THE ENGINEER, OWNER, DEVELOPER, AND GOVERNING AGENCY IF STRUCTURE IS DEEMED TO BE STRUCTURALLY UNSOUND AND/OR IN NEED OF REPAIR.

STORM SEWER NOTES:

1. ALL STORM SEWER 12" DIAMETER OR LARGER SHALL BE REINFORCED CONCRETE PIPE (RCP C-76) CLASS IV WITH MOORED TONGUE AND GROOVE JOINT WITH RUBBER GASKETS UNLESS SPECIFIED OTHERWISE (ASTM C-443).
2. ALL STORM SEWER LEADS SHALL BE CONSTRUCTED AT 1.00% MINIMUM SLOPE.
3. ALL STORM SEWER 10" OR LESS AND/OR LEADS SHALL BE SDR 26.
4. JOINTS FOR P.V.C. PIPE SHALL BE ELASTOMERIC (RUBBER GASKET) AS SPECIFIED IN A.S.T.M. DESIGNATION D-3212.

WATER MAIN NOTES:

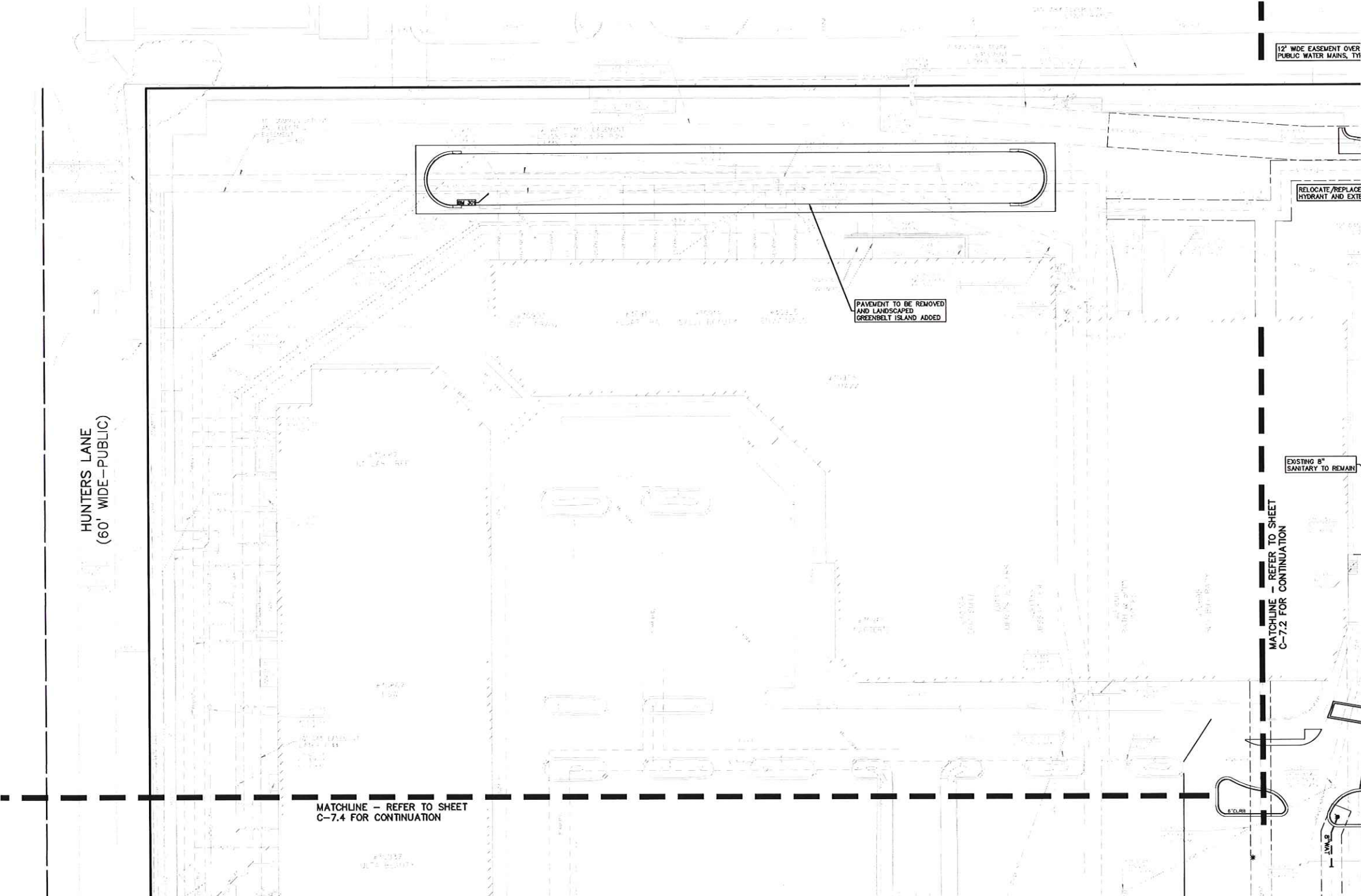
1. ALL WATER MAIN SHALL BE INSTALLED WITH A MINIMUM COVER OF 5.5' BELOW FINISH GRADE. WHEN WATER MAINS MUST DIP TO PASS UNDER A STORM SEWER OR SANITARY SEWER, THE SECTIONS WHICH ARE DEEPER THAN NORMAL SHALL BE KEPT TO A MINIMUM LENGTH BY THE USE OF VERTICAL ELEVEN AND A QUARTER (11.25°) DEGREE BENDS, PROPERLY ANCHORED.
2. ALL TEES, BENDS, CONNECTIONS, ETC. ARE CONSIDERED INCIDENTAL TO THE JOB.
3. PHYSICAL CONNECTIONS SHALL NOT BE MADE BETWEEN EXISTING AND NEW WATER MAINS UNTIL REQUIRED TESTING IS SATISFACTORILY COMPLETED.
4. MAINTAIN 10' HORIZONTAL CLEARANCE BETWEEN OUTER EDGE OF WATERMAIN AND ANY SANITARY/STORM SEWER OR STRUCTURE.
5. NO PHYSICAL CONNECTION TO THE EXISTING WATER MAIN CAN BE MADE UNTIL ALL NEW WATER MAIN PASSES PRESSURE AND BACTERIOLOGICAL TESTS TO THE SATISFACTION OF THE CITY.
6. ALL WATER MAIN AND FITTINGS (3" DIAMETER AND LARGER) SHALL BE DUCTILE IRON, CLASS 54.

7. WATER MAIN SERVICE LEADS SHALL BE TYPE "K" ANNEALED SEAMLESS COPPER WITH FLARED FITTINGS, UNLESS OTHERWISE NOTED.
8. ALL FIRE HYDRANTS SHALL BE EAW #58R MODEL #250 PER CITY STANDARDS.
9. ALL HYDRANTS TO BE A MINIMUM OF 5' FROM BACK OF CURB, TYP.
10. ALL NECESSARY FITTINGS, THRUST BLOCKS, RESTRAINING GLANDS, BLOW OFFS, ETC. FOR WATER MAIN ARE CONSIDERED INCIDENTAL TO THIS PROJECT. THE CONTRACTOR SHALL INSTALL THESE ITEMS AS NECESSARY AND AS REQUIRED BY THE CITY.
11. THE WATER MAIN CONTRACTOR SHALL NOTIFY THE INSPECTION DEPARTMENT OF THE CITY AT LEAST THREE WORKING DAYS IN ADVANCE OF STARTING CONSTRUCTION.

SANITARY SEWER NOTES:

1. DOWNSPOUTS, WEEP TILE, FOOTING DRAINS OR ANY CONDUIT THAT CARRIES STORM OR GROUND WATER SHALL NOT BE ALLOWED TO DISCHARGE INTO A SANITARY SEWER.
2. ALL SANITARY LEADS SHALL BE CONSTRUCTED AT 1.00% MINIMUM SLOPE.
3. ALL SANITARY SEWER 8" OR LARGER SHALL BE P.V.C. TRUSS PIPE (ASTM D2680) AND FITTINGS, WITH ELASTOMERIC GASKET JOINTS PER ASTM D3212 UNLESS OTHERWISE NOTED.
4. ALL SANITARY SEWER LEADS SHALL BE POLYVINYL CHLORIDE (PVC) SDR 23.5 PIPE AND FITTINGS. ALL JOINTS TO BE ELASTOMERIC GASKET JOINTS PER ASTM D3212 UNLESS OTHERWISE NOTED.
5. SANITARY LEADS SHALL BE PROVIDED WITH CLEANOUTS EVERY 100 FEET AND AT EVERY BEND AS SHOWN. ALL CLEANOUTS TO BE PROVIDED WITH E.L.W. #165 BOX OR EQUAL.

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LEGEND

● PIGN FOUND	⊗ BRASS PLUG SET	⊙ SEC. CORNER FOUND
⊗ PIGN SET	⊗ MONUMENT FOUND	⊙ RECORDED
⊗ NAIL FOUND	⊗ MONUMENT SET	⊙ MEASURED
⊗ NAIL & CAP SET		⊙ CALCULATED

EXISTING

— 0" ELEC. 44-0	ELEC. PHONE OR CABLE TV OH LINE, POLE & DWP WIRE	— 0" ELEC. 44-0	ELEC. 44-0
— LG-CATV	UNDERGROUND CABLE TV CATV PRESTAL	— LG-ELEC. 44-0	UNDERGROUND CABLE TV CATV PRESTAL
— 0" LG-PHON	TELEPHONE U.S. CABLE, PRESTAL & MAN-POLE	— 0" LG-ELEC. 44-0	ELECTRIC U.S. CABLE, MAN-POLE, METERS & HAND-OLE
— 0" LG-ELEC. 44-0	GAS MAIN VALVE & GAS LINE W/VALVE	— 0" LG-ELEC. 44-0	WATER MAIN VALVE, GATE VALVE, TAPPING SLEEVE & VALVE
— 0" LG-ELEC. 44-0	SANITARY BEHMER, CLEANOUT & MAN-OLE	— 0" LG-ELEC. 44-0	STORM SEWER, CLEANOUT & MAN-OLE
— 0" LG-ELEC. 44-0	SQUARE ROUND & BEEHIVE CATV-BASH, YARD DRAIN	— 0" LG-ELEC. 44-0	WATER VALVE BOMBYCRANT VALVE BOX SERVICE S-1, TOP
— 0" LG-ELEC. 44-0	MANHOLE TRANSFORMER, IRRIGATION CONTROL, V.V. VALVE	— 0" LG-ELEC. 44-0	UNCOVERED STRUCTURE
— 0" LG-ELEC. 44-0	CONTOUR LINE	— 0" LG-ELEC. 44-0	CONTOUR LINE
— 0" LG-ELEC. 44-0	FENCE	— 0" LG-ELEC. 44-0	GLAND RAIL
— 0" LG-ELEC. 44-0	STREET LIGHT	— 0" LG-ELEC. 44-0	SDN
— 0" LG-ELEC. 44-0	CONC.	— 0" LG-ELEC. 44-0	CONC.
— 0" LG-ELEC. 44-0	ASPH.	— 0" LG-ELEC. 44-0	ASPH.

PROPOSED

— 0" LG-ELEC. 44-0	CONC.	— 0" LG-ELEC. 44-0	CONC.
— 0" LG-ELEC. 44-0	ASPH.	— 0" LG-ELEC. 44-0	ASPH.

REFERENCE DRAWINGS

WATER MAIN	TRANSVERSE MAP #47, O.R.M.A. APRIL 2018
WATER MAIN	TRANSVERSE MAP #234, MARCH 2017
ELECTRIC	LOCATION MAP #234, OTE, JUNE 2018
GAS	TRANSVERSE MAP #234, OTE, JUNE 2018
OTHER	HUNTER SQUARE LAND TITLE SURVEY, PEA GROUP #11-0022, MARCH 2021
OTHER	LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THE DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS EITHER EXPRESSED OR IMPLIED AS TO THE CORRECTNESS OF RECORD DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXACT UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.
OTHER	INC. AUGUST 1981, UPDATED 1988
OTHER	ORCHARD PLACE REHABILITATION PROJECT AS BUILT, OHM CONSULTING ENGINEERS, RECORDED OCTOBER 1988, UPDATED DECEMBER 2008
OTHER	HUNTER SQUARE BUILDING #1, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #2, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	ORCHARD PLACE REHABILITATION PROJECT AS BUILT, OHM CONSULTING ENGINEERS, RECORDED OCTOBER 1988, UPDATED DECEMBER 2008
OTHER	HUNTER SQUARE BUILDING #1, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #2, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #3, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #4, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #5, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #6, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #7, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #8, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #9, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #10, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #11, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #12, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #13, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #14, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #15, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #16, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #17, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #18, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #19, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004
OTHER	HUNTER SQUARE BUILDING #20, OHM CONSULTING ENGINEERS, RECORDED DECEMBER 2004

GENERAL GRADING AND EARTHWORK NOTES:

THESE NOTES APPLY TO ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT

1. CONTRACTOR SHALL FIELD VERIFY ALL EXISTING TREES AND BRUSH AND REMOVE ALL THAT ARE NECESSARY TO GRADE SITE.
2. ALL GRADES ARE TO TOP OF PAVEMENT UNLESS OTHERWISE NOTED.
3. THE STAGING OF CONSTRUCTION ACTIVITIES SHALL OCCUR ONLY WITHIN THE SITE BOUNDARIES. ANY CONSTRUCTION ACTIVITIES OUTSIDE OF THE SITE BOUNDARIES SHALL BE AT THE SOLE RESPONSIBILITY AND RISK OF THE CONTRACTOR.
4. ALL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL MEET THE REQUIREMENTS OF THE AUTHORIZED PUBLIC AGENCY OF JURISDICTION. AN EROSION CONTROL PERMIT MUST BE SECURED FROM THE CITY PRIOR TO CONSTRUCTION.
5. ALL EARTHWORK AND GRADING OPERATIONS SHALL BE PERFORMED IN ACCORDANCE WITH THE SOILS INVESTIGATION AND REPORT.
6. REFER TO CONSTRUCTION PLANS FOR ADDITIONAL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES AND NOTES.
7. ALL SLOPES EXCEEDING 1:6 MUST BE STABILIZED BY SOODING OR BY PLACING A MULCH BLANKET PEGGED IN PLACE OVER SEED.
8. ALL DISTURBED AREAS SHALL BE SEEDS AND MULCHED OR SOODED IN ACCORDANCE WITH THE LANDSCAPE PLANS. PROVIDE A MINIMUM OF 3" OF TOPSOIL IN THESE AREAS UNLESS OTHERWISE NOTED.
9. THE CONTRACTOR SHALL NOTE EXISTING UNDERGROUND UTILITIES WITHIN AND ADJACENT TO THE SITE. BACKFILL FOR EXISTING UTILITY TRENCHES SHALL BE EXAMINED CRITICALLY. ANY TRENCHES FOUND TO HAVE SOFT, UNSTABLE OR UNSUITABLE BACKFILL MATERIAL, IN THE OPINION OF THE THIRD PARTY TESTING COMPANY, THAT ARE TO BE WITHIN THE ZONE OF INFLUENCE OF PROPOSED BUILDINGS OR PAVEMENT SHALL BE COMPLETELY EXCAVATED AND BACKFILLED WITH SUITABLE MATERIAL.
10. ON-SITE FILL CAN BE USED IF THE SPECIFIED COMPACTION REQUIREMENTS CAN BE ACHIEVED. IF ON-SITE SOIL IS USED, IT SHOULD BE CLEAN AND FREE OF FROZEN SOIL, ORGANICS, OR OTHER DELETERIOUS MATERIALS.
11. THE FINAL SUBGRADE/EXISTING AGGREGATE BASE SHOULD BE THOROUGHLY PROFFERED USING A FULLY LOADED TANDEM AXLE TRUCK OR FRONT END LOADER UNDER THE OBSERVATION OF A GEOTECHNICAL/PAVEMENT ENGINEER. LOOSE OR YIELDING AREAS THAT CANNOT BE MECHANICALLY STABILIZED SHOULD BE REINFORCED USING GEORIDS OR REMOVED AND REPLACED WITH ENGINEERED FILL OR AS DICTATED BY FIELD CONDITIONS.
12. SUBGRADE UNDERCUTTING, INCLUDING BACKFILLING SHALL BE PERFORMED TO REPLACE MATERIALS SUSCEPTIBLE TO FROST HEAVING AND UNSTABLE SOIL CONDITIONS. ANY EXCAVATIONS THAT MAY BE REQUIRED BELOW THE TOPSOIL IN FILL AREAS WILL BE CLASSIFIED AS SUBGRADE UNDERCUTTING.
13. SUBGRADE UNDERCUTTING SHALL BE PERFORMED WHERE NECESSARY AND THE EXCAVATED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR. ANY SUBGRADE UNDERCUTTING SHALL BE BACKFILLED AS RECOMMENDED IN THE GEOTECHNICAL ENGINEERING REPORT FOR THE PROJECT.
14. ANY SUBGRADE WATERING REQUIRED TO ACHIEVE REQUIRED DENSITY SHALL BE CONSIDERED INCIDENTAL TO THE JOB.

0 15 30 60
SCALE: 1" = 30'

811 Know what's below. Call before you dig.

CAUTION!!
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KEY MAP

CLIENT

RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE

HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SP4PUD RESUBMISSION	12/08/23
-------------------------	----------

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE

PRELIMINARY GRADING AND UTILITY PLAN - SHEET 1

PEA JOB NO. 22-0256

P.M.	RLS
DN.	KM
DES.	PB

DRAWING NUMBER

C-7.1

NOT FOR CONSTRUCTION

BENCHMARKS
(GPS DERIVED - NAVD83)

BM #300
DUMPLE ON ARROW ON HYDRANT AND ±109 DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

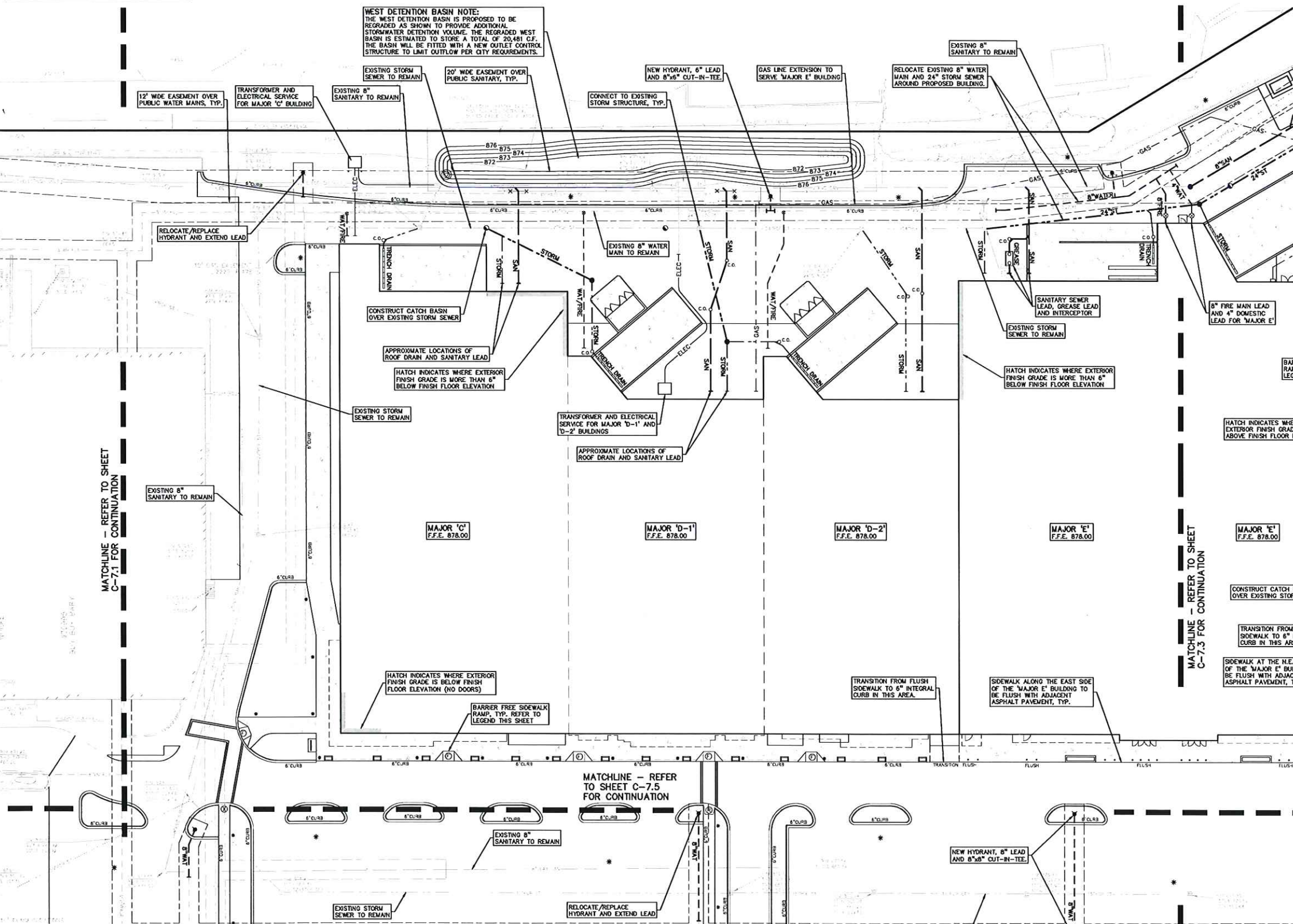
BM #301
DUMPLE ON ARROW ON HYDRANT AND ±39 DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL ±187 DIRECTLY SOUTH
FROM MCDONALD'S RESTAURANT
ELEV. - 875.56

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN 'AREAS
DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE
FLOODPLAIN' (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 26125C0513F EFFECTIVE SEPTEMBER 29, 2006.

PEA GROUP
t: 844.813.2949
www.peagroup.com

STATE OF MICHIGAN
RACHEL L. SMITH
Professional Engineer
No. 6201048828



LEGEND

EXISTING	PROPOSED
● IRON FOUND	● REC. CORNER FOUND
⊗ IRON SET	⊗ RECORDED
⊗ NAIL FOUND	⊗ MEASURED
⊗ NAIL & CAP SET	⊗ CALCULATED
○-ELEC-W-D	ELEC. PHONE CABLE TV OR LINE POLE & DUTY WIRE
○-ELEC-W-E	UNDERGROUND CABLE TV CABLE FEEDS
○-ELEC-W-F	TELEPHONE OR CABLE FEEDS TO Bldg. & MANHOLE
○-ELEC-W-G	ELECTRIC GAS CABLE MANHOLE METER & MANHOLE
○-ELEC-W-H	GAS MANHOLE & GAS LINE MANHOLE
○-ELEC-W-I	WATERMANHOLE, GATE VALVE, TAPPING BLEED & VALVE
○-ELEC-W-J	SANITARY SEWER, GATE VALVE & MANHOLE
○-ELEC-W-K	STORM SEWER, GATE VALVE & MANHOLE
○-ELEC-W-L	SQUARE ROAD & REEVE CATCH-BASIN, VAD DRAIN
○-ELEC-W-M	WATER VALVE, BONDING VALVE BOX, SERVICE, SPLIT
○-ELEC-W-N	MANHOLE, TRANSFORMER, IRRIGATION CONTROL VALVE
○-ELEC-W-O	UNDEVELOPED STRUCTURE
○-ELEC-W-P	SPOT ELEVATION
○-ELEC-W-Q	CONTOUR LINE
○-ELEC-W-R	FINISH
○-ELEC-W-S	GLASS
○-ELEC-W-T	8\"/>

REFERENCE DRAWINGS

- WATER MAIN TRANSMISSION MAP #1, 01/14/19, APR. 2019
- WATER MAIN TRANSMISSION MAP #1, 01/14/19, APR. 2019
- ELECTRIC LOCATION MAP #2019/04, DATE: 2/26/2019
- GAS TRANSMISSION MAP, CONSUMERS ENERGY, #13423-1
- OTHER HUNTER SQUARE LAND TITLE & SURVEY, PEA GROUP #1-2023, MARCH 2023
- OTHER LOG-HAMANN AT HUNTER SQUARE AS BUILT, ORCHARD LAKE, HLTZ & WILMONT, INC.
- OTHER INC. AUGUST 1981, LOCATED 1981
- OTHER ORCHARD PLACE AS BUILT, ORCHARD LAKE, HLTZ & WILMONT, INC.
- OTHER BEDED FIRM, AIRY FIRM, LOCATED MAY 2005
- OTHER HUNTER SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS BUILT, ORCHARD LAKE, HLTZ & WILMONT, INC., ISSUED SEPTEMBER 1981.
- OTHER ORCHARD PLACE REPAIR PLAN PROJECT AS BUILT, 01/04/2005, ST/PO
- OTHER ENGINEERS, REEVE, OCTOBER 1985, LOCATED DECEMBER 2005
- OTHER HUNTER SQUARE BUILDING "D" ON-CORRELATING ENGINEER, DECEMBER 2004
- OTHER ORCHARD LAKE ROAD REPAIR PLAN, 13 MILE ROAD TO 14 MILE ROAD, HRC INC., OCTOBER 2019

LEGEND

0 15 30 60
SCALE: 1" = 30'

811 *Know what's below. Call before you dig.*

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KEY MAP

SITE SHEET KEY MAP

REFERENCE DRAWINGS

WATER MAIN TRANSMISSION MAP #1, 01/14/19, APR. 2019
WATER MAIN TRANSMISSION MAP #1, 01/14/19, APR. 2019
ELECTRIC LOCATION MAP #2019/04, DATE: 2/26/2019
GAS TRANSMISSION MAP, CONSUMERS ENERGY, #13423-1
HUNTER SQUARE LAND TITLE & SURVEY, PEA GROUP #1-2023, MARCH 2023
LOG-HAMANN AT HUNTER SQUARE AS BUILT, ORCHARD LAKE, HLTZ & WILMONT, INC.
INC. AUGUST 1981, LOCATED 1981
ORCHARD PLACE AS BUILT, ORCHARD LAKE, HLTZ & WILMONT, INC.
BEDED FIRM, AIRY FIRM, LOCATED MAY 2005
HUNTER SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS BUILT, ORCHARD LAKE, HLTZ & WILMONT, INC., ISSUED SEPTEMBER 1981.
ORCHARD PLACE REPAIR PLAN PROJECT AS BUILT, 01/04/2005, ST/PO
ENGINEERS, REEVE, OCTOBER 1985, LOCATED DECEMBER 2005
HUNTER SQUARE BUILDING "D" ON-CORRELATING ENGINEER, DECEMBER 2004
ORCHARD LAKE ROAD REPAIR PLAN, 13 MILE ROAD TO 14 MILE ROAD, HRC INC., OCTOBER 2019

SIDEWALK RAMP LEGEND:

- SIDEWALK RAMP "TYPE D" (D)
- SIDEWALK RAMP "TYPE F" (F)
- SIDEWALK RAMP "TYPE P" (P)
- SIDEWALK RAMP "TYPE R" (R)
- CURB DROP ONLY (C)

REFER TO LATEST EDITION OF MDOT R-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN, 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SP/PLD REUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
PRELIMINARY GRADING AND UTILITY PLAN - SHEET 2

PEA JOB NO.	22-0256
P.M.	RLS
DN.	KM
DES.	PB
DRAWING NUMBER	

C-7.2

NOT FOR CONSTRUCTION

BENCHMARKS:
(GPS DERIVED - NAVD83)

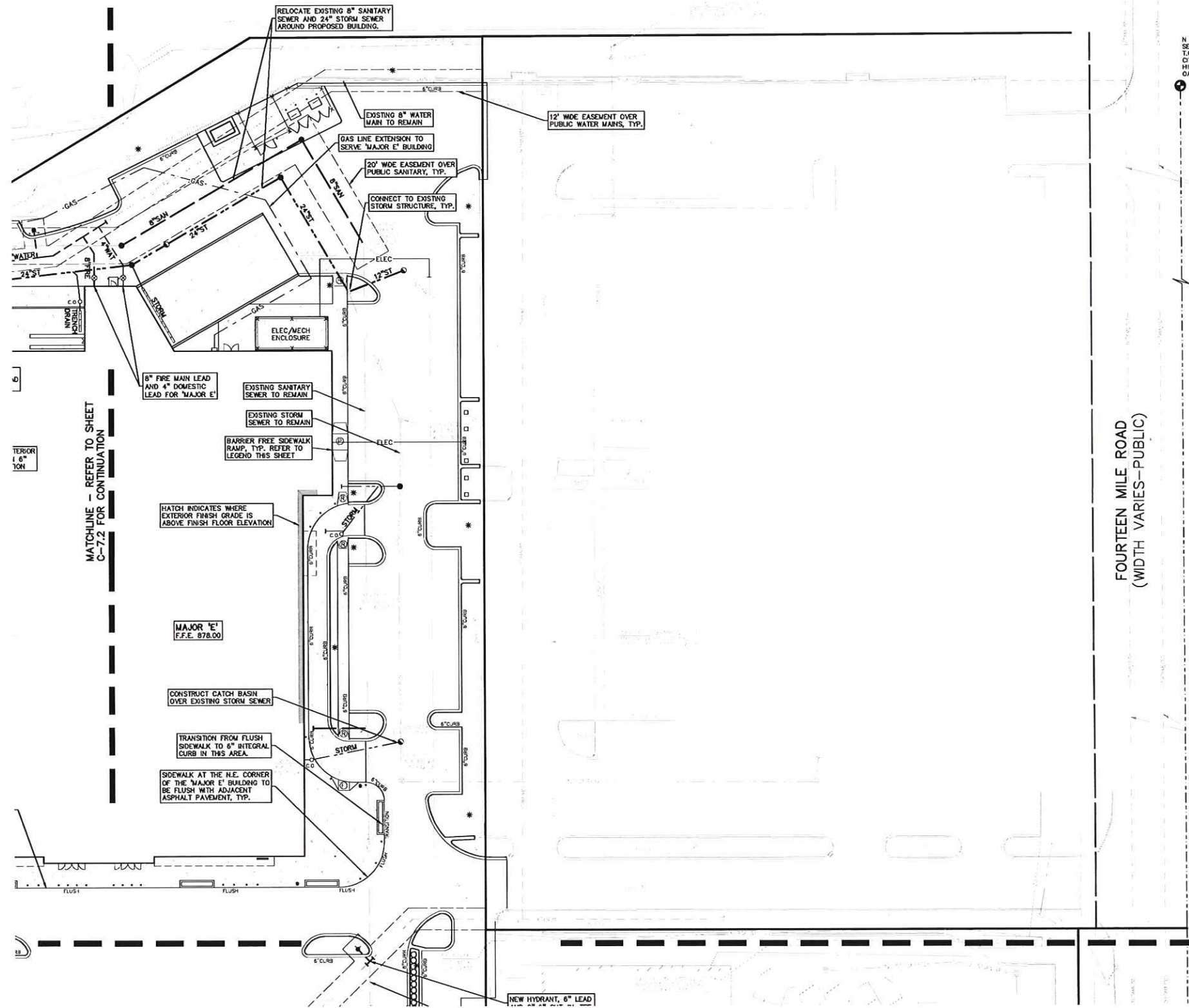
BM #300
SIMPLE ON ARROW ON HYDRANT AND 4109' DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

BM #301
SIMPLE ON ARROW ON HYDRANT AND 439' DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL 4187' DIRECTLY SOUTH
FROM MCDONALD'S RESTAURANT
ELEV. - 875.56

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN AREAS
DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE
FLOODPLAIN (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 26125C0513F EFFECTIVE SEPTEMBER 29, 2006.

PEA GROUP
t: 844.813.2949
www.peagroup.com



N 1/4 CORNER
SECTION 3
TOWNSHIP 36 N
RANGE 10 E
CITY OF FARMINGTON HILLS
OAKLAND CO., MI

LEGEND

● IRON FOUND	⊗ BRASS PLUG SET	⊙ SEC. CORNER FOUND
⊗ IRON SET	⊗ MONUMENT FOUND	⊙ RECORDED
⊗ NAIL FOUND	⊗ MONUMENT SET	⊙ MEASURED
⊗ NAIL & CAP SET		⊙ CALCULATED

EXISTING

- ELEC—W—C— ELEC. PHONE OR CABLE TV OR LINE POLE & DUTY #/RE
- L-G—CABLE— ELEC. UNDERGROUND CABLE TV OR FIBER OPT.
- L-G—P—C— TELEPHONE U.S. CABLE, FIBER OPT. & MANHOLE
- L-G—ELEC—ELEC. U.S. CABLE, MANHOLE, METER & MANHOLE
- GAS— GAS MAIN, VALVE & GAS LINE MANHOLE
- WATER— WATER MAIN, VALVE, CATCH BASIN, TAPPING SERVICE & VALVE
- S&V— SANITARY SEWER, CLEANOUT & MANHOLE
- S&V— STORM SEWER, CLEANOUT & MANHOLE
- S&V— SQUARE, ROUND & BEEHIVE CATCH-BASIN, VARD DRAIN
- WATER VALVE BOMBYNANT VALVE BOX SERVICE S-UTD/F
- MTRBX— MANHOLE TRANSFORMER, REGULATION CONTROL, VALVE
- UNDEVELOPED STRUCTURE

PROPOSED

- ELEC— ELEC.
- GAS— GAS
- WATER— WATER
- S&V— S&V
- CONC— CONCRETE
- ASPH— ASPHALT

REFERENCE DRAWINGS

- WATER MAIN: TRANSMISSION MAP #1, OLIVIA, APRIL 2019
- WATER MAIN: TRANSMISSION MAP #2, OLIVIA, MARCH 2021
- ELECTRIC: LOCATION MAP 2023-314, DATE: JUNE 2023
- GAS: TRANSMISSION MAP, CONSUMERS ENERGY, 01-29-2021
- OTHER: HUNTER SQUARE LAND TITLE, RUVBY, PEA GROUP #11-002, MARCH 2021
- OTHER: LOG-WALKER #1 HUNTER SQUARE AS BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC.
- OTHER: ORCHARD PLACE #2 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., AUGUST 1981, LOCATED 1988
- OTHER: HUNTER SQUARE #2 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., ISSUED FEBRUARY 1981, LOCATED MAY 2025
- OTHER: HUNTER SQUARE #3 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., ISSUED FEBRUARY 1981, LOCATED MAY 2025
- OTHER: ORCHARD PLACE #1 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., ISSUED SEPTEMBER 1983
- OTHER: ORCHARD PLACE #2 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., ISSUED SEPTEMBER 1983
- OTHER: ORCHARD PLACE #3 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., ISSUED SEPTEMBER 1983
- OTHER: HUNTER SQUARE #4 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., ISSUED OCTOBER 1985, LOCATED DECEMBER 2025
- OTHER: HUNTER SQUARE #5 BLDG. TO ORCHARD PARK, HLTZ & W/CLMNT, INC., ISSUED OCTOBER 1985, LOCATED DECEMBER 2025
- OTHER: ORCHARD LAKE ROAD REHABILITATION-13 MILE ROAD TO 14 MILE ROAD, HLTZ & W/CLMNT, INC., OCTOBER 2018

0 15 30 60
SCALE: 1" = 30'

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KEY MAP

SIDEWALK RAMP LEGEND:

- SIDEWALK RAMP "TYPE D" ⊙
- SIDEWALK RAMP "TYPE F" ⊙
- SIDEWALK RAMP "TYPE P" ⊙
- SIDEWALK RAMP "TYPE R" ⊙
- CURB DROP ONLY ⊙

REFER TO LATEST EDITION OF MOOT #1-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

CLIENT

RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE

HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SPAPUD RESUBMISSION	12/06/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
PRELIMINARY GRADING AND UTILITY PLAN - SHEET 3

PEA JOB NO.	22-0256
P.M.	RLS
DN.	KM
DES.	PB
DRAWING NUMBER	

C-7.3

NOT FOR CONSTRUCTION

MATCHLINE - REFER TO SHEET C-7.6 FOR CONTINUATION

MATCHLINE - REFER TO SHEET C-7.2 FOR CONTINUATION

FOURTEEN MILE ROAD
(WIDTH VARIES-PUBLIC)

NORTH LINE SECTION 3

BENCHMARKS:
(GPS DERIVED - NAVD83)

BM #300
DUMPLE ON ARROW ON HYDRANT AND 1109' DIRECTLY
NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE
BUY BUY BABY.
ELEV. - 877.55

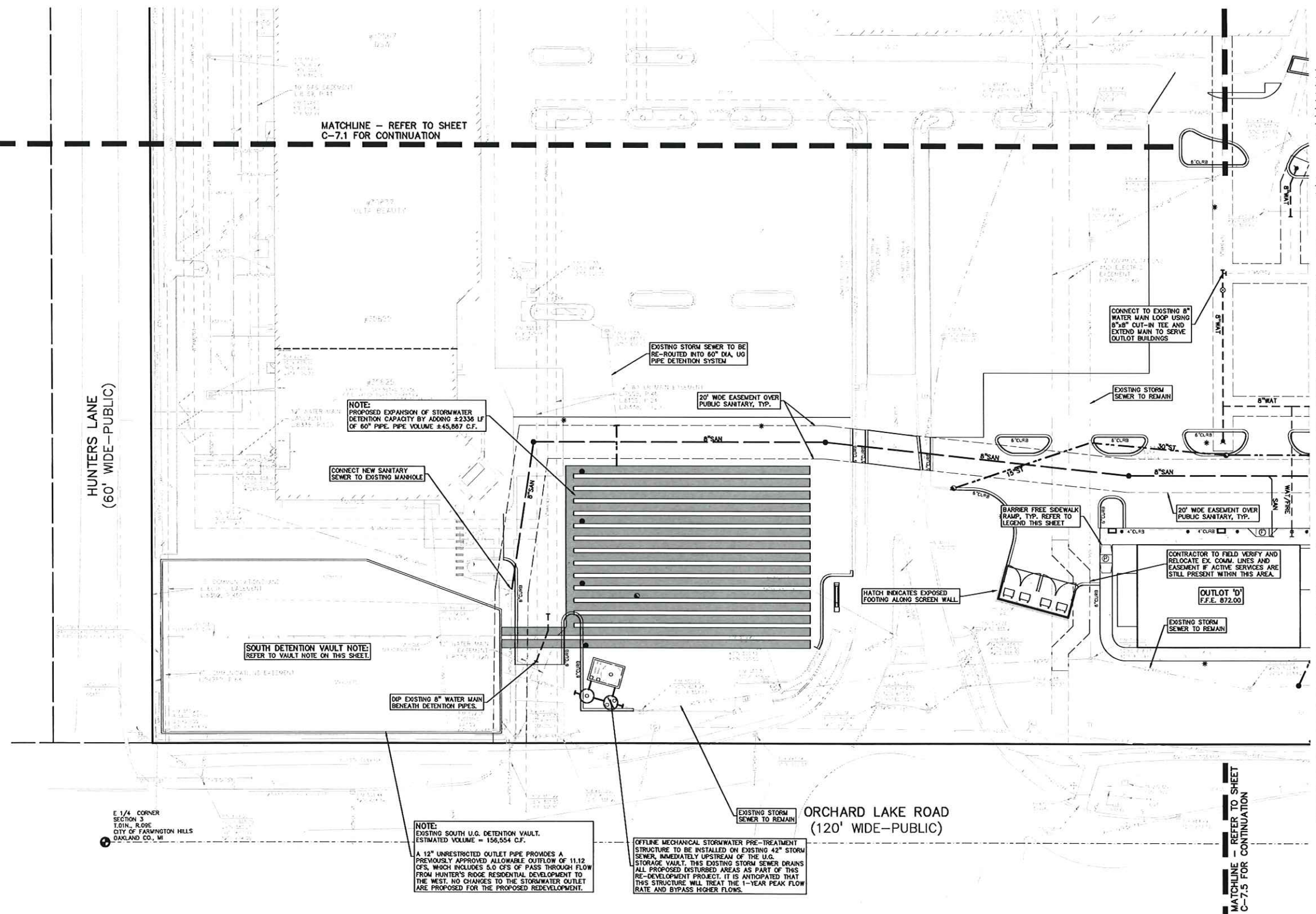
BM #301
DUMPLE ON ARROW ON HYDRANT AND 139' DIRECTLY WEST FROM
SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF
SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL 1107' DIRECTLY SOUTH
FROM MCDONALDS RESTAURANT
ELEV. - 875.56

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN AREAS
DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE
FLOODPLAIN (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP
PANEL NUMBER 26125C0513F EFFECTIVE SEPTEMBER 29, 2006.

PEA GROUP
t: 844.813.2949
www.peagroup.com

RACHEL SMITH
STATE OF MICHIGAN
PROFESSIONAL ENGINEER
NO. 2701048528



LEGEND

EXISTING	PROPOSED
IRON FOUND	BRASS PILE SET
IRON SET	MONUMENT FOUND
NAIL FOUND	MONUMENT SET
NAIL & CAP SET	RECORDED
	MARKED
	CALCULATED

LEGEND

0-1-ELEC-W-O	ELEC. PHONE OR CABLE TV OR LINE POLE 8" OR 1" PIPE	0-1-ELEC-O-T	ELEC. PHONE OR CABLE TV OR LINE POLE 8" OR 1" PIPE
UG-CATV	UNDERGROUND CABLE TV CATV FEEDER	UG-ELEC-O-T	UNDERGROUND CABLE TV CATV FEEDER
UG-PHON	TELEPHONE U.S. CABLE ROUTER & SWITCH	UG-ELEC-W-O	TELEPHONE U.S. CABLE ROUTER & SWITCH
UG-ELEC	ELECTRIC U.S. CABLE MANHOLE METERS AND OLE	UG-ELEC-W-O	ELECTRIC U.S. CABLE MANHOLE METERS AND OLE
UG-ELEC	GAS MAIN, VALVE & GAS LINE & WAGER	UG-ELEC-W-O	GAS MAIN, VALVE & GAS LINE & WAGER
UG-ELEC	WATER MAIN, 12" GATE VALVE, TAPPING BREEZE & VALVE	UG-ELEC-W-O	WATER MAIN, 12" GATE VALVE, TAPPING BREEZE & VALVE
UG-ELEC	SANITARY SEWER, 4" MANHOLE & WAGER	UG-ELEC-W-O	SANITARY SEWER, 4" MANHOLE & WAGER
UG-ELEC	STORM SEWER, 4" MANHOLE & WAGER	UG-ELEC-W-O	STORM SEWER, 4" MANHOLE & WAGER
UG-ELEC	SQUARE ROUND & MEDIAN CATV-BASED, 1" HOOD DRAIN	UG-ELEC-W-O	SQUARE ROUND & MEDIAN CATV-BASED, 1" HOOD DRAIN
UG-ELEC	WATER VALVE, 8" OR 10" GATE VALVE, BOX SERVICE, 94-1277	UG-ELEC-W-O	WATER VALVE, 8" OR 10" GATE VALVE, BOX SERVICE, 94-1277
UG-ELEC	MULTI-PHASE TRANSFORMER, 1500VA, 240V, 3-Phase	UG-ELEC-W-O	MULTI-PHASE TRANSFORMER, 1500VA, 240V, 3-Phase
UG-ELEC	UNDEVELOPED STRUCTURE	UG-ELEC-W-O	UNDEVELOPED STRUCTURE
UG-ELEC	SPOT ELEVATION	UG-ELEC-W-O	SPOT ELEVATION
UG-ELEC	CONTOURLINE	UG-ELEC-W-O	CONTOURLINE
UG-ELEC	FENCE	UG-ELEC-W-O	FENCE
UG-ELEC	CLANDANAL	UG-ELEC-W-O	CLANDANAL
UG-ELEC	STREET LIGHT	UG-ELEC-W-O	STREET LIGHT
UG-ELEC	SON	UG-ELEC-W-O	SON
UG-ELEC	CONCRETE	UG-ELEC-W-O	CONCRETE
UG-ELEC	ASPHALT	UG-ELEC-W-O	ASPHALT

REFERENCE DRAWINGS

WATER MAIN	TRANSMISSION MAP BY OJLM, APRIL 2018
WATER MAIN	TRANSMISSION MAP BY OJLM, MARCH 2007
ELECTRIC	LOCATION MAP DATE: JUNE 2011
GAS	TRANSMISSION MAP, CONSUMERS ENERGY, 11-18-2011
OTHER	PLATERS SQUARE LAND TITLE SURVEY, PEA GROUP, 11-18-2011, MARCH 2011
OTHER	LOD-MANN'S AT HUNTERS SQUARE AS B.L.T.E. ORCHARD PARK, H.L.T. & MCDONALD'S, INC.
OTHER	ORCHARD PLACE AS B.L.T.E. ORCHARD PARK, H.L.T. & MCDONALD'S, INC.
OTHER	HUNTERS SQUARE PARKING LOT AND DRIVEWAY RENOVATIONS AS B.L.T.E.
OTHER	ORCHARD PLACE REHABILITATION PROJECT AS B.L.T.E. 04-11-2011, UPDATED MAY 2015
OTHER	ORCHARD PLACE REHABILITATION PROJECT AS B.L.T.E. 04-11-2011, UPDATED MAY 2015
OTHER	ENGINEERS, INC. 10-15-2011, UPDATED DECEMBER 2012
OTHER	HUNTERS SQUARE, ONE 1/4" ON COMB. THIS ENGINEER, DECEMBER 2004
OTHER	ORCHARD LAKE ROAD REHABILITATION 13 MILE ROAD TO 14 MILE ROAD, HRC INC. OCTOBER 2019

KEY MAP

811 *Know what's below. Call before you dig.*

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KEY MAP

SITE SHEET KEY MAP

CLIENT
RPT REALTY
20750 CHMC CENTER DRIVE, SUITE #310
BENTLEYVILLE, MICHIGAN, 48314

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

NO.	DESCRIPTION	DATE
1-	5-PAUP-REGUL-6/15/2023	12/08/23

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
PRELIMINARY GRADING AND UTILITY PLAN - SHEET 4

PEA JOB NO. 22-0256

P.M. RLS

DN. KM

DES. PB

DRAWING NUMBER
C-7.4

SIDEWALK RAMP LEGEND:

SIDEWALK RAMP "TYPE D"	⊙
SIDEWALK RAMP "TYPE F"	⊙
SIDEWALK RAMP "TYPE P"	⊙
SIDEWALK RAMP "TYPE R"	⊙
CURB DROP ONLY	⊙

REFER TO LATEST EDITION OF MDOT 8-28 STANDARD PLAN FOR SIDEWALK RAMP DETAILS.

SOUTH DETENTION VAULT NOTE:
ORIGINAL SITE DESIGN (1979) APPEARS TO HAVE PROVIDED TWO STORMWATER DETENTION BASINS FOR THE NORTH RETAIL PARCEL AND TWO ADDITIONAL DETENTION BASINS FOR THE SOUTHERN RETAIL PARCEL (1981). IN 1994, THE SITE WAS RE-DEVELOPED AND ALL DRAINAGE FROM THE PREVIOUS FOUR BASINS WAS ROUTED TO A SINGLE U.G. DETENTION VAULT AT THE SOUTHEAST CORNER OF THE COMBINED PROPERTY. THAT VAULT REMAINS IN USE TODAY.

THE WEST BASIN CURRENTLY DRAINS A PORTION OF THE ADJACENT RESIDENTIAL PROPERTY TO THE WEST, AND THE OUTFLOW FROM THIS BASIN DRAINS THROUGH THIS PROPERTY AND INTO THE SOUTH VAULT, WHERE THE FLOW PASSES THROUGH.

NO ADDITIONAL DETENTION WAS REQUIRED FOR THE MCDONALDS SITE WHEN CONSTRUCTED (1994).

DETENTION FOR THE OLD NAVY PORTION OF THE SITE IS SEPARATE FROM THE REMAINDER OF THE PROPERTY.

PER THE ANALYSIS COMPLETED BY OJLM, IN 2006, A TOTAL REQUIRED VOLUME OF 135,434 C.F. WAS AGREED UPON FOR THIS DEVELOPMENT WITH AN ALLOWABLE OUTFLOW TO THE CITY'S SYSTEM USING A 12" UNRESTRICTED PIPE FOR A TOTAL OF 11.12 CFS, WHICH INCLUDES 5.0 CFS OF PASS THROUGH FLOW FROM THE HUNTER'S RIDGE RESIDENTIAL DEVELOPMENT TO THE WEST.

THE PROPOSED DEVELOPMENT SHALL EXPAND DETENTION STORAGE NEAR THE SOUTHERN VAULT BY ADDING 42336 LF OF 60" PIPE IMMEDIATELY NORTH OF THE EXISTING VAULT, AND BY RE-GRADING THE WEST BASIN TO MAXIMIZE THE STORAGE CAPACITY. DETAILED CALCULATIONS FOR STORAGE CAPACITY AND OUTFLOW RATES ARE PROVIDED ON SHEET C-12.0. IN SUMMARY:

ORIGINAL SOUTH VAULT STORAGE = 156,554 C.F.
ADDITIONAL 60" PIPE STORAGE = 45,867 C.F.
RE-GRADED WEST BASIN STORAGE = 20,481 C.F.

TOTAL ON-SITE DETENTION STORAGE = 222,902 C.F.
CITY REQUIRED STORAGE VOLUME (2" OVER SITE) = 218,599 C.F.

E 1/4 CORNER
SECTION 3
TOWN OF ROSE
CITY OF FARMINGTON HILLS
OAKLAND CO., MI

MATCHLINE - REFER TO SHEET C-7.5 FOR CONTINUATION

NOT FOR CONSTRUCTION

BENCHMARKS
(GPS DERIVED - NAVD83)

BM #300
DIMPLE ON ARROW ON HYDRANT AND ±109' DIRECTLY NORTHEASTERLY FROM NORTHEAST BUILDING CORNER STORE BUY BUY BABY.
ELEV. - 877.55

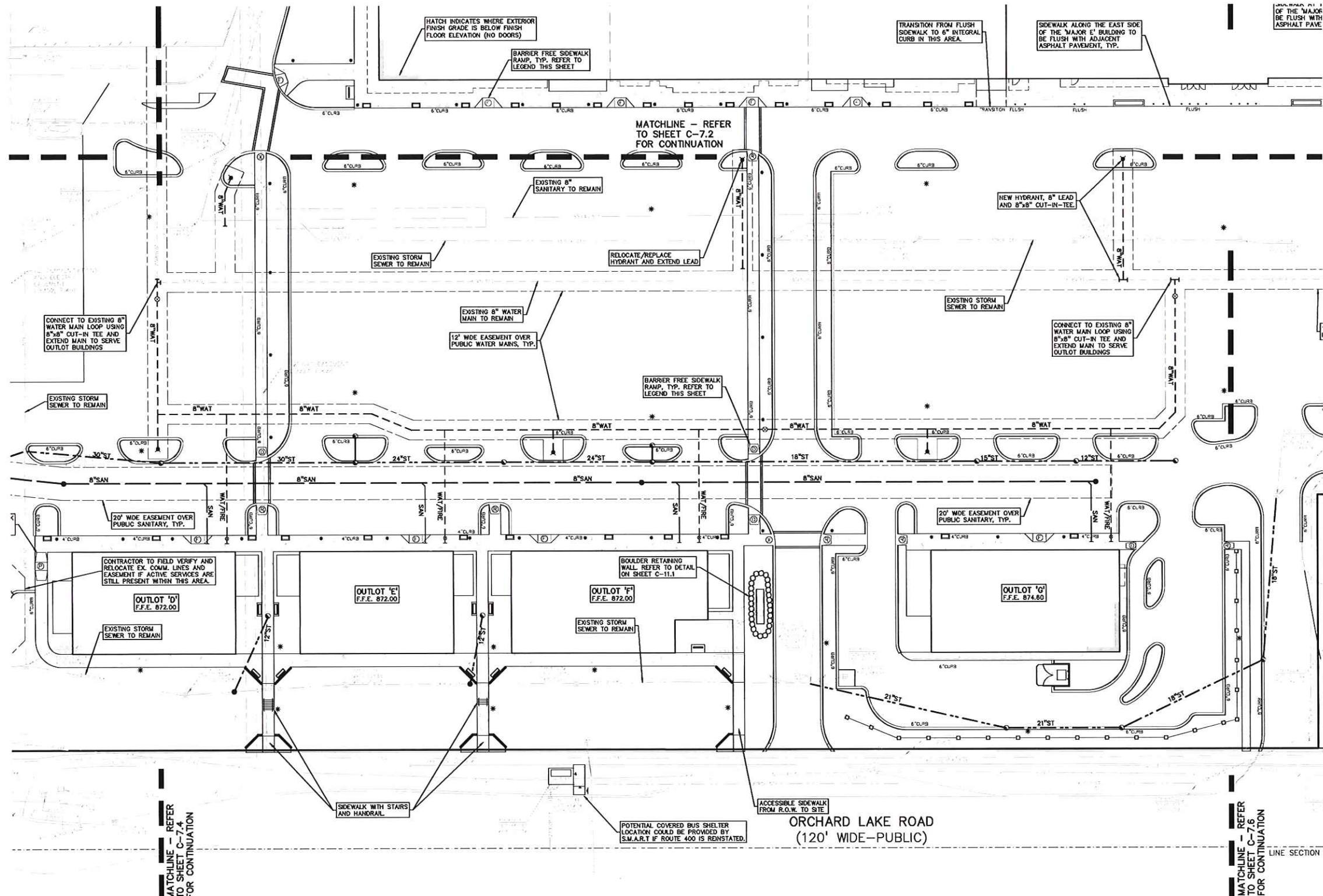
BM #301
DIMPLE ON ARROW ON HYDRANT AND ±39' DIRECTLY WEST FROM SOUTHWEST BUILDING CORNER AT SOUTHWEST CORNER OF SITE.
ELEV. - 874.61

BM #302
LIGHT POLE BASE IN SHOPPING MALL ±187' DIRECTLY SOUTH FROM MCDONALD'S RESTAURANT.
ELEV. - 875.55

FLOODPLAIN NOTE:
BY GRAPHIC PLOTTING, THE SUBJECT PARCEL IS IN AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN (ZONE X) PER FEMA FLOOD INSURANCE RATE MAP PANEL NUMBER 21250513F EFFECTIVE SEPTEMBER 29, 2006.

PEA GROUP
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STATE OF MICHIGAN
RACHEL SMITH
PROFESSIONAL ENGINEER
6201048838



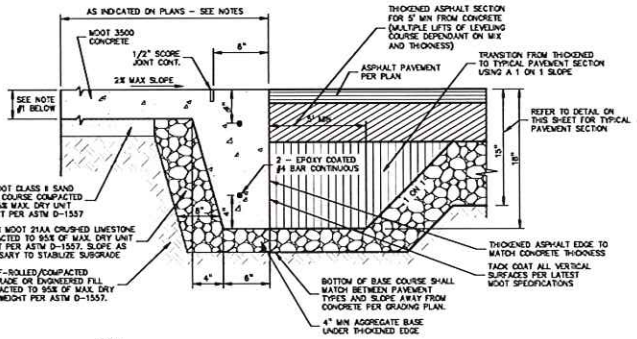
NOT FOR CONSTRUCTION

PAVING NOTES:

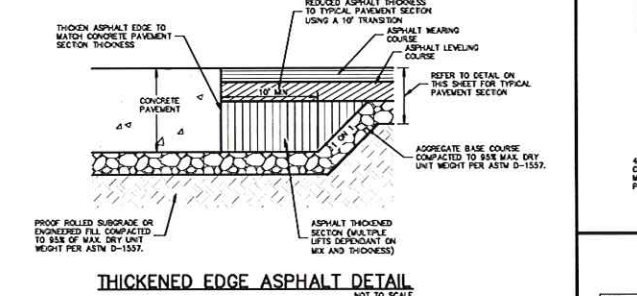
- IN AREAS WHERE NEW PAVEMENTS ARE BEING CONSTRUCTED, THE TOPSOIL AND SOIL CONTAINING ORGANIC MATTER SHALL BE REMOVED PRIOR TO PAVEMENT CONSTRUCTION.
- REFER TO ARCHITECTURAL PLANS FOR DETAILS OF FROST SLAB AT EXTERIOR BUILDING DOORS.
- CONSTRUCTION TRAFFIC SHOULD BE MINIMIZED ON THE NEW PAVEMENT. IF CONSTRUCTION TRAFFIC IS ANTICIPATED ON THE PAVEMENT STRUCTURE, THE INITIAL LIFT THICKNESS COULD BE INCREASED AND PLACEMENT OF THE FINAL LIFT COULD BE DELAYED UNTIL THE MAJORITY OF THE CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED. THIS ACTION WILL ALLOW REPAIR OF LOCALIZED DAMAGE, IF ANY DOES OCCUR, AS WELL AS REDUCE LOAD DAMAGE ON THE PAVEMENT SYSTEM.
- ALL EXPANSION JOINTS AND CONCRETE PAVEMENT JOINTS TO BE SEALED.
- CONCRETE PAVEMENT JOINTING - UNLESS SHOWN OTHERWISE IN THE PLANS OR REQUIRED BY THE AUTHORITY HAVING JURISDICTION:
 - WHERE PROPOSED CONCRETE ABUTS A STRUCTURE, PROVIDE A MINIMUM 1/2" EXPANSION JOINT. THE JOINT FILLER BOARD MUST BE AT LEAST THE FULL DEPTH OF THE CONCRETE AND HELD DOWN A 1/2" TO ALLOW FOR SEALING.
 - WHERE PROPOSED CONCRETE ABUTS EXISTING CONCRETE OR IN BETWEEN POURS OF PROPOSED CONCRETE (CONSTRUCTION JOINT), PROVIDE 5/8" DOMELS EVERY 30' CENTER TO CENTER HALF WAY ALONG THE THICKNESS OF THE PROPOSED PAVEMENT. ALTERNATE DOMELS SIZES AND SPACING MUST BE APPROVED THE ENGINEER PRIOR TO COMMENCING WORK AND VIA THE SUBMITTAL PROCESS.
 - WHERE PROPOSED CONCRETE ABUTS EXISTING OR PROPOSED SIDEWALK OR CURBING, PROVIDE A MINIMUM 1/2" EXPANSION JOINT.
 - CONTROL LONGITUDINAL AND/OR TRANSVERSE JOINTS SHALL BE PLACED TO PROVIDE PANELS WITHIN THE PAVEMENT AS SQUARE AS POSSIBLE WITH THE FOLLOWING MAXIMUM SPACING PARAMETERS:
 - 5.4.1. 8-INCH THICK CONCRETE PAVEMENT: 12' X 12'
 - 5.4.2. 8-INCH THICK CONCRETE PAVEMENT: 15' X 15'
 - 5.4.3. 4-INCH THICK CONCRETE PAVEMENT: 12' X 12'
 - 5.4.4. 4-INCH THICK CONCRETE PAVEMENT: 15' X 15'
 - IRREGULAR-SHAPED PANELS MAY REQUIRE THE USE OF REINFORCING MESH OR FIBER MESH AS DETERMINED BY THE ENGINEER. THE USE OF MESH MUST BE APPROVED THE ENGINEER PRIOR TO COMMENCING WORK AND VIA THE SUBMITTAL PROCESS.
 - IF A JOINT PLAN IS NOT PROVIDED IN THE PLANS, THE CONTRACTOR SHALL SUBMIT ONE TO THE ENGINEER FOR REVIEW PRIOR TO COMMENCING WORK AND VIA THE SUBMITTAL PROCESS.
- CONCRETE CURBING JOINTING - UNLESS SHOWN OTHERWISE IN THE PLANS OR REQUIRED BY THE AUTHORITY HAVING JURISDICTION:
 - JOINTS WHEN ADJACENT TO ASPHALT PAVEMENT:
 - 6.1.1. PLACE CONTRACTION JOINTS AT 10' INTERVALS
 - 6.1.2. PLACE 1/2" EXPANSION JOINT AT CATCH BASINS, EXISTING AND PROPOSED SIDEWALK OR EXISTING CURBING.
 - 6.1.3. PLACE 1" EXPANSION JOINT:
 - 6.1.3.1. AT SPRING POINTS OF INTERSECTIONS OR ONE OF THE END OF RADIUS LOCATIONS IN A CURVE
 - 6.1.3.2. AT 400' MAXIMUM INTERVALS ON STRAIGHT RUNS
 - 6.1.3.3. AT THE END OF RADIUS AT OPPOSITE ENDS IN A CURVED LANDSCAPE ISLAND
 - JOINTS WHEN TIED TO CONCRETE PAVEMENT:
 - 6.2.1. PLACE CONTRACTION JOINTS OPPOSITE ALL TRANSVERSE CONTRACTION JOINTS IN PAVEMENT
 - 6.2.2. PLACE 1/2" EXPANSION JOINT AT CATCH BASINS, EXISTING AND PROPOSED SIDEWALK OR EXISTING CURBING.
 - 6.2.3. PLACE 1" EXPANSION JOINT OPPOSITE ALL TRANSVERSE EXPANSION JOINTS IN PAVEMENT
 - 6.2.4. CURB AND GUTTER AND CONCRETE SHALL BE TIED TOGETHER SIMILAR TO A LONGITUDINAL LANE THE JOINT (MOOT BI JOINT)
 - IN BETWEEN POURS OF PROPOSED CONCRETE CURBING (CONSTRUCTION JOINT):
 - 6.3.1. CARRY THE REBAR CONTINUOUSLY BETWEEN POURS
 - 6.3.2. IF THE REBAR IS NOT LONG ENOUGH TO CARRY CONTINUOUSLY, THEN TIE TWO PIECES OF REBAR PER THE LATEST MOOT SPECIFICATIONS
- CONCRETE SIDEWALK JOINTING - UNLESS SHOWN OTHERWISE IN THE PLANS OR REQUIRED BY THE AUTHORITY HAVING JURISDICTION:
 - PLACE TRANSVERSE CONTRACTION JOINTS EQUAL TO THE WIDTH OF THE WALK WHEN WIDTH IS LESS THAN 8'
 - PLACE TRANSVERSE AND LONGITUDINAL CONTRACTION JOINTS EQUAL TO 1/2 THE WIDTH OF THE WALK WHEN WIDTH IS EQUAL TO OR GREATER THAN 8'
 - PLACE 1" EXPANSION JOINT WHERE ABUTTING SIDEWALK RAMP AND/OR RADIUS IN INTERSECTION
 - PLACE TRANSVERSE 1/2" EXPANSION JOINT AT MAXIMUM OF 100' SPACING
 - PLACE 1/2" EXPANSION JOINT WHEN ABUTTING A FIXED STRUCTURE, OTHER PAVEMENT (CONCRETE PAVEMENT AND DRIVE APPROACHES), UTILITY STRUCTURES, LIGHT POLE BASES AND COLUMNS

CONSTRUCTION MATERIAL SUBMITTALS

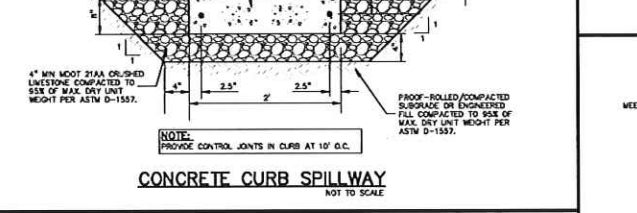
- UNLESS REQUIRED OTHERWISE IN THE PROJECT SPECIFICATIONS, THE CONTRACTOR SHALL ONLY SUBMIT THE FOLLOWING CONSTRUCTION MATERIAL SUBMITTALS, AS APPLICABLE TO THE PLANS, FOR REVIEW BY THE ENGINEER. UNLESS APPROVED IN ADVANCE AND IN WRITING BY THE ENGINEER, ANY MATERIAL SUBMITTALS PROVIDED TO THE ENGINEER FOR REVIEW IN ADDITION TO THIS LIST SHALL BE RETURNED TO THE CONTRACTOR WITHOUT A REVIEW BEING PERFORMED.
- SOIL EROSION AND SEDIMENTATION CONTROL MEASURES
 - UTILITY TRENCH BACKFILL MATERIAL WITH ALL MATERIAL DATA INCLUDED IN THE SUBMITTAL BEING DATED WITHIN 60 DAYS OF THE SUBMITTAL UNLESS APPROVED OTHERWISE BY THE ENGINEER
 - STORM AND SANITARY SEWER PIPING INCLUDING JOINTS
 - STORM AND SANITARY SEWER STRUCTURES
 - STORM AND SANITARY SEWER STRUCTURE FRAME AND COVERS INCLUDING CLEAN OUTS
 - WATER DISTRIBUTION SYSTEM PIPING INCLUDING JOINTS
 - WATER DISTRIBUTION SYSTEM STRUCTURES
 - WATER DISTRIBUTION SYSTEM STRUCTURE FRAME AND COVERS
 - WATER DISTRIBUTION SYSTEM SHUT OFF BOXES
 - WATER DISTRIBUTION SYSTEM FIRE HYDRANTS
 - WATER DISTRIBUTION SYSTEM GATE VALVES
 - STORM WATER MANAGEMENT OUTLET CONTROL STRUCTURES INCLUDING COVERS OR GRATES
 - STORM WATER MANAGEMENT OUTLET SEDIMENTATION BASIN RISERS INCLUDING GRATES
 - STORM WATER MANAGEMENT MECHANICAL PRE-TREATMENT UNITS INCLUDING COVERS
 - SANITARY SEWER OIL/GREASE SEPARATORS
 - STORM WATER MANAGEMENT UNDERGROUND DETENTION SYSTEM MATERIAL AND SHOP DRAWINGS DEPICTING THE LAYOUT OF THE SYSTEM
 - PAVEMENT AGGREGATE BASE MATERIAL WITH ALL MATERIAL DATA INCLUDED IN THE SUBMITTAL BEING DATED WITHIN 60 DAYS OF THE SUBMITTAL UNLESS APPROVED OTHERWISE BY THE ENGINEER
 - PAVEMENT UNDERDRAIN MATERIAL AND BACKFILL WITH ALL BACKFILL MATERIAL DATA INCLUDED IN THE SUBMITTAL BEING DATED WITHIN 60 DAYS OF THE SUBMITTAL UNLESS APPROVED OTHERWISE BY THE ENGINEER
 - PAVEMENT MIX DESIGNS SUBMITTED FOR REVIEW BY THE ENGINEER MUST FOLLOW THE CURRENT MOOT REVIEW CHECKLISTS AS SUMMARIZED BELOW AND ALL MATERIAL DATA INCLUDED IN THE SUBMITTAL BEING DATED WITHIN 60 DAYS OF THE SUBMITTAL UNLESS APPROVED OTHERWISE BY THE ENGINEER:
 - CONCRETE MIX DESIGN REVIEW CHECKLIST (FORM 2000)
 - SUBGRADE MIX DESIGN CHECKLIST (FORM 1849)
 - MARSHALL MIX DESIGN CHECKLIST (FORM 1849)
 - SITE FENCING AND GATES INCLUDING FOOTINGS
 - SITE RAILINGS INCLUDING FOOTING OR EMBEDMENTS
 - ANY ITEMS SHOWN IN THE PLANS OR DETAIL SHEETS THAT SPECIFICALLY STATE FOR THE CONTRACTOR TO SUBMIT A SHOP DRAWING TO THE ENGINEER FOR REVIEW. THESE ITEMS INCLUDE, BUT ARE NOT LIMITED TO:
 - RETAINING WALL MATERIAL AND STRUCTURAL CALCULATIONS
 - TRENCH DRAIN MATERIAL AND SHOP DRAWING DEPICTING THE LAYOUT OF THE SYSTEM
 - ANY SPECIALTY ITEMS SHOWN IN THE PLANS OR DETAIL SHEETS THAT SPECIFICALLY DO NOT STATE FOR THE CONTRACTOR SHALL SUBMIT A SHOP DRAWING TO THE ENGINEER FOR REVIEW BUT THE CONTRACTOR REQUESTS TO BE REVIEWED. THE CONTRACTOR'S REQUEST FOR REVIEW MUST BE IN WRITING AND APPROVED BY THE ENGINEER PRIOR TO SUBMITTING THE INFORMATION.



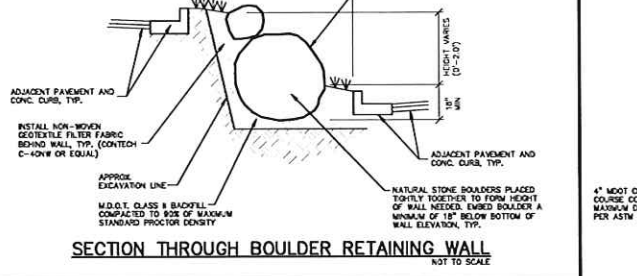
THICKENED EDGE SIDEWALK TO FLUSH ASPHALT DETAIL
NOT TO SCALE



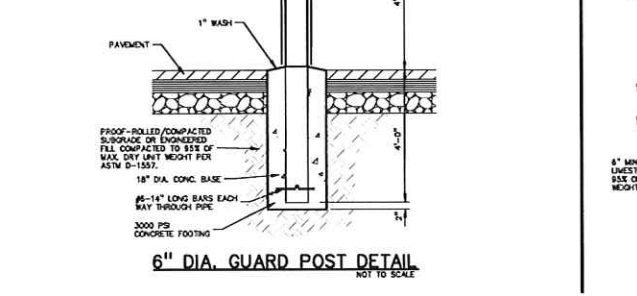
THICKENED EDGE ASPHALT DETAIL
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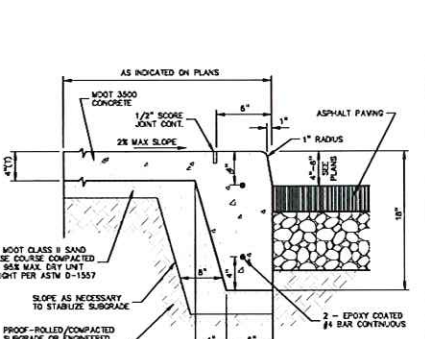
CONCRETE CURB SPILLWAY
NOT TO SCALE



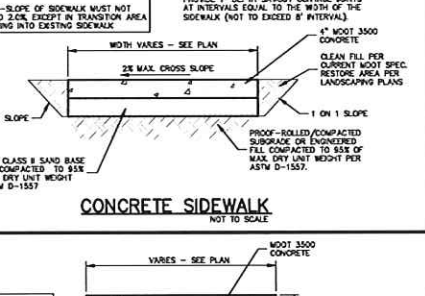
SECTION THROUGH BOULDER RETAINING WALL
NOT TO SCALE



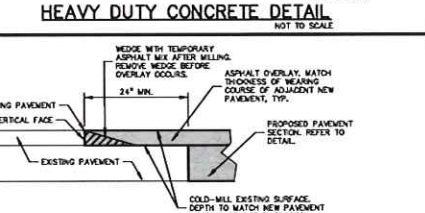
6" DIA. GUARD POST DETAIL
NOT TO SCALE



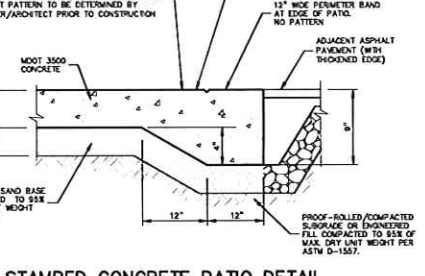
INTEGRAL CURB AND SIDEWALK
NOT TO SCALE



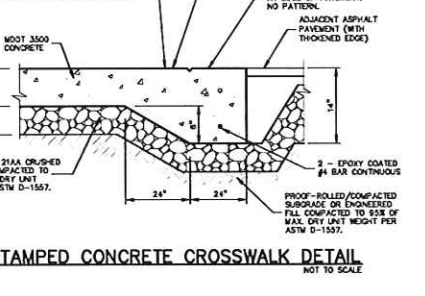
CONCRETE SIDEWALK
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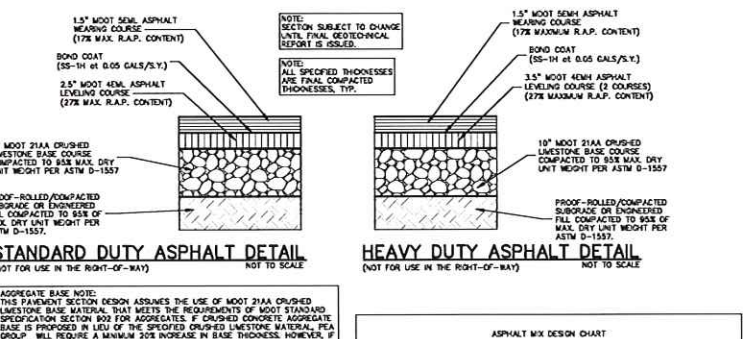
HEAVY DUTY CONCRETE DETAIL
NOT TO SCALE



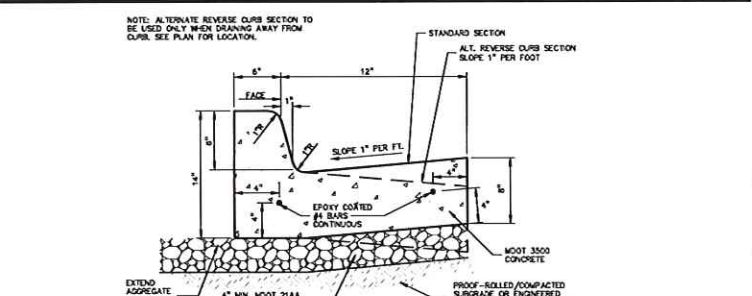
BUTT JOINT DETAIL
NOT TO SCALE



STAMPED CONCRETE PATIO DETAIL
NOT TO SCALE



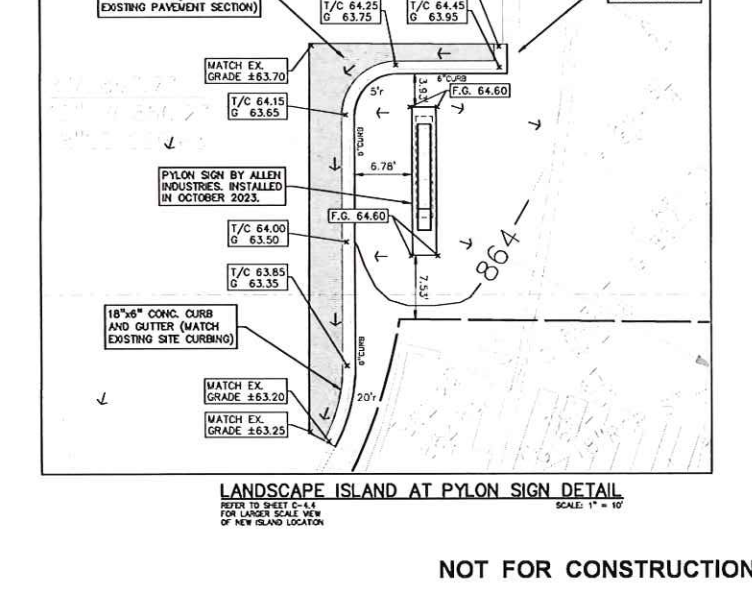
STANDARD DUTY ASPHALT DETAIL
NOT TO SCALE



HEAVY DUTY ASPHALT DETAIL
NOT TO SCALE



18" x 6" STANDARD CONCRETE CURB AND GUTTER
NOT TO SCALE



LANDSCAPE ISLAND AT PYLON SIGN DETAIL
SCALE: 1" = 10'



CAUTION!
THE LOCATION AND DEPTHS OF EXISTING UTILITIES ARE SHOWN ON THESE PLANS FOR INFORMATION ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND DEPTHS OF ALL UTILITIES PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

CLIENT
RPT REALTY
2075 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 1/4 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS
1 - SPAPUD RESUBMISSION 12/08/23

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
NOTES AND DETAILS

PEA JOB NO. 22-0256

P.M. RLS
DN. KM
DES. PB
DRAWING NUMBER

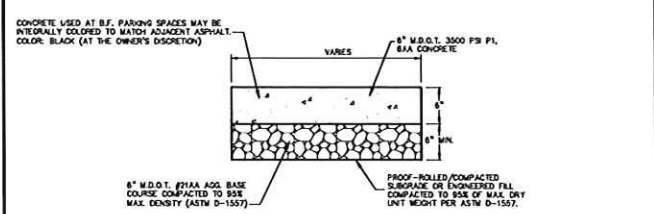
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NOT FOR CONSTRUCTION

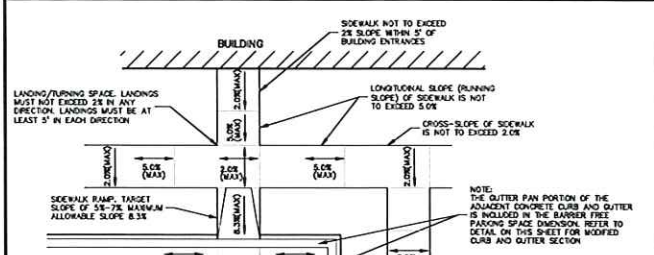
GENERAL BARRIER FREE NOTES:

THE FOLLOWING NOTES PROVIDE AN OUTLINE OF SOME OF THE REQUIREMENTS CONTAINED WITHIN THE "STANDARDS FOR ACCESSIBLE DESIGN - AMERICANS WITH DISABILITIES ACT 2010", AND "ACCESSIBLE AND USABLE BUILDINGS AND FACILITIES", ICC/ANSI A117.1-2009. THE CONTRACTOR IS RESPONSIBLE FOR ALL OF THE REQUIREMENTS PRESENTED WITHIN THESE DOCUMENTS, WHICH ARE AVAILABLE IN FULL UPON REQUEST.

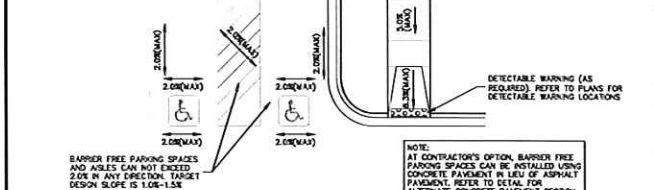
1. AN ACCESSIBLE ROUTE CONSISTS OF WALK SURFACES, CURB RAMP AND RAMPS. AT LEAST ONE ACCESSIBLE ROUTE SHALL BE PROVIDED WITHIN THE SITE FROM ACCESSIBLE PARKING SPACES AND SIDEWALKS, TO THE BUILDING OR FACILITY ENTRANCE THEY SERVE.
2. THE RUNNING SLOPE OF ALL WALKING SURFACES SHALL NOT EXCEED 5% (1:20) AND THE CROSS-SLOPE SHALL NOT EXCEED 2% (1:48).
3. WALKING SURFACES MUST BE LEVEL WITH PERMITTED VERTICAL CHANGES IN LEVEL NOT TO EXCEED 1/4", OR BEVELED CHANGES IN LEVEL NOT TO EXCEED 1/2". REFER TO DETAIL ON THIS SHEET. ANY CHANGE IN LEVEL GREATER THAN 1/2" MUST BE RAMPED.
4. TURNING SPACES ALONG ACCESSIBLE ROUTES MUST BE AT LEAST 5 FEET WIDE IN ALL DIRECTIONS AND NOT EXCEED 2% SLOPE (1:48) IN ANY DIRECTION.
5. ACCESSIBLE ROUTES WILL BE DESIGNED TO BE A MINIMUM OF 5 FEET WIDE. THE MINIMUM CLEAR WIDTH IS 3 FEET.
6. RAMPS ALONG ACCESSIBLE ROUTES WILL HAVE A RUNNING SLOPE GREATER THAN 5% (1:20) AND LESS THAN 8.3% (1:12).
7. THE CROSS-SLOPE OF RAMP RUNS SHALL NOT EXCEED 2% (1:48).
8. THE MINIMUM CLEAR WIDTH OF ANY RAMP IS 36 INCHES.
9. THE MAXIMUM RISE FOR ANY RAMP (NOT INCLUDING CURB RAMPS) SHALL NOT EXCEED 30 INCHES. LANDINGS ARE REQUIRED AT THE TOP AND BOTTOM OF EACH RAMP. LANDINGS SHALL HAVE A CROSS-SLOPE NOT EXCEEDING 2% (1:48). SHALL BE 5 FEET LONG AND AT LEAST AS WIDE AS THE RAMP CLEAR WIDTH. IF THERE IS A CHANGE OF DIRECTION AT A LANDING, THEN THE LANDING MUST BE AT LEAST 5 FEET WIDE AND 5 FEET LONG.
10. HANDRAILS ARE REQUIRED FOR ANY RAMP (NOT INCLUDING CURB RAMPS) WITH A RISE GREATER THAN 6 INCHES. ALL HANDRAILS ARE REQUIRED TO HAVE EDGE PROTECTION UNLESS ADJOINING ANOTHER ACCESS POINT OR IF THE VERTICAL DROP-OFF AT THE EDGE OF THE RAMP DOES NOT EXCEED 1/2" IN 10 FEET. EDGE PROTECTION CAN BE PROVIDED BY MEANS OF A 4" MIN. CURB OR BARRIER.
11. CURB RAMPS ALONG ACCESSIBLE ROUTES SHALL NOT RISE MORE THAN 6 INCHES, NOR BE STEEPER THAN 8.3% (1:12). APPROXIMATING SLOPES TO THE RAMP CANNOT EXCEED 5%, WHICH INCLUDES SIDEWALKS, PAVEMENT, CUTTERS ETC.
12. IF CURB RAMP SIDES ARE FLARED, THE FLARES SHALL NOT BE STEEPER THAN 10% (1:10).
13. LANDINGS ARE REQUIRED AT THE TOP OF ALL CURB RAMPS. THE CLEAR LENGTH OF THE LANDING SHALL BE A MINIMUM OF 3' AND WILL BE AS WIDE AS THE CURB RAMP.
14. CURB RAMPS SHALL BE LOCATED OR PROTECTED TO PREVENT THEIR OBSTRUCTION BY PARKED VEHICLES.
15. HANDRAILS ARE NOT REQUIRED ON CURB RAMPS.
16. WHERE DETECTABLE WARNING IS REQUIRED AT CURB RAMPS, THE DETECTABLE WARNING SHALL BE 24" MINIMUM IN DEPTH AND SHALL EXTEND THE FULL WIDTH OF THE RAMP. THE DETECTABLE WARNING SHALL BE LOCATED SO THE EDGE NEAREST THE CURB IS 8 INCHES MINIMUM AND 8 INCHES MAXIMUM FROM THE CURB LINE.
17. ACCESSIBLE PARKING SPACES ON SITE SHALL BE PROVIDED AS REQUIRED IN SECTION 502 OF THE A.D.A. IF THE SITE HAS MORE THAN ONE PARKING FACILITY, EACH FACILITY IS REQUIRED TO MEET THESE REQUIREMENTS SEPARATELY. THE REQUIRED NUMBER OF SPACES SHALL BE BASED ON THE TOTAL NUMBER OF PARKING SPACES IN EACH PARKING FACILITY ON SITE.
18. FOR EVERY SIX OR FRACTION OF SIX ACCESSIBLE PARKING SPACES, ONE VAN ACCESSIBLE SPACE SHALL BE PROVIDED.
19. ACCESSIBLE PARKING SPACES SHALL BE LOCATED ON THE SHORTEST ACCESSIBLE ROUTE FROM PARKING TO A BUILDING ENTRANCE. IF THERE IS MORE THAN ONE ACCESSIBLE ENTRANCE, PARKING SHALL BE DISPERSED ALONG THE SHORTEST ACCESSIBLE ROUTE TO THE ACCESSIBLE ENTRANCES.
20. BARRIER FREE CAR PARKING SPACES SHALL BE A MINIMUM OF 8 FEET WIDE WITH AN ACCESS AISLE 5 FEET WIDE MINIMUM. VAN ACCESSIBLE PARKING SPACES SHALL BE AT LEAST 11 FEET WIDE WITH A 5' WIDE ACCESS AISLE. VAN ACCESSIBLE SPACES ARE ALSO ACCEPTABLE WITH AN 8 FOOT WIDTH AND 8 FOOT WIDE ACCESS AISLE. THE ACCESS AISLE IN ALL CASES MUST EXTEND THE FULL LENGTH OF THE PARKING SPACE.
21. SURFACE SLOPES WITHIN THE PARKING SPACES AND AISLES SHALL NOT EXCEED 2% (1:48).
22. ACCESSIBLE AREAS INCLUDING PARKING SPACES, AISLES AND PATHWAYS, REQUIRE A MINIMUM VERTICAL CLEARANCE OF 98 INCHES.
23. ACCESSIBLE PARKING SPACES ARE REQUIRED TO BE IDENTIFIED BY SIGNS. THE SIGNS SHALL INCLUDE THE INTERNATIONAL SYMBOL OF ACCESSIBILITY. VAN PARKING SPACES ARE REQUIRED TO BE DESIGNATED AS "VAN ACCESSIBLE". REFER TO DETAILS ON THIS SHEET.



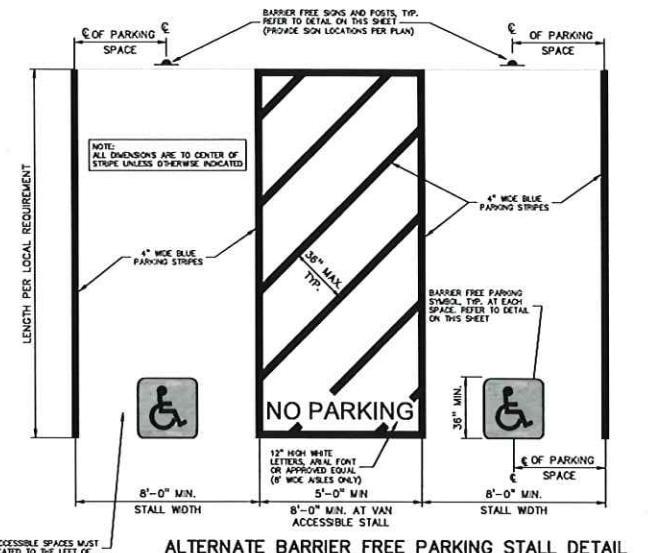
BARRIER FREE PARKING SPACE ALTERNATE CONCRETE PAVEMENT DETAIL
NOT TO SCALE



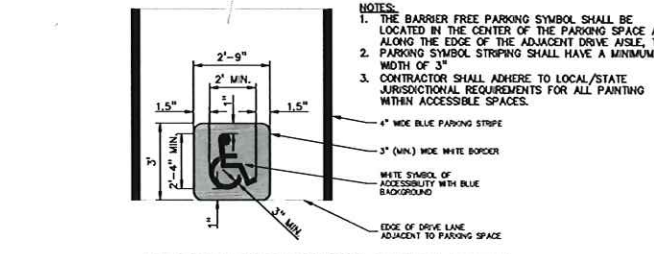
ALTERNATE BARRIER FREE PARKING STALL DETAIL
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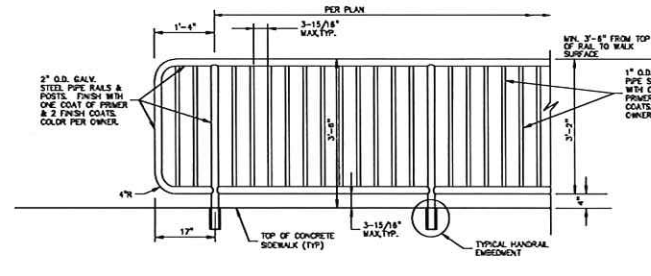
PARKING SPACE GRADING INFORMATION
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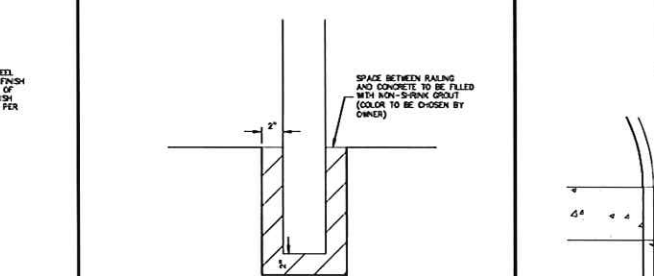
ALTERNATE BARRIER FREE PARKING STALL DETAIL
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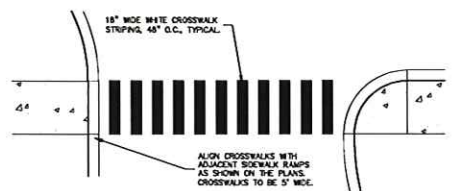
BARRIER FREE PARKING SYMBOL DETAIL (INTERNATIONAL SYMBOL OF ACCESSIBILITY)
NOT TO SCALE



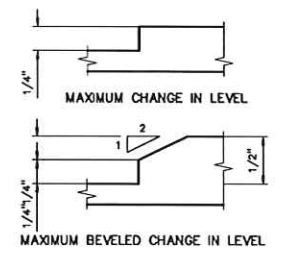
PEDESTRIAN GUARDRAIL
NOT TO SCALE



HANDRAIL EMBEDMENT DETAIL
NOT TO SCALE



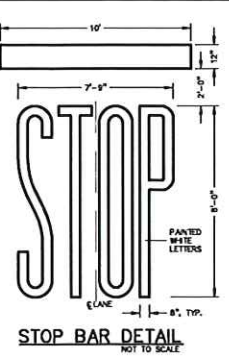
STRIPED CROSSWALK DETAIL
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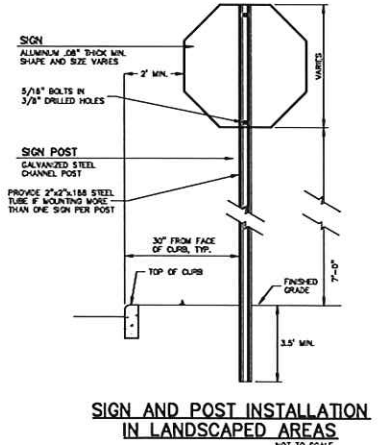
BARRIER FREE SURFACE CHANGE IN LEVEL TOLERANCE
NOT TO SCALE



PICKUP PARKING SIGN DETAIL
NOT TO SCALE



STOP BAR DETAIL
NOT TO SCALE



SIGN AND POST INSTALLATION IN LANDSCAPED AREAS
NOT TO SCALE



ELECTRIC VEHICLE PARKING SIGN DETAIL
NOT TO SCALE



NO PARKING SIGN DETAIL
NOT TO SCALE



DO NOT ENTER SIGN DETAIL
NOT TO SCALE



STOP SIGN DETAIL
NOT TO SCALE

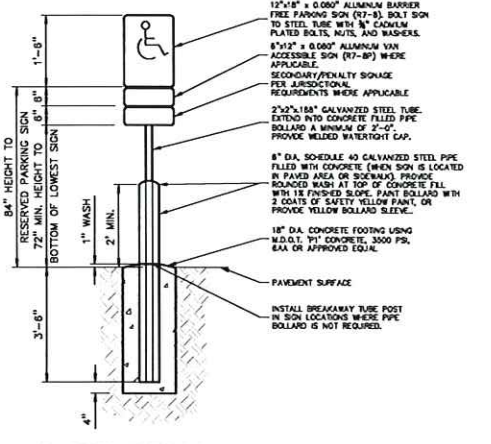


ONE WAY SIGN DETAIL
NOT TO SCALE



CROSSWALK SIGN DETAIL
NOT TO SCALE

- BARRIER FREE SIGN NOTES:**
1. ONE SIGN IS REQUIRED AT EACH BARRIER FREE PARKING SPACE.
 2. ALL SIGNS SHALL COMPLY WITH THE LATEST STANDARDS OF THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
 3. WHEN TWO BARRIER FREE PARKING SPACES ARE ADJACENT AND FACING EACH OTHER, TWO SIGNS ARE REQUIRED, BUT CAN BE MOUNTED ON THE SAME POST.
 4. SIGN POSTS SHALL BE 2" NOM. SQUARE 14-GAUGE GALVANIZED STEEL TUBE WITH 7/16" HOLES AT 1" CENTERS. POSTS SHALL TELESCOPE INSIDE ANCHOR POSTS A MINIMUM OF 12".
 5. ANCHOR POSTS SHALL BE 2.25" NOM. SQUARE 12-GAUGE GALVANIZED STEEL POST, A MINIMUM OF 3 FEET LONG.
 6. IF THESE NOTES AND DETAILS CONFLICT WITH LOCAL CODES AND ORDINANCES, THE STRICTER REQUIREMENT SHOULD BE USED.
 7. ALTERNATE MATERIALS MAY BE USED IF IN COMPLIANCE WITH A.D.A. GUIDELINES AND LOCAL REQUIREMENTS.



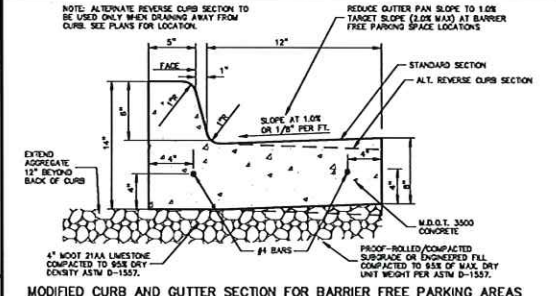
BARRIER FREE SIGN AND POST DETAIL
NOT TO SCALE



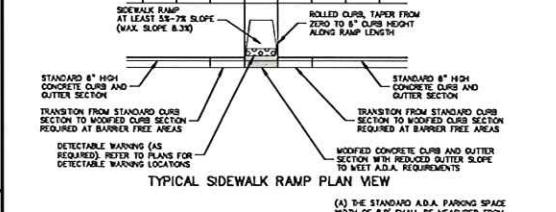
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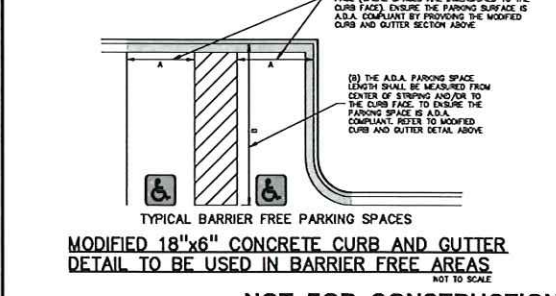
VAN ACCESSIBLE PARKING SIGN DETAIL
NOT TO SCALE



MODIFIED CURB AND GUTTER SECTION FOR BARRIER FREE PARKING AREAS



TYPICAL SIDEWALK RAMP PLAN VIEW



TYPICAL BARRIER FREE PARKING SPACE MODIFIED 18\"/>



CAUTION!
THE EXISTING UTILITIES AND ELEVATIONS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS MADE FOR THE ACCURACY OF THE INFORMATION SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
BOTHWELL MICHIGAN 48306

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS MICHIGAN 48334

REVISIONS

1-SP4RUP RESUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
DETAILS

PEA JOB NO.	22-0256
P.M.	RLS
DN.	KM
DES.	PB
DRAWING NUMBER	

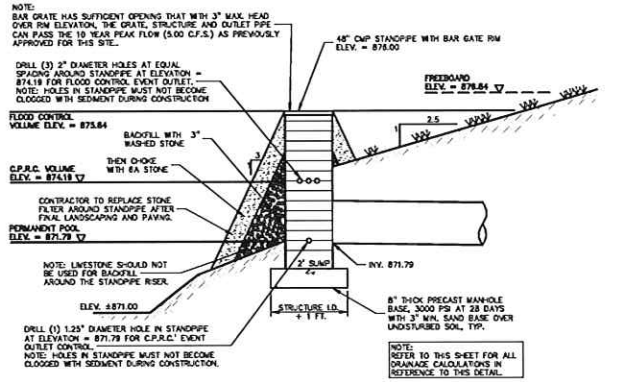
C-11.2

NOT FOR CONSTRUCTION

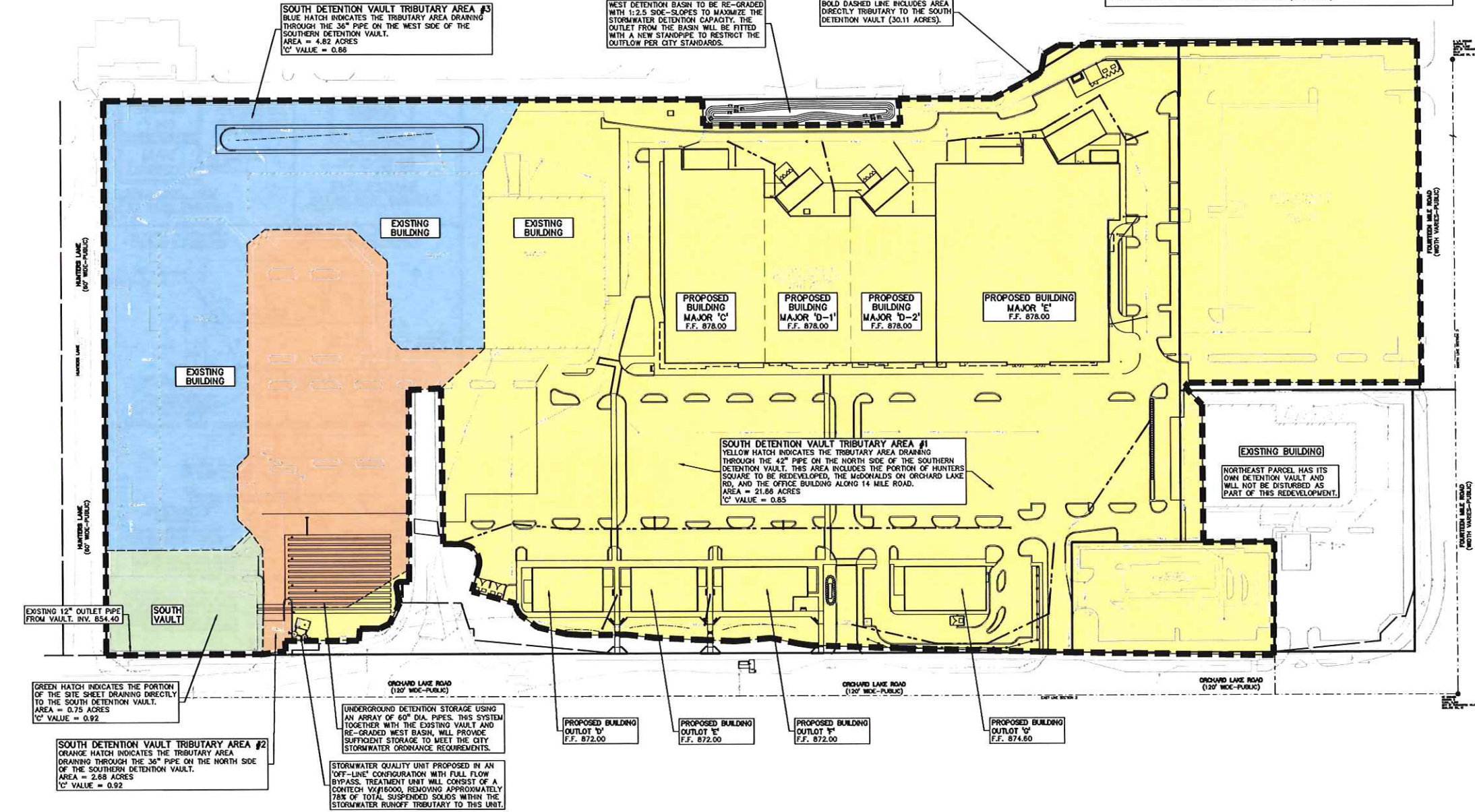
STORMWATER CALCULATIONS FOR SOUTH VAULT AND PIPE STORAGE:
 TRIBUTARY AREA TO STORAGE SYSTEM = 30.11 ACRES
 RUNOFF COEFFICIENT 'C' = 0.86
CITY REQUIRED FLOOD CONTROL STORAGE VOLUME FOR ENTIRE PROPERTY:
 2" OF STORMWATER VOLUME ACROSS THE TRIBUTARY AREA
 30.11 ACRES x 2" OVER THE SITE = 218,999 C.F.
CHANNEL PROTECTION RATE CONTROL (CPRC) VOLUME FOR ENTIRE PROPERTY:
 CPRC = 6897 x AREA x C = 6897 x 30.11 x 0.86 = 178,595 C.F.
 NOTE: DETENTION VOLUME OF 19,384 C.F. AND CPRC STORAGE OF 8839 C.F. IS TO BE STORED IN THE WEST BASIN. THEREFORE, NET STORAGE VOLUMES REQUIRED IN THE SOUTH VAULT AND PIPE SYSTEM ARE:
 NET FLOOD CONTROL VOLUME REQUIRED (218,999 - 19,384) = 199,215 C.F.
 NET CPRC EVENT VOLUME REQUIRED (178,595 - 8,839) = 173,909 C.F.
 REFER TO STAGE-STORAGE TABLE ON THIS SHEET FOR THE COMBINED VAULT AND PIPE SYSTEM.
MAXIMUM STORAGE VOLUME PROVIDED ON SITE = 222,902 C.F.
 CITY FLOOD CONTROL VOLUME STORED AT ELEVATION 862.14
 NET CPRC VOLUME STORED AT ELEVATION = 863.68
 1/2 NET CPRC VOLUME (86,955 C.F.) STORED AT ELEVATION = 857.95
SOUTH VAULT OUTLET CONTROL DESIGN:
 WHEN CALCULATING THE RESTRICTOR REQUIRED FOR THE AVERAGE RELEASE RATE, THE HEAD OVER THE RESTRICTOR MUST BE CALCULATED ASSUMING THE VAULT IS STORING HALF OF THE CPRC VOLUME.
 AVERAGE RELEASE RATE = NET CPRC VOLUME/48 HOURS = 1.01 CFS
 1/2 CPRC VOLUME (86,955 C.F.) STORAGE ELEVATION = 857.95
 12" PIPE OUTLET INVERT = 854.40
 HEAD OVER 12" OUTLET PIPE INVERT (857.95 - 854.40) = 3.55'
 USING ORIFICE EQUATION, $Q = 0.62 \times A \times (2 \times g \times h)^{3/2}$
 AND SOLVING FOR 'Q', AVERAGE OUTLET FLOW = 7.38 CFS
 WITH FLOW RATE OF 7.38 CFS, THE CPRC VOLUME WILL BE WATER IN 6.6 HOURS.
 FLOOD CONTROL EVENT OUTLET SHALL NOT EXCEED 11.12 CFS CURRENTLY ALLOWED BY CITY.
 FLOOD CONTROL STORAGE ELEVATION = 862.14
 MAX. HEAD OVER 12" OUTLET PIPE (862.14 - 854.40) = 7.74'
 USING ABOVE ORIFICE EQUATION, SOLVING FOR FLOW, Q = 10.87 CFS

STORMWATER QUALITY CALCULATION:
 TRIBUTARY AREA TO WATER QUALITY DEVICE, A = 21.86 ACRES (YELLOW SHADED AREA)
 RUNOFF COEFFICIENT 'C' = 0.85
 TIME OF CONCENTRATION AT WATER QUALITY STRUCTURE = 28 MINUTES
 RAINFALL INTENSITY FOR O.C.W.R.C. 1-YEAR EVENT, I = 30.20/(Tc + 9.17)^0.81
 USING Tc OF 28 MINUTES, RAINFALL INTENSITY = 1.69" PER HOUR.
 PEAK 1-YR FLOW RATE, Q = CIA = 0.85 x 1.69 x 21.86 = 31.40 CFS
 RECENTLY ADOPTED CITY STANDARDS REQUIRE THE ABOVE FLOW RATE BE TREATED BY A WATER QUALITY DEVICE AND REMOVE 80% TOTAL SUSPENDED SOLIDS PER NUDEP STANDARDS.
 DEVELOPER IS CURRENTLY DISCUSSING AN ALTERNATE PROPOSAL WITH CITY ENGINEERING THAT WOULD SUBSTITUTE A DEVICE CAPABLE OF REMOVING 78% OF TOTAL SUSPENDED SOLIDS USING A DIFFERENT PARTICLE SIZE DISTRIBUTION (OK 110) THAN NUDEP. THIS ITEM WILL BE RESOLVED PRIOR TO THE PLANS BEING SUBMITTED FOR ENGINEERING APPROVAL.

STORMWATER CALCULATIONS FOR WEST DETENTION BASIN:
 PER ORIGINAL DESIGN CALCULATIONS FOR HUNTER'S SQUARE (PROVIDED BY THE CITY ENGINEERING DEPT.) THE WEST BASIN TAKES 2.87 ACRES OF RUNOFF AND HAS AN ALLOWABLE 10-YR STORM OUTFLOW/PASS THROUGH RATE OF 5.0 CFS.
 USING AN ESTIMATED TIME OF CONCENTRATION OF 20 MINUTES, AND THE RAINFALL INTENSITY EQUATION USED FOR THE ORIGINAL CALCULATIONS ($I = 175/(T+25)$), AND THE RATIONAL METHOD FLOW FORMULA ($Q = CIA$), THE APPROXIMATE RUNOFF COEFFICIENT FOR THIS TRIBUTARY AREA IS:
 TIME OF CONCENTRATION (AT BASIN) = 20 MINUTES
 RAINFALL INTENSITY, I = 175/(T+25) = 3.89 IN/HR
 PEAK FLOW = 5.0 CFS
 TRIBUTARY AREA = 2.87 ACRES
 ESTIMATED RUNOFF COEFFICIENT 'C' = 0.48
CITY REQUIRED FLOOD CONTROL STORAGE VOLUME:
 2" OF STORMWATER VOLUME ACROSS THE TRIBUTARY AREA
 2.87 ACRES x 2" OVER THE SITE = 19,384 C.F.
CHANNEL PROTECTION RATE CONTROL (CPRC) VOLUME:
 CPRC = 6897 x AREA x C = 6897 x 2.87 x 0.48 = 8,839 C.F.
WEST DETENTION BASIN INFORMATION:
 WEST BASIN OUTLET INVERT = 871.79
 MAXIMUM STORAGE AVAILABLE IN WEST BASIN = 20,481 C.F. (SEE TABLE THIS SHEET)
 CITY FLOOD CONTROL VOLUME STORED AT ELEVATION 875.84
 CPRC VOLUME STORED AT ELEVATION = 874.19
 1/2 CPRC VOLUME (4420 C.F.) STORED AT ELEVATION = 873.23
WEST BASIN OUTLET CONTROL DESIGN:
 CPRC EVENT MUST BE RELEASED OVER 48 HOURS. WHEN CALCULATING THE RESTRICTOR REQUIRED FOR THE AVERAGE RELEASE RATE, HEAD OVER THE RESTRICTOR MUST BE CALCULATED ASSUMING THE BASIN IS STORING HALF OF THE CPRC VOLUME.
 AVERAGE RELEASE RATE = CPRC VOLUME/48 HOURS = 0.0512 CFS
 1/2 CPRC VOLUME (4420 C.F.) STORAGE ELEVATION = 873.23
 OUTLET INVERT = 871.79
 HEAD OVER ORIFICE (873.23 - 871.79) = 1.44'
 USING ORIFICE EQUATION, $Q = 0.62 \times A \times (2 \times g \times h)^{3/2}$
 AND SOLVING FOR 'A', AREA OF ORIFICE = 0.0088 S.F. OR 1.235 SQ.IN.
 ORIFICE SIZE = 1.25" DIA. TO BE PLACED IN STAND PIPE AT 871.79
 FLOOD CONTROL EVENT OUTLET SHALL NOT EXCEED 0.33 CFS PER ACRE
 PEAK ALLOWABLE OUTFLOW = 0.20 x 2.87 = 0.53 CFS
 FLOOD CONTROL STORAGE ELEVATION = 875.84
 MAX. HEAD OVER SECOND ORIFICE (875.84 - 871.79) = 4.05'
 USING ABOVE ORIFICE EQUATION, SOLVING FOR FLOW, Q = 0.086 CFS
 THEREFORE ANOTHER ORIFICE WILL BE REQUIRED AT THE CPRC STORAGE ELEVATION IN ORDER TO ALLOW FOR ADDITIONAL OUTFLOW (0.53 - 0.086) OF 0.444 CFS.
 SECOND ORIFICE INVERT ELEVATION = 874.19
 MAX. HEAD OVER SECOND ORIFICE (875.84 - 874.19) = 1.65'
 PEAK ALLOWABLE FLOW OUT OF SECOND ORIFICE = 0.444 CFS
 USING ORIFICE EQUATION, SOLVING FOR ORIFICE AREA 'A' = 0.0695 S.F. OR 10.0 SQ.IN.
 SECOND ORIFICE WILL BE 3 - 2" HOLES AT 874.19 (9.42 SQ.IN.)



WEST DETENTION BASIN OUTLET CONTROL STRUCTURE DETAIL
 NOT TO SCALE



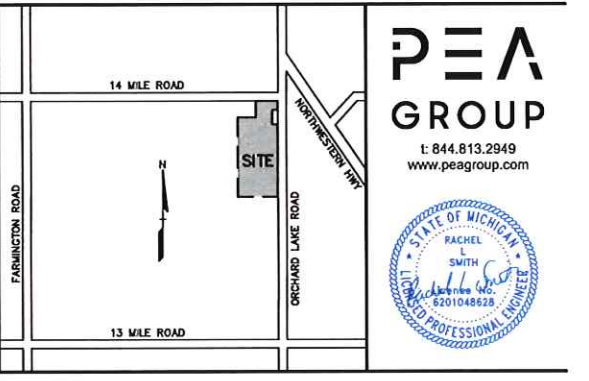
Estimated Volume in South Vault and Pipe System:

Elevation	Vault Volume	Pipe Volume	Total Volume
854.40	0	0	0
854.60	3963		3963
854.80	7927		7927
855.00	11890		11890
855.20	15854		15854
855.40	19817		19817
855.60	23780		23780
855.80	27744		27744
856.00	31707		31707
856.20	35671	615	36286
856.40	39634	1719	41353
856.60	43597	3118	46715
856.80	47561	4737	52298
857.00	51524	6531	58055
857.20	55488	8465	63953
857.40	59451	10513	69964
857.60	63414	12653	76067
857.80	67378	14866	82244
858.00	71341	17133	88474
858.20	75305	19438	94743
858.40	79268	21766	101034
858.60	83231	24101	107332
858.80	87195	26429	113624
859.00	91158	28734	119892
859.20	95122	31002	126124
859.40	99085	33214	132299
859.60	103048	35354	138402
859.80	107012	37402	144414
860.00	110975	39337	150312
860.20	114939	41130	156069
860.40	118902	42750	161652
860.60	122865	44148	167013
860.80	126829	45252	172081
861.00	130792	45867	176559
861.20	134756		180623
861.40	138719		184586
861.60	142682		188549
861.80	146646		192513
862.00	150609		196476
862.20	154573		200440
862.30	156554		202421

Estimated Volume in Regraded West Basin:

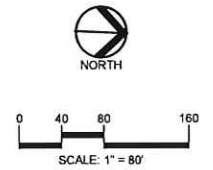
Elevation	Volume	Cumulative Volume
871.79	0	0
872.00	1768	371
873.00	3034	3405
874.00	4337	7742
875.00	5679	13421
876.00	7076	20481

Total Storage Elevation Available at Elevation 876 = 20,481 C.F.
 Total Available Stormwater Storage(Vault, Pipes, West Basin) = 222,902 C.F.



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STATE OF MICHIGAN
 RACHEL L. SMITH
 LICENSE NO. 6201048628
 PROFESSIONAL ENGINEER



811 Know what's below. Call before you dig.

CAUTION!!
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KEY MAP

CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 BOUTHFIELD, MICHIGAN 48178

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS
 1 - SPAPUD RESUBMISSION 12/08/23

ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
OVERALL DRAINAGE MAP

PEA JOB NO. 22-0256
 P.M. RLS
 D.N. KM
 DES. PB
 DRAWING NUMBER

OVERALL PLANT LIST

DECIDUOUS TREE PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
5	ALB	Alegheny Sycamore	<i>Ameiurion laevis</i>	2' Ht.	B&B
13	AB3	Bowhall Maple	<i>Acer rubrum 'Bowhall'</i>	3" Cal.	B&B
14	AR3	Scarlet Sentinel Maple	<i>Acer rubrum 'Scarlet'</i>	3" Cal.	B&B
19	CA3	American Hornbeam	<i>Carpinus caroliniana</i>	3" Cal.	B&B
9	CS3	Frax Fontaine European Hornbeam	<i>Carpinus betulus 'Frax Fontaine'</i>	3" Cal.	B&B
19	GB3	Magyar Ginkgo	<i>Ginkgo biloba 'Magyar'</i>	3" Cal.	B&B
16	GT3	Skyline Honeylocust	<i>Gleditsia triacanthos f. inermis 'Skyline'</i>	3" Cal.	B&B
6	MJ2.5	Marlee Crab	<i>Malus 'Marlee' PP14337 (white, upright, fruitless)</i>	2.5" Cal.	B&B
13	MR2.5	Royal Reddrops Crab	<i>Malus 'Royal Reddrops' PP 14375 (bright pink, upright)</i>	2.5" Cal.	B&B
17	MS2.5	Sugar Tree Crab	<i>Malus 'Sugar Tree'</i>	2.5" Cal.	B&B
4	OG3	Slimy Green Oak	<i>Quercus alba 'SFS-KW10X' P.A.F. (col. fastigiate)</i>	3" Cal.	B&B
5	OS3	Steeple Oak	<i>Quercus alba 'SFS-KW10X' (columnar)</i>	3" Cal.	B&B
4	BR3	Japanese Tree Lilac	<i>Syringa reticulata 'Hory Silk'</i>	3" Cal.	B&B
15	TS3	Boulevard Linden	<i>Tilia americana 'Boulevard'</i>	3" Cal.	B&B
5	ZS3	Green Vase Zelkova	<i>Zelkova serrata 'Green Vase'</i>	3" Cal.	B&B
164		TOTAL DEC.			

EVERGREEN TREE / UPRIGHT EVG. PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
14	AC8	Concolor Fir	<i>Abies concolor</i>	8' Ht.	B&B
15	PG8	White Spruce	<i>Picea glauca</i>	8' Ht.	B&B
12	PO8	Serbian Spruce	<i>Picea omorika</i>	8' Ht.	B&B
26	TN8	Dark Green Arborvitae	<i>Thuja occidentalis 'Nigra'</i>	6' Ht.	B&B
38	TT6	Techy Arborvitae	<i>Thuja occidentalis 'Techy'</i>	6' Ht.	B&B
105		TOTAL EVG.			

SHRUB PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
24	BG30	Green Velvet Boxwood	<i>Buxus x 'Green Velvet'</i>	30" Ht.	Cont.
14	DK30	Kodak Orange Diervilla	<i>Diervilla x 'Kodak Orange'</i>	30" Ht.	Cont.
33	JC30	Sea Green Juniper	<i>Juniperus chinensis 'Sea Green'</i>	30" Spd.	Cont.
16	PO36	Dalio Nandina	<i>Physocarpus opulifolius 'Nandina'</i>	36" Ht.	Cont.
6	SN24	Reon Flash Spirea	<i>Spiraea japonica 'Reon Flash'</i>	24" Ht.	Cont.
22	SB36	Bloomerang Lilac	<i>Syringa x panda 'Bloomerang'</i>	36" Ht.	Cont.
51	TM30	Dense Yew	<i>Taxus media 'Densiformis'</i>	30" Ht.	Cont.
36	VC36	Korean Spirea Viburnum	<i>Viburnum caralis 'Compacta'</i>	36" Ht.	Cont.
15	VB36	Blue Muffin Viburnum	<i>Viburnum dentatum 'Christina'</i>	36" Ht.	Cont.
217		TOTAL SHRUB			

PERENNIAL PLANT LIST:

QTY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
49	AM	Milium Allium	<i>Allium x 'Milium'</i>	1 Gal.	Cont.
49	CA	Feather Reed Grass	<i>Calamagrostis x acutiflora 'Yari Foerster'</i>	1 Gal.	Cont.
		TOTAL PERENNIALS			

LANDSCAPE CALCULATIONS:

PER THE CITY OF FARMINGTON HILLS ZONING ORDINANCE (EXISTING ZONING B-2 COMMUNITY BUSINESS DISTRICT, PROPOSED PUD) FRONT YARD OPEN SPACE PROVIDED 41,398 SF / 100,333 = 41% GREENSPACE PROVIDED APPROX. 17,500 SF (SEE SHT.C-1.0B FOR GREEN AREAS)

PARKING LOT LANDSCAPE:
 REQUIRED: 1 DEC. CANOPY TREE FOR EVERY 2,800 SF OF PAVEMENT. (100 SF MIN. PARKING LOT ISLAND; SEPARATE PARKING LOT FROM PUBLIC THOROUGHFARE W/CONTINUOUS 2' HT. HEDGE/WALL OR BERM ALONG R.O.W.) 285,315 SF PAVT. AREA OF CONSTRUCTION / 2,800 = 102 TREES REQUIRED
 PROVIDED: 102 DECIDUOUS TREES AND CONTINUOUS SHRUB HEDGE BETWEEN PARKING LOT AND R.O.W. AT ORCHARD LAKE ROAD.
 SHRUBS/PERENNIALS WITHIN PARKING LOT

GREENBELT BUFFER:
 REQUIRED: NONE PER CITY ORDINANCE (PROVIDE ENHANCED BUFFER GREENBELT LANDSCAPE)
 PROVIDED: 14 MILE ROAD; EXISTING TREES & SHRUBS (OUTSIDE CONSTRUCTION LIMITS)

ORCHARD LAKE ROAD: 18 EXISTING TREES, 11 DEC. CANOPY TREES, 8 EVG. TREES, 25 ORN. TREES, SHRUB HEDGE AND PERENNIALS TO PROVIDE ENHANCED BUFFER GREENBELT LANDSCAPE

BUFFER/SCREENING BETWEEN USES:
 REQUIRED: PLANTINGS AND 6' HT. WALL OR BERM BETWEEN B-2/B-3 DISTRICT AND RC-2 DISTRICT
 PROVIDED: WEST SIDE OF PROPERTY AND WEST SIDE OF PROPOSED BLDG; EXISTING 7' HT. FENCE/WALL, EXISTING BERM, 20 EXISTING TREES, AND 20 EVG REPLACEMENT TREES PROPOSED IN VOID AREAS, AND 4 PARKING LOT TREES
 AND 15 EVG, 8 DEC, 36 UPRIGHT EVG, 31 DEC SHRUBS AND 33 EVG SHRUBS AND 28 DEC SHRUBS SW SIDE AT LANDBANK PARKING AREA

REPLACEMENT TREES:

REQUIRED: REGULATED TREES: REPLACE 1:1 = 34; 3" DEC. TREES OR 8' HT. EVG
 REQUIRED: LANDMARK TREES: 0; x 0.25 = 0, NONE REQUIRED
 PROVIDED: 34 TREES (14, DEC. TREES AND 20, 8' HT. EVG)
 SEE SHEET T-1.0 FOR TREE CALCULATIONS

288 TOTAL TREES SURVEYED (ON-SITE AND ADJ. TO SITE)

STANDARD TREES	34 (1:1 REPLACEMENT)
LANDMARK TREES	(7 LANDMARK TREES SAVED)
LANDMARK TREES REMOVED	0
LANDMARK TREE DBH REMOVED	0 (25% DBH REPLACEMENT)
REPLACEMENT DBH REQUIRED	0 INCHES

113 TREES GREATER THAN 6 INCHES DBH (ON-SITE)
 97 TREES LESS THAN 6 INCHES DBH (ON-SITE)

KEY:

- = PARKING LOT TREES
 - ⊕ = GREENBELT/ BUFFER LANDSCAPE
 - ⊕ = REPLACEMENT TREES
 - ⊗ = UPRIGHT EVG. BUFFER SCREEN SHRUB
 - ⊗ = DECIDUOUS BUFFER SCREEN SHRUB
 - ▨ = SHRUBS/ PERENNIALS/ ORNAMENTAL GRASS TO BE DETAILED DURING CONSTRUCTION DWGS.
 - ▨ = EVERGREEN SHRUB HEDGE TO BE DETAILED DURING CONSTRUCTION DWGS.
 - ▨ = RAISED PLANTER PERENNIALS/ ANNUALS TO BE DETAILED DURING CONSTRUCTION DWGS.
 - ▨ = IRRIGATED SEED LAWN
 - ⊗ = EXISTING TREE TO REMAIN WITH TREE PROTECTION FENCE
- SEE SHEET L-1.1 TO L-1.6 FOR ENLARGED LANDSCAPE PLANS
 SEE SHEET L-2.0 FOR LANDSCAPE NOTES AND DETAILS
 PROPOSED CANOPY TREES TO BE MIN. 3" CAL. AND 8' HT. EVG
 ALL LAWN AND LANDSCAPE AREAS TO BE IRRIGATED WITH AN UNDERGROUND SPRINKLER SYSTEM
 PLANTINGS TO BE MIN. 4' OFF PROPERTY LINE

14 MILE ROAD

ORCHARD LAKE ROAD

13 MILE ROAD

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 www.peagroup.com

STATE OF MICHIGAN
 JANE L. EVANS
 LANDSCAPE ARCHITECT
 (No. 100,000,000)

NORTH

0 30 60 120
 SCALE: 1" = 60'

811
 Know what's below.
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CAUTION!!
 THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM ALL APPLICABLE AGENCIES AND AUTHORITIES. THE USER SHALL BE RESPONSIBLE FOR VERIFYING THE ACCURACY OF ALL INFORMATION PROVIDED HEREON. THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

ORCHARD LAKE RD.

SITE SHEET KEY MAP

CLIENT
RPT REALTY
 20750 CVMC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SP&PUD RESUBMISSION	12/08/23
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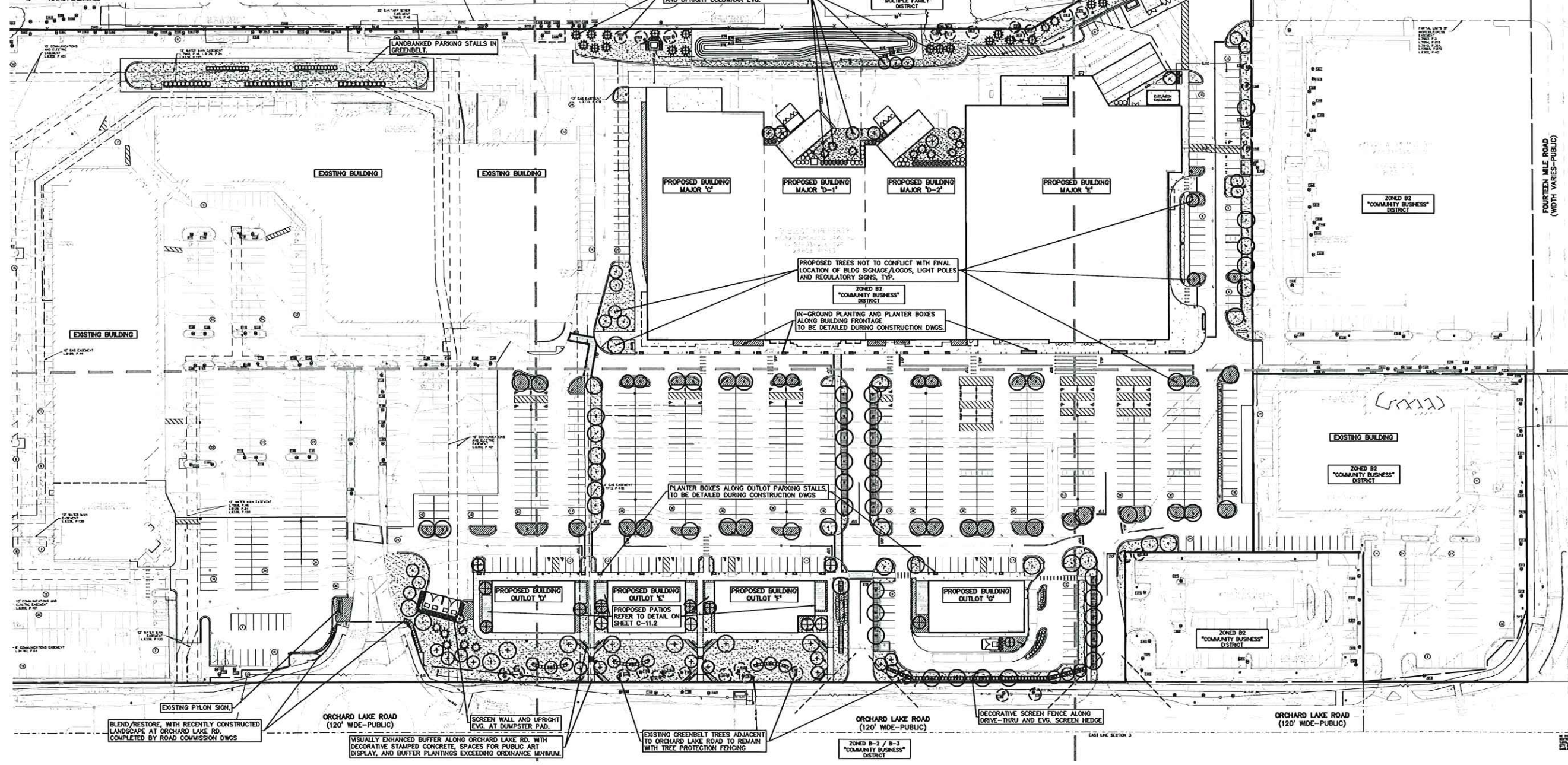
ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
OVERALL LANDSCAPE PLAN

PEA JOB NO. 22-0256

P.M. RLS
 D.N. BGG
 DES. JLE

DRAWING NUMBER
L-1.0



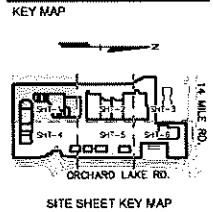
NOT FOR CONSTRUCTION



0 15 30 60
 SCALE: 1" = 30'



CAUTION!
 THE LOCATIONS AND DEPTHS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN ON THIS DRAWING AS OBTAINED FROM THE 811 SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND DEPTHS OF ALL UTILITIES PRIOR TO THE START OF CONSTRUCTION.



CLIENT
RPT REALTY
 20750 CAVO CENTER DRIVE, SUITE #310
 SOUTHFIELD MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS MICHIGAN 48334

REVISIONS

1 - SPAPUD RESUBMISSION	12/08/23
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ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023
 DRAWING TITLE
LANDSCAPE PLAN - SHEET 1

PEA JOB NO. 22-0256
 P.M. RLS
 DN. BGG
 DES. JLE
 DRAWING NUMBER

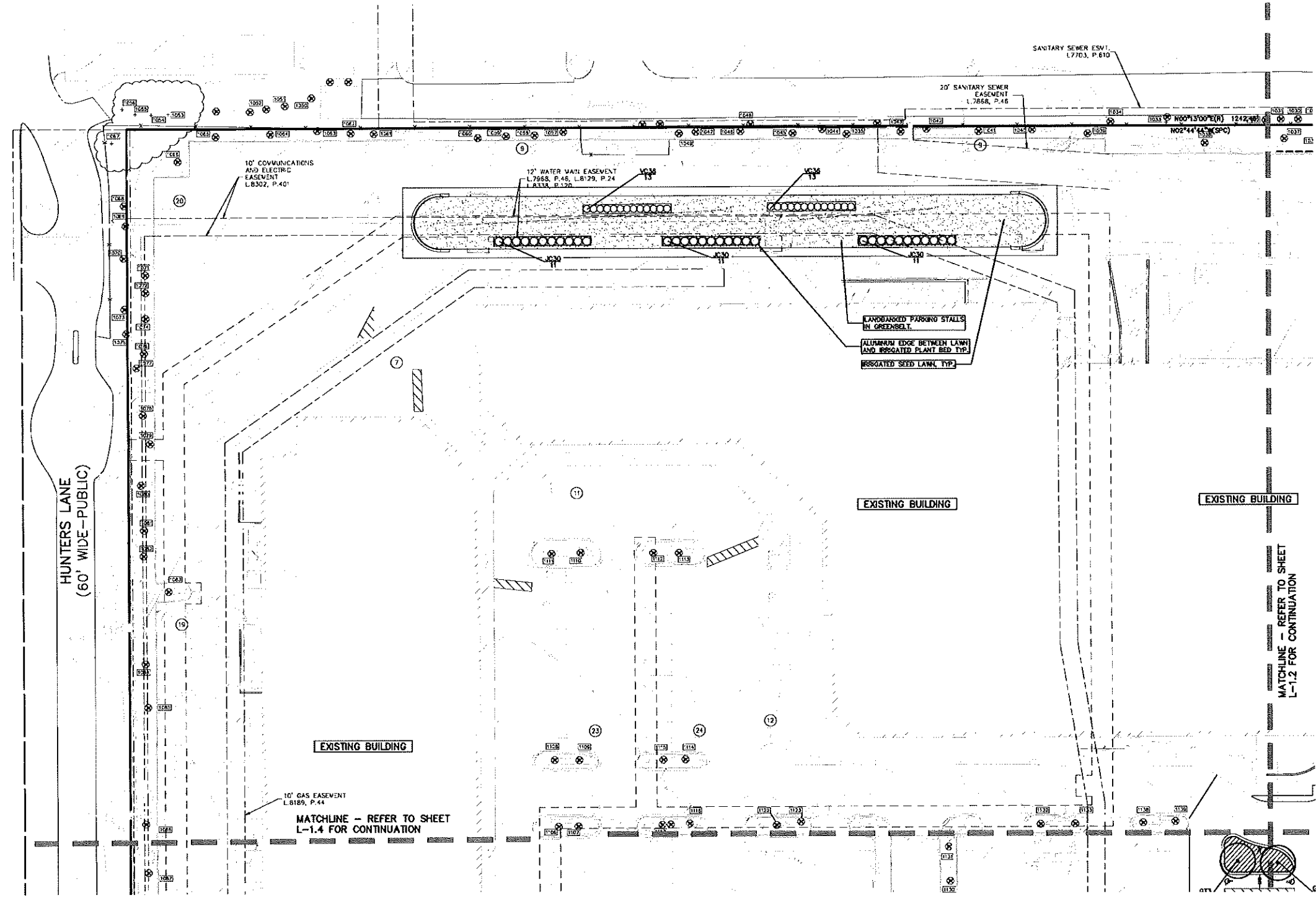
SHEET L-11
 SHRUB PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC.
33	JC30	Sea Green Juniper	<i>Juniperus chinensis 'Sea Green'</i>	30" Sprd	Cont.
26	VC36	Korean Spice Viburnum	<i>Viburnum carlesii 'Compacta'</i>	36" Ht.	Cont.
59	TOTAL SHRUB				

KEY:

- PARKING LOT TREES
- GREENBELT LANDSCAPE
- REPLACEMENT TREES
- UPRIGHT EVG. BUFFER SCREEN SHRUB
- DECOUVOUS BUFFER SCREEN SHRUB
- SHRUBS/ PERENNIALS/ ORNAMENTAL GRASS TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- EVERGREEN SHRUB HEDGE TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- RAISED PLANTER PERENNIALS/ ANNUALS TO BE DETAILED DURING CONSTRUCTION DWGS.
- IRRIGATED SEED LAWN
- EXISTING TREE TO REMAIN WITH TREE PROTECTION FENCE

SEE SHEET L-1.0 FOR OVERALL LANDSCAPE PLANS
 SEE SHEET L-2.0 FOR LANDSCAPE NOTES AND DETAILS
 PROPOSED TREES TO BE MIN. OF 3" CAL. AND 8' HT. EVG
 ALL LAWN AND LANDSCAPE AREAS TO BE IRRIGATED WITH AN UNDERGROUND SPRINKLER SYSTEM
 PLANTINGS TO BE MIN. 4' OFF PROPERTY LINE



NOT FOR CONSTRUCTION **L-1.1**

SHEET L-1.2

DECIDUOUS TREE PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
4	AB3	Bowhall Maple	<i>Acer rubrum 'Bowhall'</i>	3" Cal.	B&B
3	CA3	American Hornbeam	<i>Carpinus caroliniana</i>	3" Cal.	B&B
1	QT3	Skyline Honeylocust	<i>Gleditsia triacanthos f. inermis 'Skycole'</i>	3" Cal.	B&B
5	MS2.5	Sugar Tree Crab	<i>Malus 'Sugar Tyme'</i>	2.5" Cal.	B&B
2	QO3	Skinny Genes Oak	<i>Quercus alba 'JFS-KW2QX' P.A.F. (col./fastigate)</i>	3" Cal.	B&B
15	TOTAL DEC.				

EVERGREEN TREE / UPRIGHT EVG. PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
5	AC8	Concolor Fir	<i>Abies concolor</i>	8' Ht.	B&B
13	PG8	White Spruce	<i>Picea glauca</i>	8' Ht.	B&B
9	PO8	Serbian Spruce	<i>Picea omorika</i>	8' Ht.	B&B
26	TN6	Dark Green Arborvitae	<i>Thuja occidentalis 'Nigra'</i>	6' Ht.	B&B
9	TT6	Techy Arborvitae	<i>Thuja occidentalis 'Techy'</i>	6' Ht.	B&B
62	TOTAL EVG.				

SHRUB PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
16	PO36	Diablo Ninebark	<i>Physocarpus opulifolius 'Monlo'</i>	36" Ht.	Cont.
12	VB36	Blue Muffin Viburnum	<i>Viburnum dentatum 'Christom'</i>	36" Ht.	Cont.
28	TOTAL SHRUB				

KEY:

- = PARKING LOT TREES
- = GREENBELT LANDSCAPE
- = REPLACEMENT TREES
- = UPRIGHT EVG. BUFFER SCREEN SHRUB
- = DECIDUOUS BUFFER SCREEN SHRUB
- = SHRUBS/ PERENNIALS/ ORNAMENTAL GRASS TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- = EVERGREEN SHRUB HEDGE TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- = RAISED PLANTER PERENNIALS/ ANNUALS TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- = IRRIGATED SEED LAWN
- = EXISTING TREE TO REMAIN WITH TREE PROTECTION FENCE

SEE SHEET L-1.0 FOR OVERALL LANDSCAPE PLANS
SEE SHEET L-2.0 FOR LANDSCAPE NOTES AND DETAILS

PROPOSED TREES TO BE MIN. OF 3" CAL. AND 8' HT. EVG.
ALL LAWN AND LANDSCAPE AREAS TO BE IRRIGATED WITH AN UNDERGROUND SPRINKLER SYSTEM
PLANTINGS TO BE MIN. 4' OFF PROPERTY LINE

PEA GROUP
t: 844.813.2949
www.peagroup.com

JANET L. PEA
LANDSCAPE ARCHITECT
NO. 1790
STATE OF MICHIGAN

NORTH

0 15 30 60
SCALE: 1" = 30'

811 *Know what's below. Call before you dig.*

CAUTION!
THE USER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND UTILITIES AS SHOWN ON THIS DRAWING SET ONLY. APPROVED BY STATE OF MICHIGAN REGISTERED AS TO THE QUALITIES OF ACCURACY OF THE INFORMATION PROVIDED. THE USER SHALL BE RESPONSIBLE FOR OBTAINING THE LOCAL UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

CLIENT
RPT REALTY
20750 CVMC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

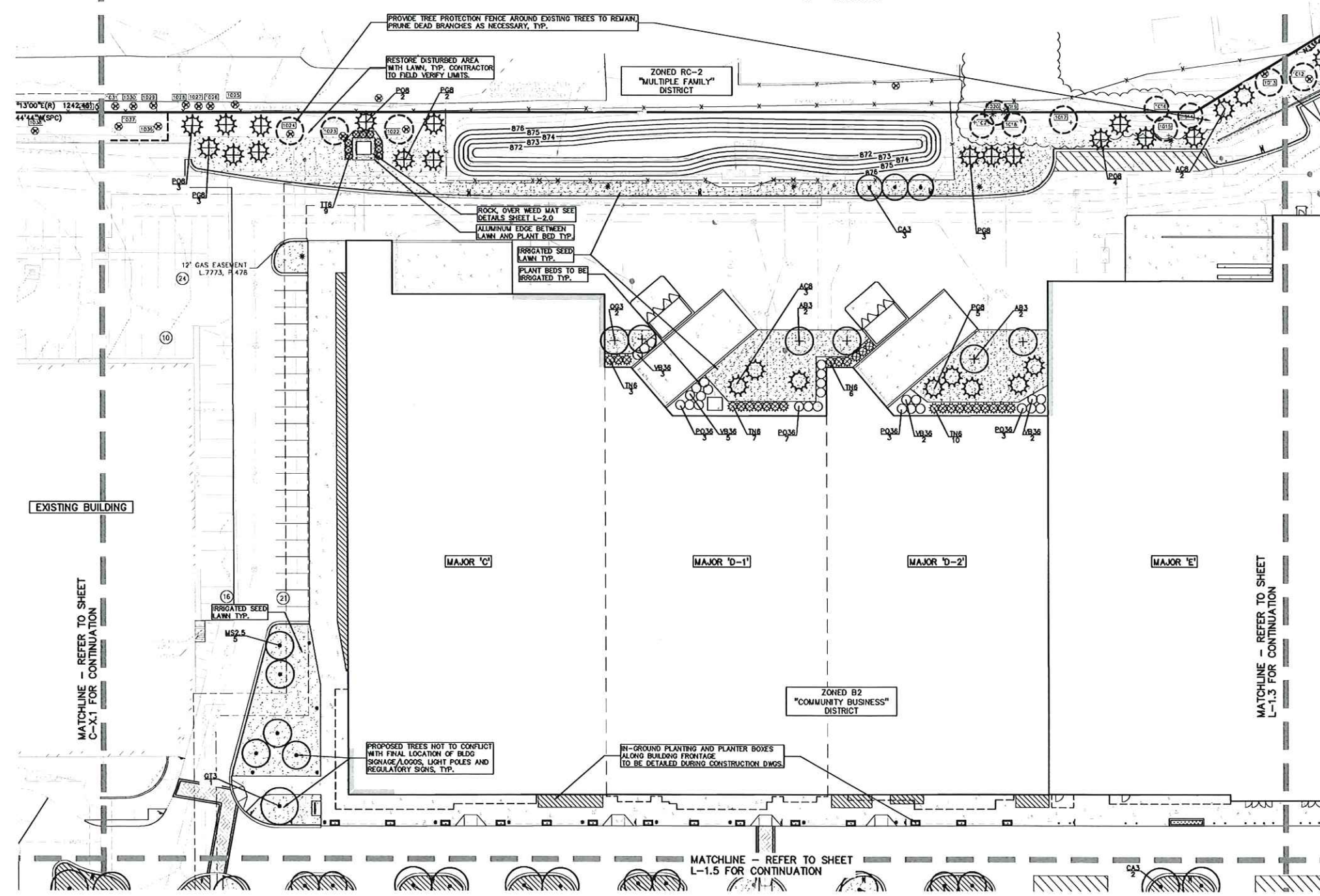
REVISIONS

NO.	DESCRIPTION	DATE
1	SPRINKLER SUBMISSION	12/08/23

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE:
LANDSCAPE PLAN - SHEET 2

PEA JOB NO. 22-0256
P.M. RLS
DN. BGG
DES. JLE
DRAWING NUMBER
L-1.2



NOT FOR CONSTRUCTION **L-1.2**

SHEET L-13

DECIDUOUS TREE PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
1	ALB	Allegheny Serviceberry	<i>Amelanchier laevis</i>	8" HL	B&B
5	AR3	Scarlet Sentinel Maple	<i>Acer rubrum 'Scarlet'</i>	3" Cal	B&B
2	CB3	Frans Fontaine European Hornbeam	<i>Carpinus betulus 'Fran Fontaine'</i>	3" Cal	B&B
1	GT3	Skyline Honeylocust	<i>Gleditsia triacanthos f. Inermis 'Skycole'</i>	3" Cal	B&B
5	QS3	Streetspire Oak	<i>Quercus alba 'JFS-KW1GX' (columnar)</i>	3" Cal	B&B
14		TOTAL DEC.			

EVERGREEN TREE/UPRIGHT EVG. PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
3	AC8	Concolor Fir	<i>Abies concolor</i>	8" HL	B&B
2	PG8	White Spruce	<i>Picea glauca</i>	8" HL	B&B
3	PO8	Serbian Spruce	<i>Picea omorika</i>	8" HL	B&B
4	TT6	Techny Arborvitae	<i>Thuja occidentalis 'Techny'</i>	6"	B&B
12		TOTAL EVG.			

SHRUB PLANT LIST:

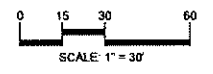
QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
3	VB36	Blue Muffin Viburnum	<i>Viburnum dentatum 'Christon'</i>	36" HL	Concl.
3		TOTAL SHRUB			

KEY:

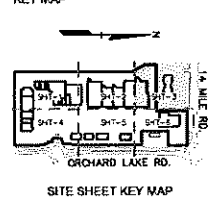
- = PARKING LOT TREES
- = GREENBELT LANDSCAPE
- = REPLACEMENT TREES
- = UPRIGHT EVG. BUFFER SCREEN SHRUB
- = DECIDUOUS BUFFER SCREEN SHRUB
- = SHRUBS/ PERENNIALS/ ORNAMENTAL GRASS (TO BE IRRIGATED)
- = EVERGREEN SHRUB HEDGE (TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED))
- = RAISED PLANTER PERENNIALS/ ANNUALS (TO BE DETAILED DURING CONSTRUCTION DWGS.)
- = IRRIGATED SEED LAWN
- = EXISTING TREE TO REMAIN WITH TREE PROTECTION FENCE

SEE SHEET L-1.0 FOR OVERALL LANDSCAPE PLANS
SEE SHEET L-2.0 FOR LANDSCAPE NOTES AND DETAILS
PROPOSED TREES TO BE MIN. OF 3" CAL. AND 8' HT. EVG
ALL LAWN AND LANDSCAPE AREAS TO BE IRRIGATED WITH AN UNDERGROUND SPRINKLER SYSTEM
PLANTINGS TO BE MIN. 4' OFF PROPERTY LINE

PEA GROUP
 1: 844.813.2949
 www.peagroup.com



CAUTION!
 THE LOCATIONS AND ELEVATIONS OF EXISTING LAWN AND IRRIGATION SYSTEMS ARE SHOWN ON THIS DRAWING ONLY AND ARE NOT TO BE RELIED UPON FOR CONSTRUCTION. THE CONTRACTOR SHALL BE EXCLUSIVELY RESPONSIBLE FOR VERIFYING THE EXISTING UTILITY LOCATIONS AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.



CLIENT
RPT REALTY
 26750 CIVIC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN 48074

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

NO.	DESCRIPTION	DATE
1	SPAPUD RESUBMISSION	12/09/23

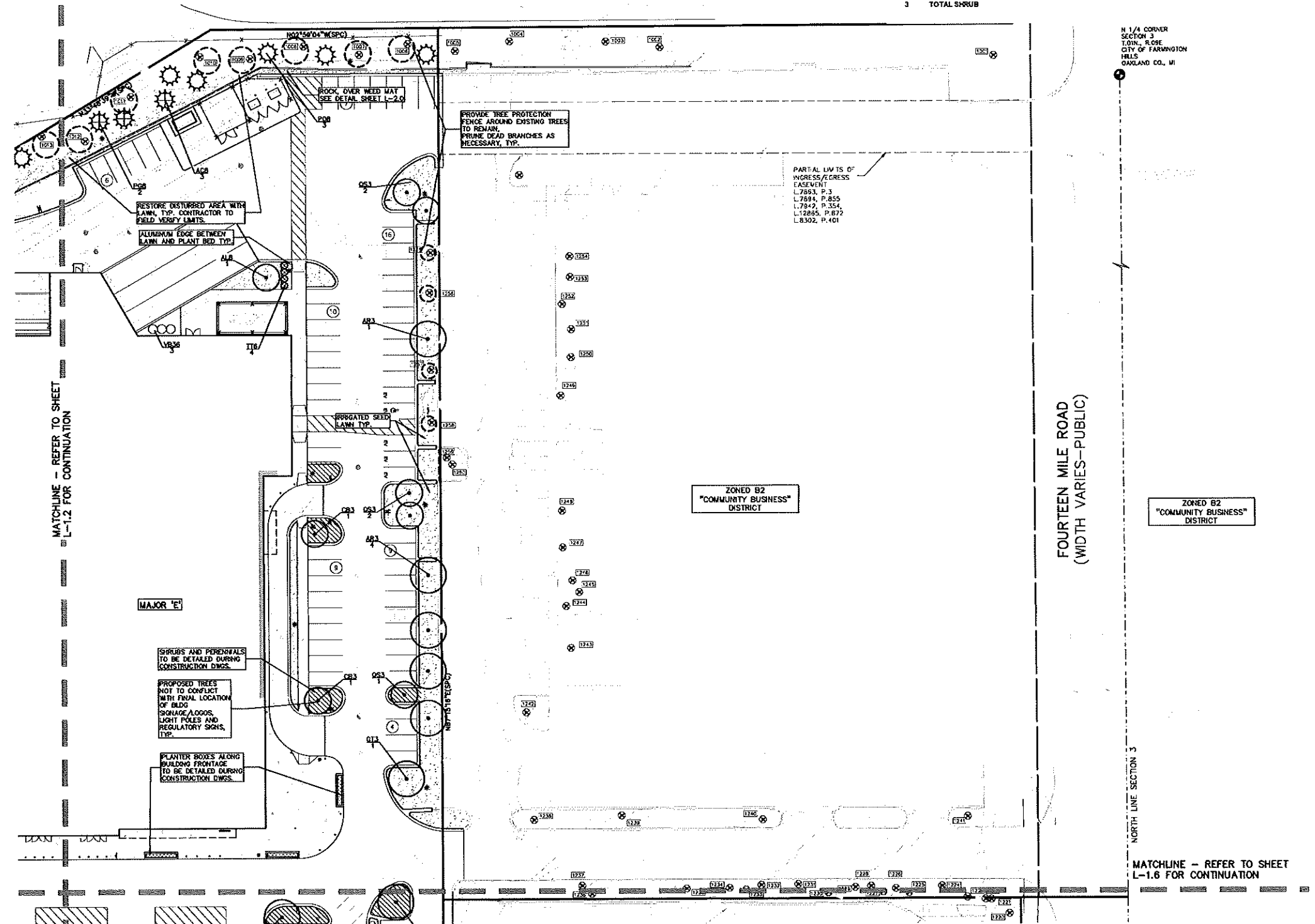
ORIGINAL ISSUE DATE
 SEPTEMBER 15, 2023

DRAWING TITLE

LANDSCAPE PLAN - SHEET 3

PEA JOB NO.	22-0256
P.M.	RLS
CN.	BGG
DES.	JLE
DRAWING NUMBER	

NOT FOR CONSTRUCTION **L-1.3**



MATCHLINE - REFER TO SHEET L-1.2 FOR CONTINUATION

FOURTEEN MILE ROAD (WIDTH VARIES-PUBLIC)

NORTH LINE SECTION 3

MATCHLINE - REFER TO SHEET L-1.6 FOR CONTINUATION

PROJECT NO. 22-0256; DRAWING NO. L-1.3; DATE: 12/09/23; SCALE: 1"=30'; SHEET NO. 3 OF 3

SHEET L-14

DECIDUOUS TREE PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC.
3	AL8	Allegheny Serviceberry	<i>Amelanchier laevis</i>	8" HL	B&B
1	GB3	Magyar Ginkgo	<i>Ginkgo biloba 'Magyar'</i>	3" Cal	B&B
1	GT3	Skyline Honeylocust	<i>Gleditsia tricanthos f. Inermis 'Skycole'</i>	3" Cal	B&B
4	MR2.5	Royal Raindrops Crab	<i>Malus 'JFS-KW5 PP 14375' (bright pink, upright)</i>	2.5" Cal	B&B
1	MS2.5	Sugar Time Crab	<i>Malus 'Sugar Tyme'</i>	2.5" Cal	B&B
1	OQ3	Skinny Genes Oak	<i>Quercus alba 'JFS-KW20X P.A.F. (col./fastigate)</i>	3" Cal	B&B
2	SR3	Japanese Tree Lilac	<i>Syringa reticulata 'Troy Silk'</i>	3" Cal	B&B
2	TB3	Boulevard Linden	<i>Tilia americana 'Boulevard'</i>	3" Cal	B&B
4	ZS3	Green Vase Zelkova	<i>Zelkova serrata 'Green Vase'</i>	3" Cal	B&B
19		TOTAL DEC.			

EVERGREEN TREE / UPRIGHT EVG. PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC.
6	AC8	Concolor Fir	<i>Abies concolor</i>	8" HL	B&B
21	TT6	Techny Arborvitae	<i>Thuja occidentalis 'Techny'</i>	6" HL	B&B
27		TOTAL EVG.			

SHRUB PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC.
14	DK30	Kodak Orange Clerypa	<i>Diervilla x 'Kodak Orange'</i>	30" HL	Cort.
14		TOTAL SHRUB			

PERENNIAL PLANT LIST:

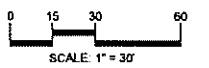
QTY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC.
31	CA	Feather Reed Grass	<i>Calamagrostis x acutiflora 'Karl Foerster'</i>	1 Gal.	Cort.
31		TOTAL PERENNIALS			

KEY:

- = PARKING LOT TREES
- = GREENBELT LANDSCAPE
- = REPLACEMENT TREES
- = UPRIGHT EVG. BUFFER SCREEN SHRUB
- = DECIDUOUS BUFFER SCREEN SHRUB
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- = EVERGREEN SHRUB HEDGE TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- = RAISED PLANTER PERENNIALS / ANNUALS TO BE DETAILED DURING CONSTRUCTION DWGS.
- = IRRIGATED SEED LAWN
- = EXISTING TREE TO REMAIN WITH TREE PROTECTION FENCE

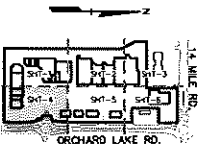
SEE SHEET L-1.0 FOR OVERALL LANDSCAPE PLANS
SEE SHEET L-2.0 FOR LANDSCAPE NOTES AND DETAILS
PROPOSED TREES TO BE MIN. OF 3" CAL. AND 8" HT. EVG.
ALL LAWN AND LANDSCAPE AREAS TO BE IRRIGATED WITH AN UNDERGROUND SPRINKLER SYSTEM
PLANTINGS TO BE MIN. 4' OFF PROPERTY LINE

PEA GROUP
 t: 84.813.2949
 www.peagroup.com



CAUTION!
 THE LOCATION AND DEPTHS OF UTILITIES SHOWN ON THIS PLAN ARE BASED ON THE DATA PROVIDED BY THE CLIENT. PEAGROUP IS NOT RESPONSIBLE FOR THE LOCATION AND DEPTHS OF UTILITIES SHOWN ON THIS PLAN. THE CLIENT SHALL VERIFY THE LOCATION AND DEPTHS OF UTILITIES PRIOR TO THE START OF CONSTRUCTION.

KEY MAP



CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 BOWFIELD, MICHIGAN 49713

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SP&P/UD RESUBMISSION	12/09/23

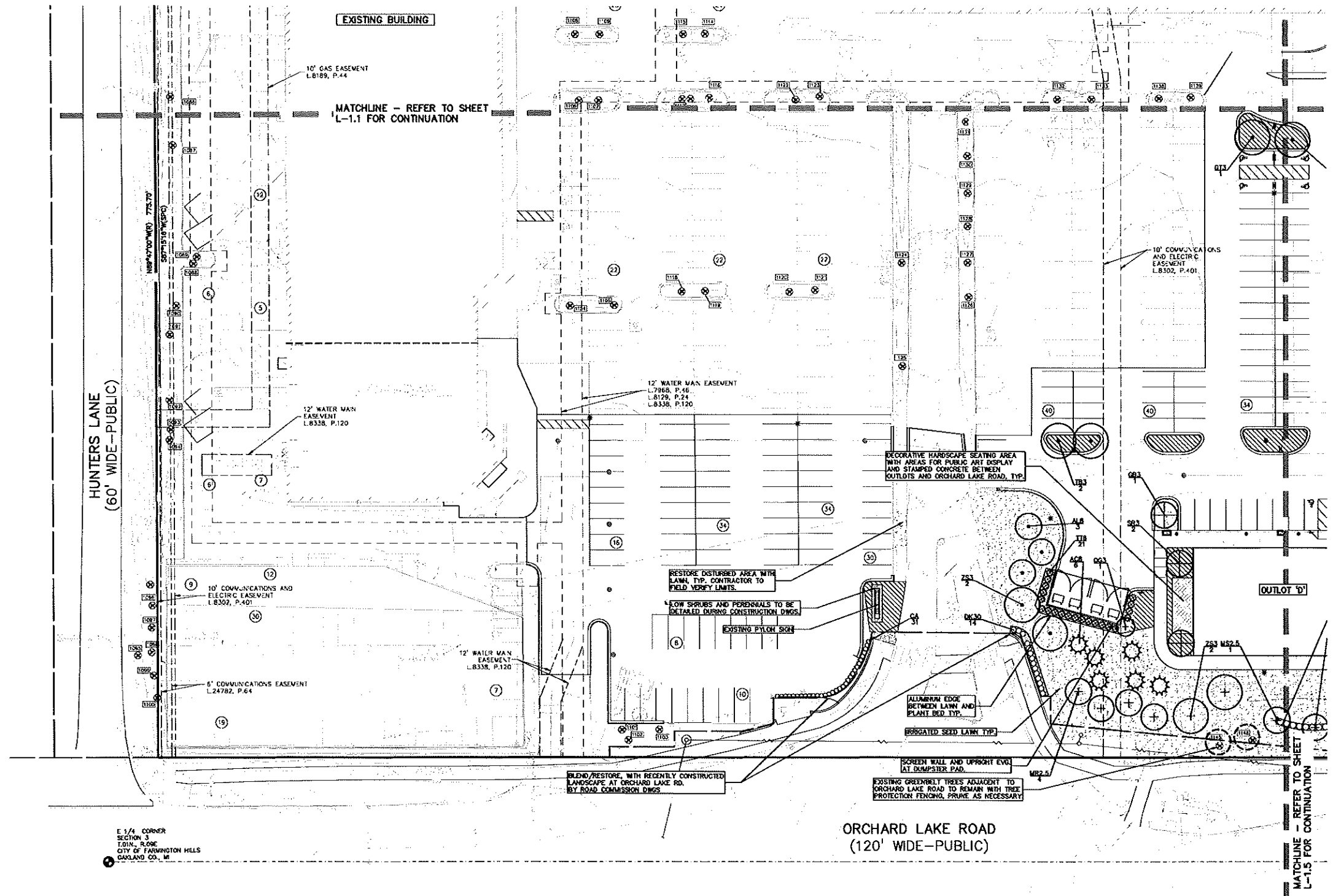
ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
LANDSCAPE PLAN - SHEET 4

PEA JOB NO. 22-0256
P.M. RLS
DN. BGG
DES. J.E.
DRAWING NUMBER

L-1.4

NOT FOR CONSTRUCTION



HUNTERS LANE
(60' WIDE-PUBLIC)

ORCHARD LAKE ROAD
(120' WIDE-PUBLIC)

E 1/4 CORNER
 SECTION 3
 T.01N., R.09E
 CITY OF FARMINGTON HILLS
 OAKLAND CO., MI

MATCHLINE - REFER TO SHEET L-1.5 FOR CONTINUATION

PEA GROUP, 14000 RIVERCHASE DRIVE, SUITE 100, FARMINGTON HILLS, MI 48334, TEL: 248.813.2949, FAX: 248.813.2950, WWW.PEAGROUP.COM

SHEET L-1.5

DECIDUOUS TREE PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
1	AL8	Allegheny Serviceberry	<i>Amelanchier laevis</i>	8" HL	B&B
4	AB3	Bowhall Maple	<i>Acer rubrum 'Bowhall'</i>	3" Cal	B&B
4	AR3	Scarlet Sentinel Maple	<i>Acer rubrum 'Scarson'</i>	3" Cal	B&B
12	CA3	American Hornbeam	<i>Carpinus caroliniana</i>	3" Cal	B&B
7	CB3	Frans Fontaine European Hornbeam	<i>Carpinus betulus 'Fran Fontaine'</i>	3" Cal	B&B
14	GB3	Magyar Ginkgo	<i>Ginkgo biloba 'Magyar'</i>	3" Cal	B&B
9	GT3	Skyline Honeylocust	<i>Gleditsia triacanthos f. inermis 'Skycole'</i>	3" Cal	B&B
6	MJ2.5	Marilee Crab	<i>Malus 'Jardin' PP14337 (white, upright, fruitless)</i>	2.5" Cal	B&B
9	MR2.5	Royal Raindrops Crab	<i>Malus 'JFS-KWS PP 14375' (bright pink, upright)</i>	2.5" Cal	B&B
10	MS2.5	Sugar Tyme Crab	<i>Malus 'Sugar Tyme'</i>	2.5" Cal	B&B
1	QG3	Skippy Genes Oak	<i>Quercus alba 'JFS-KW2QX' P.A.F. (col./fastigate)</i>	3" Cal	B&B
2	SR3	Japanese Tree Lilac	<i>Syringa reticulata 'Tovy SR'</i>	3" Cal	B&B
13	TB3	Boulevard Linden	<i>Tilia americana 'Boulevard'</i>	3" Cal	B&B
1	ZS3	Green Vase Zelkova	<i>Zelkova serrata 'Green Vase'</i>	3" Cal	B&B
93		TOTAL DEC.			

EVERGREEN TREE/UPRIGHT EVG. PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
4	TT6	Techry Arborvitae	<i>Thuja occidentalis 'Techry'</i>	6" HL	B&B
4		TOTAL EVG.			

SHRUB PLANT LIST:

QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
24	BG30	Green Velvet Boxwood	<i>Buxus x 'Green Velvet'</i>	30" HL	Cont.
6	SN24	Neon Flash Spirea	<i>Spiraea japonica 'Neon Flash'</i>	24" HL	Cont.
22	SB36	Blooming Lilac	<i>Syringa x pendula 'Blooming Lilac'</i>	36" HL	Cont.
51	TM30	Dense Yew	<i>Taxus x media 'Densiformis'</i>	30" HL	Cont.
11	VC36	Korean Spice Viburnum	<i>Viburnum carlesii 'Compecta'</i>	36" HL	Cont.
114		TOTAL SHRUB			

PERENNIAL PLANT LIST:

QTY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC
18	CA	Feather Reed Grass	<i>Calamagrostis x acutiflora 'Karl Foerster'</i>	1 Gal	Cont.
18		TOTAL PERENNIALS			

KEY:

- = PARKING LOT TREES
- = GREENBELT LANDSCAPE
- = REPLACEMENT TREES
- = UPRIGHT EVG. BUFFER SCREEN SHRUB
- = EVERGREEN BUFFER SCREEN SHRUB
- = DECIDUOUS BUFFER SCREEN SHRUB
- = SHRUBS/ PERENNIALS/ ORNAMENTAL GRASS TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
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- = RAISED PLANTER PERENNIALS/ ANNUALS TO BE DETAILED DURING CONSTRUCTION DWGS.
- = EXISTING TREE TO REMAIN WITH TREE PROTECTION FENCE

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SEE SHEET L-2.0 FOR LANDSCAPE NOTES AND DETAILS
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PLANTINGS TO BE MIN. 4' OFF PROPERTY LINE

PEA GROUP
t: 844.813.2949
www.peagroup.com

STATE OF MICHIGAN
JANET L. BOWEN
LANDSCAPE ARCHITECT
(NO. 1500)
(NO. 1500)

NORTH

0 15 30 60
SCALE: 1" = 30'

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KEY MAP

ORCHARD LAKE RD.

SITE SHEET KEY MAP

CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
BOWTIE, MICHIGAN 48317

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS

1 - SPADUD RESUBMISSION	12/06/23
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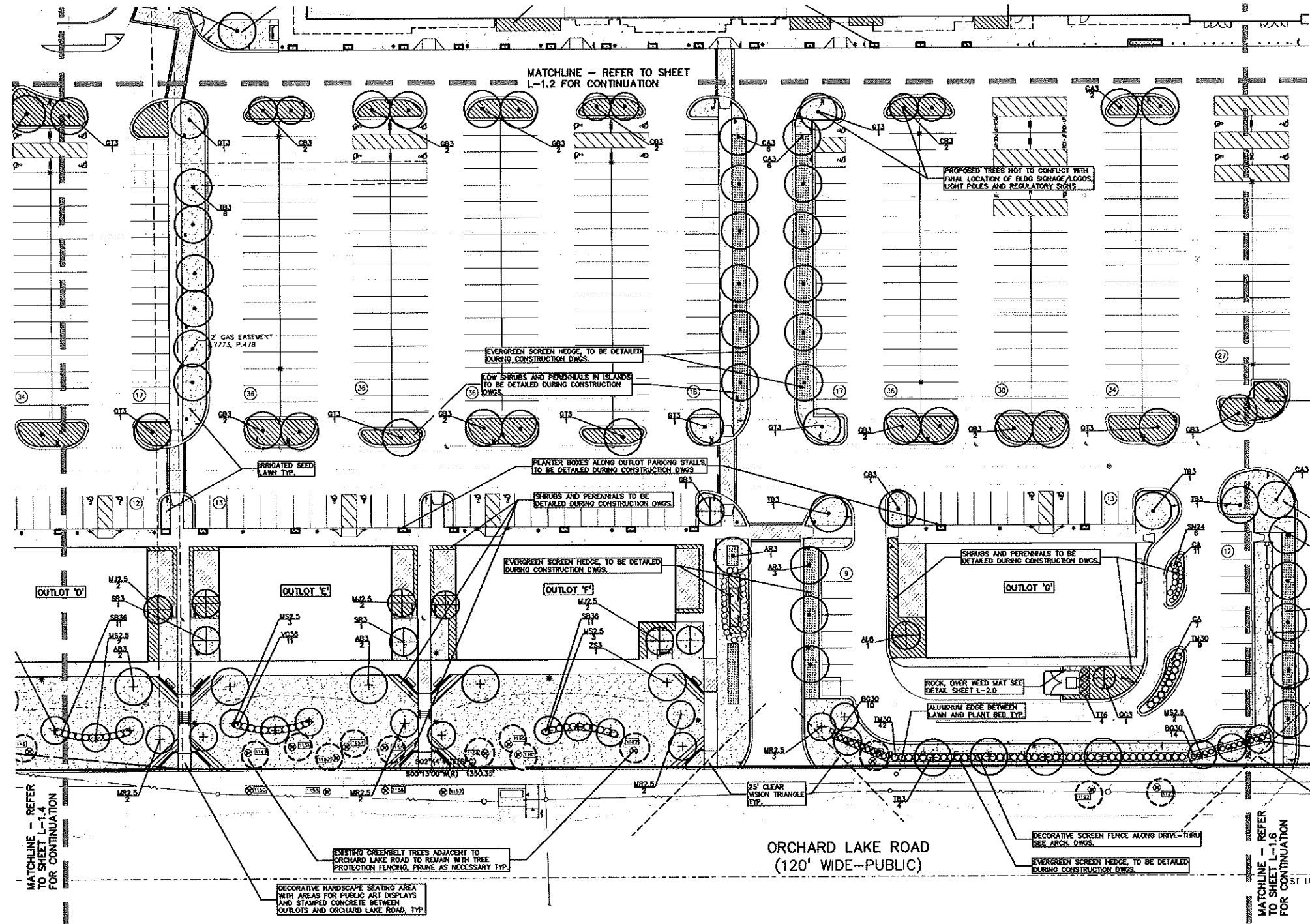
ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
LANDSCAPE PLAN - SHEET 5

PEA JOB NO. 22-0256

P.M. RLS
DN. BGG
DES. JLE

DRAWING NUMBER



NOT FOR CONSTRUCTION **L-1.5**

SHEET L-14

DECIDUOUS TREE PLANT LIST:

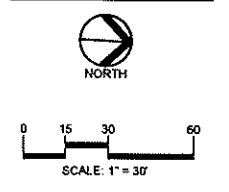
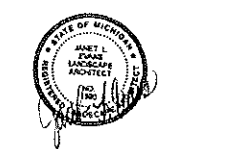
QUANTITY	KEY SYMBOL	COMMON NAME	SCIENTIFIC NAME	SIZE	SPEC.
5	AR3	Scarlet Sentinel Maple	<i>Acer rubrum 'Scarsen'</i>	3" Cal	B&B
4	CA3	American Hornbeam	<i>Carpinus caroliniana</i>	3" Cal	B&B
4	GB3	Mayyar Ginkgo	<i>Ginkgo biloba 'Mayyar'</i>	3" Cal	B&B
4	GT3	Skyline Honeylocust	<i>Gleditsia triacanthos f. Inermis 'Skycole'</i>	3" Cal	B&B
1	MS2.5	Sugar Tine Crab	<i>Malus 'Sugar Tyme'</i>	2.5" Cal	B&B
23		TOTAL DEC.			

KEY:

- (Symbol: Circle with dot) = PARKING LOT TREES
- (Symbol: Circle with cross) = GREENBELT LANDSCAPE
- (Symbol: Circle with plus) = REPLACEMENT TREES
- (Symbol: Circle with vertical lines) = UPRIGHT EVO. BUFFER SCREEN SHRUB
- (Symbol: Circle with horizontal lines) = EVERGREEN BUFFER SCREEN SHRUB
- (Symbol: Circle with diagonal lines) = DECIDUOUS BUFFER SCREEN SHRUB
- (Symbol: Circle with wavy lines) = SHRUBS/ PERENNIALS/ ORNAMENTAL GRASS TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- (Symbol: Circle with grid) = EVERGREEN SHRUB HEDGE TO BE DETAILED DURING CONSTRUCTION DWGS. (TO BE IRRIGATED)
- (Symbol: Circle with stippled) = RAISED PLANTER PERENNIALS/ ANNUALS TO BE DETAILED DURING CONSTRUCTION DWGS.
- (Symbol: Circle with X) = EXISTING TREE TO REMAIN WITH TREE PROTECTION FENCE

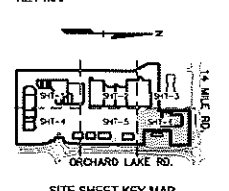
SEE SHEET L-1.0 FOR OVERALL LANDSCAPE PLANS
SEE SHEET L-2.0 FOR LANDSCAPE NOTES AND DETAILS
PROPOSED TREES TO BE MIN. OF 3" CAL. AND 8' HT. EVO
ALL LAWN AND LANDSCAPE AREAS TO BE IRRIGATED WITH AN UNDERGROUND SPRINKLER SYSTEM
PLANTINGS TO BE MIN. 4' OFF PROPERTY LINE

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811 *Know what's below. Call before you dig.*

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CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
BRIGHTFIELD, MICHIGAN 48321

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

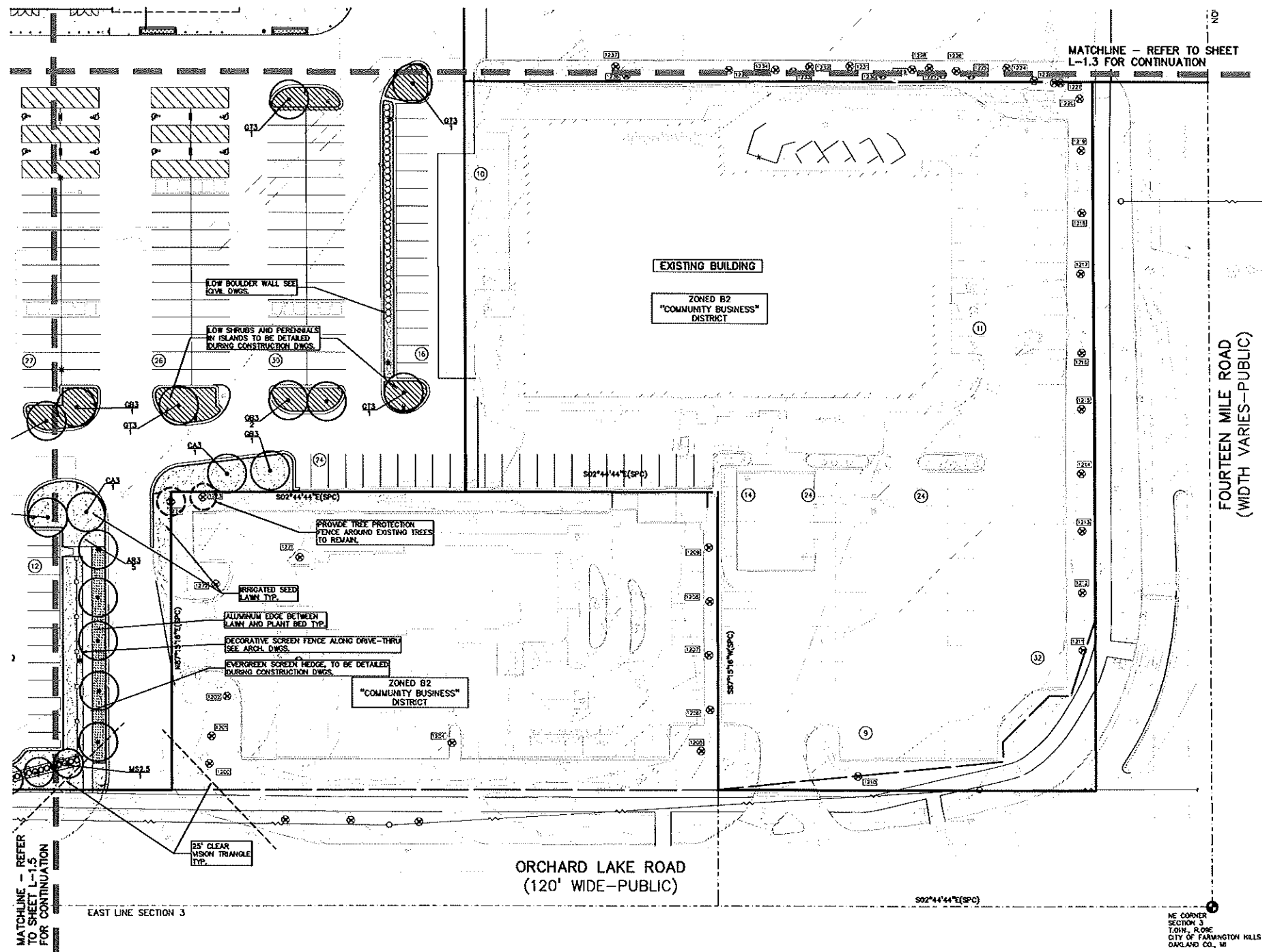
REVISIONS	DATE
1 - SPAPUD RE SUBMISSION	12/09/23

ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
LANDSCAPE PLAN - SHEET 6

PEA JOB NO.	22-0256
P.M.	RLS
D.N.	BGG
DES.	JLE
DRAWING NUMBER	L-1.6

NOT FOR CONSTRUCTION **L-1.6**





CAUTION!
 THE LOCATION AND DEPTHS OF UTILITIES ARE NOT GUARANTEED BY ANY AGENCY OR INDIVIDUAL. NO WARRANTY IS GIVEN EXPRESSED OR IMPLIED AS TO THE EXACT LOCATION OR DEPTHS OF UTILITIES. THE EXACT LOCATION AND DEPTHS OF UTILITIES SHALL BE DETERMINED BY THE USER OF THIS INFORMATION.

KEY MAP

CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 SOUTHFIELD MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS MICHIGAN 48334

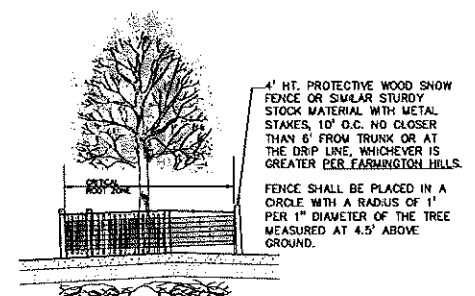
REVISIONS	
1 - SPAPUD RESUBMISSION	12/08/23

ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
LANDSCAPE DETAILS

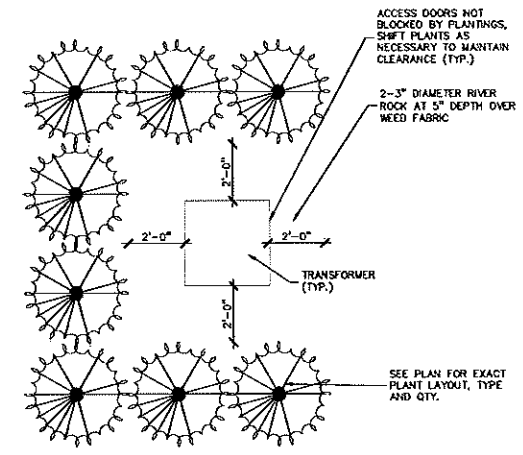
PEA JOB NO.	22-0256
P.M.	RLS
D.W.	BZ
DES.	J.E.
DRAWING NUMBER	

L-2.0



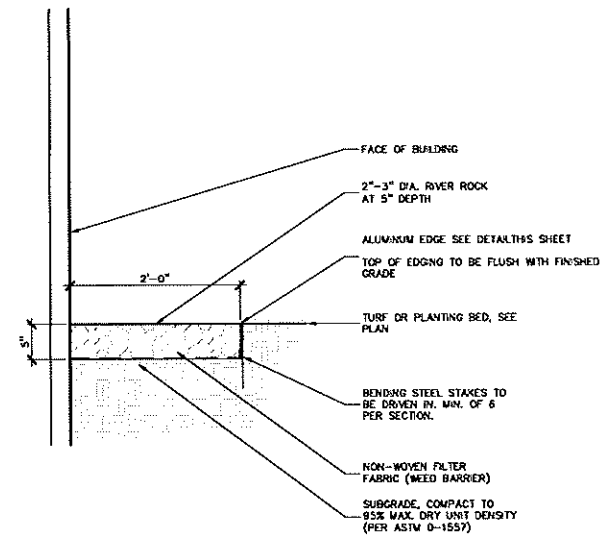
- TREE PROTECTION NOTES PER FARMINGTON HILLS:**
- TREE PROTECTION WILL BE ERRECTED PRIOR TO START OF CONSTRUCTION ACTIVITIES AND SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
 - NO PERSON MAY CONDUCT ANY ACTIVITY WITHIN THE DROP LINE OF ANY TREE DESIGNATED TO REMAIN, INCLUDING, BUT NOT LIMITED TO PLACING SOLVENTS, BUILDING MATERIAL, CONSTRUCTION EQUIPMENT, OR SOIL DEPOSITS WITHIN DROP LINES.
 - GRADE CHANGES MAY NOT OCCUR WITHIN THE DRP LINE OF PROTECTED TREES.
 - DURING CONSTRUCTION, NO PERSON SHALL ATTACH ANY DEVICE OR WIRE TO ANY REMAINING TREE.
 - ALL UTILITY SERVICE REQUESTS MUST INCLUDE NOTIFICATION TO THE INSTALLER THAT PROTECTED TREES MUST BE AVOIDED. ALL TRENCHING SHALL OCCUR OUTSIDE OF THE PROTECTIVE FENCING.
 - TREES LOCATED ON ADJACENT PROPERTY THAT MAY BE AFFECTED BY CONSTRUCTION ACTIVITIES MUST BE PROTECTED.
 - TREES TO BE REMOVED SHALL BE MARKED IN THE FIELD WITH RED PAINT OR FLAGS AND INSPECTED BY FARMINGTON HILLS PLANNING DEPT PRIOR TO ANY TREES BEING REMOVED. A TREE POINT MUST BE DELAYED FROM THE PLANNING DEPT PRIOR TO ANY TREES BEING REMOVED.

3 TREE PROTECTION DETAIL, PER CITY
 NOT TO SCALE

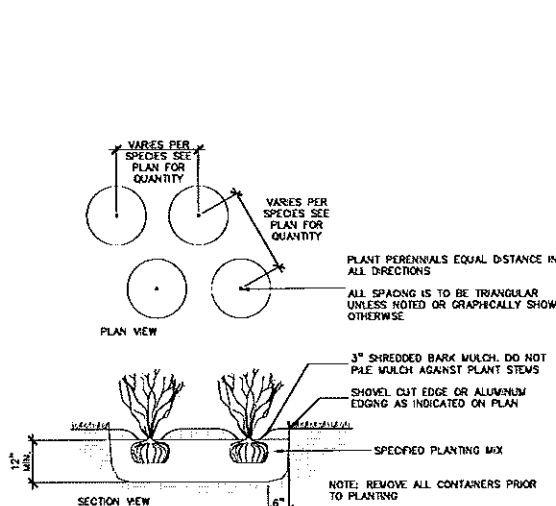


6 TRANSFORMER SCREENING DETAIL
 SCALE: 1" = 3'-0"

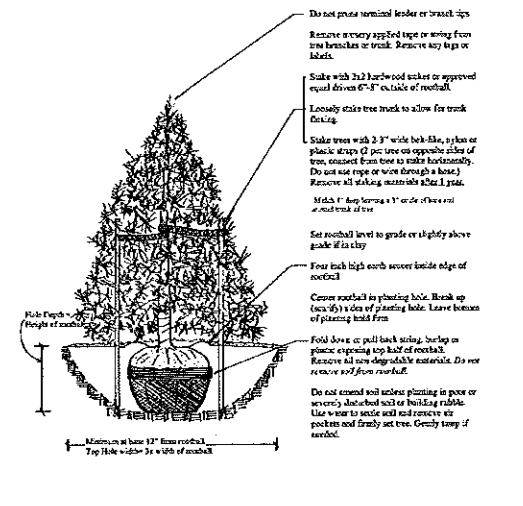
- GENERAL PLANTING NOTES:**
- LANDSCAPE CONTRACTOR SHALL VISIT SITE, INSPECT EXISTING SITE CONDITIONS AND REVIEW PROPOSED PLANTING AND RELATED WORK. IN CASE OF DISCREPANCY BETWEEN PLAN AND PLANT LIST, PLAN SHALL GOVERN QUANTITIES. CONTACT LANDSCAPE ARCHITECT WITH ANY CONCERNS.
 - CONTRACTOR SHALL VERIFY LOCATIONS OF ALL ON SITE UTILITIES PRIOR TO BEGINNING CONSTRUCTION ON HIS/HER PHASE OF WORK. ELECTRIC, GAS, TELEPHONE, CABLE TELEVISION MAY BE LOCATED BY CALLING MISS DIG 1-800-482-7171. ANY DAMAGE OR INTERRUPTION OF SERVICES SHALL BE THE RESPONSIBILITY OF CONTRACTOR. CONTRACTOR SHALL COORDINATE ALL RELATED ACTIVITIES WITH OTHER TRADES ON THE JOB AND SHALL REPORT ANY UNACCEPTABLE JOB CONDITIONS TO OWNER'S REPRESENTATIVE PRIOR TO COMMENCING.
 - ALL PLANT MATERIAL TO BE PREMIUM GRADE NURSERY STOCK AND SHALL SATISFY AMERICAN ASSOCIATION OF NURSERYMEN STANDARD FOR NURSERY STOCK. ALL LANDSCAPE MATERIAL SHALL BE NORTHERN GROWN, NO. 1, GRADE.
 - CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL QUANTITIES SHOWN ON LANDSCAPE PLAN PRIOR TO PROING THE WORK.
 - THE OWNER'S REPRESENTATIVE RESERVES THE RIGHT TO REJECT ANY PLANT MATERIAL NOT MEETING SPECIFICATIONS.
 - ALL SINGLE STEM SHADE TREES TO HAVE STRAIGHT TRUNKS AND SYMMETRICAL CROWNS.
 - ALL SINGLE TRUNK SHADE TREES TO HAVE A CENTRAL LEADER; TREES WITH FORKED OR IRREGULAR TRUNKS WILL NOT BE ACCEPTED.
 - ALL MULTI STEM TREES SHALL BE HEAVILY BRANCHED AND HAVE SYMMETRICAL CROWNS. ONE SPOED TREES OR THOSE WITH THIN OR OPEN CROWNS SHALL NOT BE ACCEPTED.
 - ALL EVERGREEN TREES SHALL BE HEAVILY BRANCHED AND FULL TO THE GROUND, SYMMETRICAL IN SHAPE AND NOT SHEARED FOR THE LAST FIVE GROWING SEASONS.
 - ALL TREES TO HAVE CLAY OR CLAY LOAM BALLS, TREES WITH SAND BALLS WILL BE REJECTED.
 - NO MACHINERY IS TO BE USED WITHIN THE DROP LINE OF EXISTING TREES; HAND GRADE ALL LAWN AREAS WITHIN THE DROP LINE OF EXISTING TREES.
 - ALL TREE LOCATIONS SHALL BE STAKED BY LANDSCAPE CONTRACTOR AND ARE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF THE PLANT MATERIAL.
 - IT IS MANDATORY THAT POSITIVE DRAINAGE IS PROVIDED AWAY FROM ALL BUILDINGS.
 - ALL PLANTING BEDS SHALL RECEIVE 3" SHREDDED HARDWOOD BARK MULCH WITH PRE EMERGENT, SEE SPECIFICATIONS. SHREDDED PALETTE AND DYED MULCH WILL NOT BE ACCEPTED.
 - ALL LANDSCAPED AREAS SHALL RECEIVE 3" COMPACTED TOPSOIL.
 - SEE SPECIFICATIONS FOR ADDITIONAL COMMENTS, REQUIREMENTS, PLANTING PROCEDURES AND WARRANTY STANDARDS.
 - FOR NON-LAWN SEED MAX AREAS, AS NOTED ON PLAN, BRUSH MOW ONCE SEASONALLY FOR INVASIVE SPECIES CONTROL.
 - CONTRACTOR SHALL NOT INSTALL PLANTS UNDER BUILDING OVERHANG AND SHALL NOTIFY LANDSCAPE ARCHITECT IF DRAWINGS CONFLICT WITH BUILDING OVERHANGS.
 - TREES SHALL NOT CONFLICT/ BLOCK PROPOSED REGULATORY/ DIRECTION SIGNAGE, MONUMENT SIGNS, ADDRESS OR LIGHT POLES. SHIFT TREES AS NECESSARY TYP.



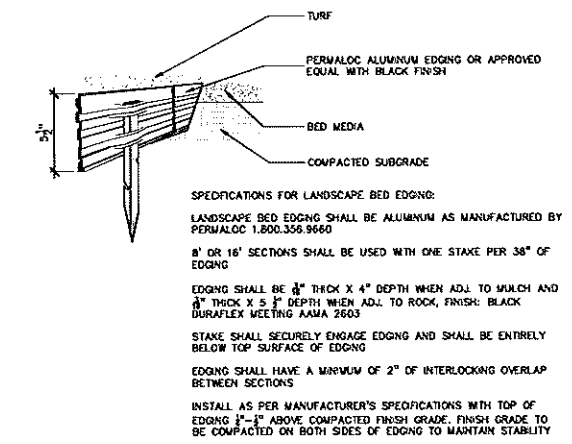
8 STONE MAINTENANCE STRIP
 SCALE: 1" = 1'-0"



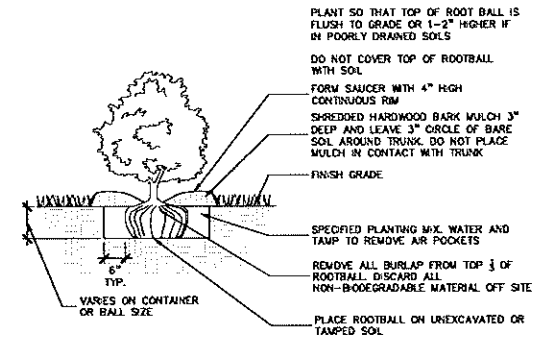
5 PERENNIAL PLANTING DETAIL
 SCALE: 1" = 2'-0"



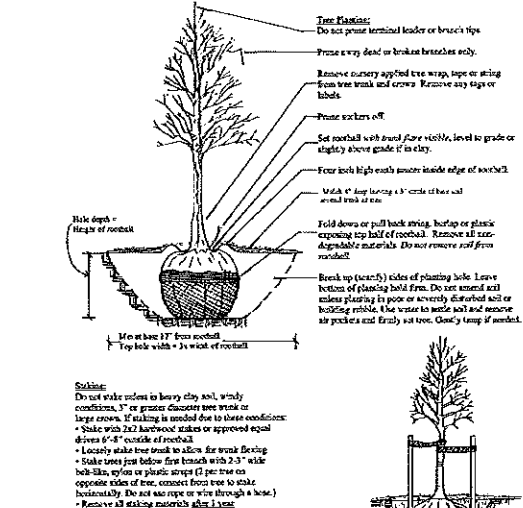
2 EVERGREEN TREE DETAIL, PER CITY
 NOT TO SCALE



7 ALUMINUM EDGE DETAIL
 SCALE: 1/2" = 1'-0"

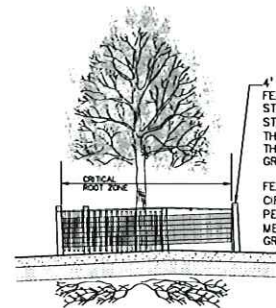


4 SHRUB PLANTING DETAIL
 SCALE: 1" = 2'-0"



1 DECIDUOUS TREE DETAIL, PER CITY
 NOT TO SCALE

NOT FOR CONSTRUCTION



1 TREE PROTECTION DETAIL, PER CITY
NOT TO SCALE

TREE PROTECTION NOTES:

- TREE PROTECTION WILL BE ERRECTED PRIOR TO START OF CONSTRUCTION ACTIVITIES AND SHALL REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE.
- NO PERSON MAY CONDUCT ANY ACTIVITY WITHIN THE DRIP LINE OF ANY TREE DESIGNATED TO REMAIN, INCLUDING, BUT NOT LIMITED TO PLACING SOLVENTS, BUILDING MATERIAL, CONSTRUCTION EQUIPMENT, OR SOIL DEPOSITS WITHIN DRIP LINES.
- GRADE CHANGES MAY NOT OCCUR WITHIN THE DRIP LINE OF PROTECTED TREES.
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- TREES LOCATED ON ADJACENT PROPERTY THAT MAY BE AFFECTED BY CONSTRUCTION ACTIVITIES MUST BE PROTECTED.
- TREES TO BE REMOVED SHALL BE MARKED IN THE FIELD WITH RED PAINT OR FLAGS AND INSPECTED BY FARMINGTON HILLS PLANNING OFFICE PRIOR TO ANY TREES BEING REMOVED. A TREE PERMIT MUST BE OBTAINED FROM THE PLANNING DEPT PRIOR TO ANY TREES BEING REMOVED.

KEY

- (1607) = EXISTING TREE TO REMAIN AND CORRESPONDING TREE TAG
- (172) = EXISTING TREE TO BE REMOVED AND CORRESPONDING TREE TAG
- LM = LANDMARK TREE
- = TREE PROTECTION FENCING

REFER TO SHEET T-1.1 TO T-1.4 FOR ENLARGED TREE PLANS
REFER TO SHEET T-1.4 AND T-1.5 FOR EXISTING TREE LIST
REFER TO SHEET L-1.0 FOR TREE REPLACEMENT INFORMATION

REGULATED TREE = ANY TREE 6" CAL. AND ABOVE IN GOOD-POOR CONDITION
REGULATED TREE; REPLACE AT 1:1
LANDMARK TREE; REPLACE AT 25% OF DBH OF REMOVED TREE

288 TOTAL TREES SURVEYED (ON-SITE AND ADJ. TO SITE)	
STANDARD TREES	
STANDARD TREES REMOVED:	34 (1:1 REPLACEMENT)
LANDMARK TREES	
LANDMARK TREES REMOVED:	0
LANDMARK TREE DBH REMOVED:	0 (25% DBH REPLACEMENT)
REPLACEMENT DBH REQUIRED:	0 INCHES

113 TREES GREATER THAN 6 INCHES DBH (ON-SITE)
97 TREES LESS THAN 6 INCHES DBH (ON-SITE)



NOT TO SCALE



CAUTION!!
THE LOCATION AND PLACEMENT OF EXISTING UNDERGROUND UTILITIES AS SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO GUARANTEE IS GIVEN EXPRESSLY OR IMPLIED AS TO THE COMPLETENESS OR ACCURACY THEREOF. EXISTING UTILITIES SHALL BE EXCAVATED RESPONSIBLY FOR EXPOSURE TO THE EXISTING UTILITIES AND ELEVATIONS PRIOR TO THE START OF CONSTRUCTION.

KEY MAP

CLIENT
RPT REALTY
20750 CIVIC CENTER DRIVE, SUITE #310
SOUTHFIELD, MICHIGAN, 48074

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
ORCHARD LAKE ROAD AT 14 MILE ROAD
FARMINGTON HILLS, MICHIGAN 48334

REVISIONS	
1 - SP4PUD RESUBMISSION	12/08/23

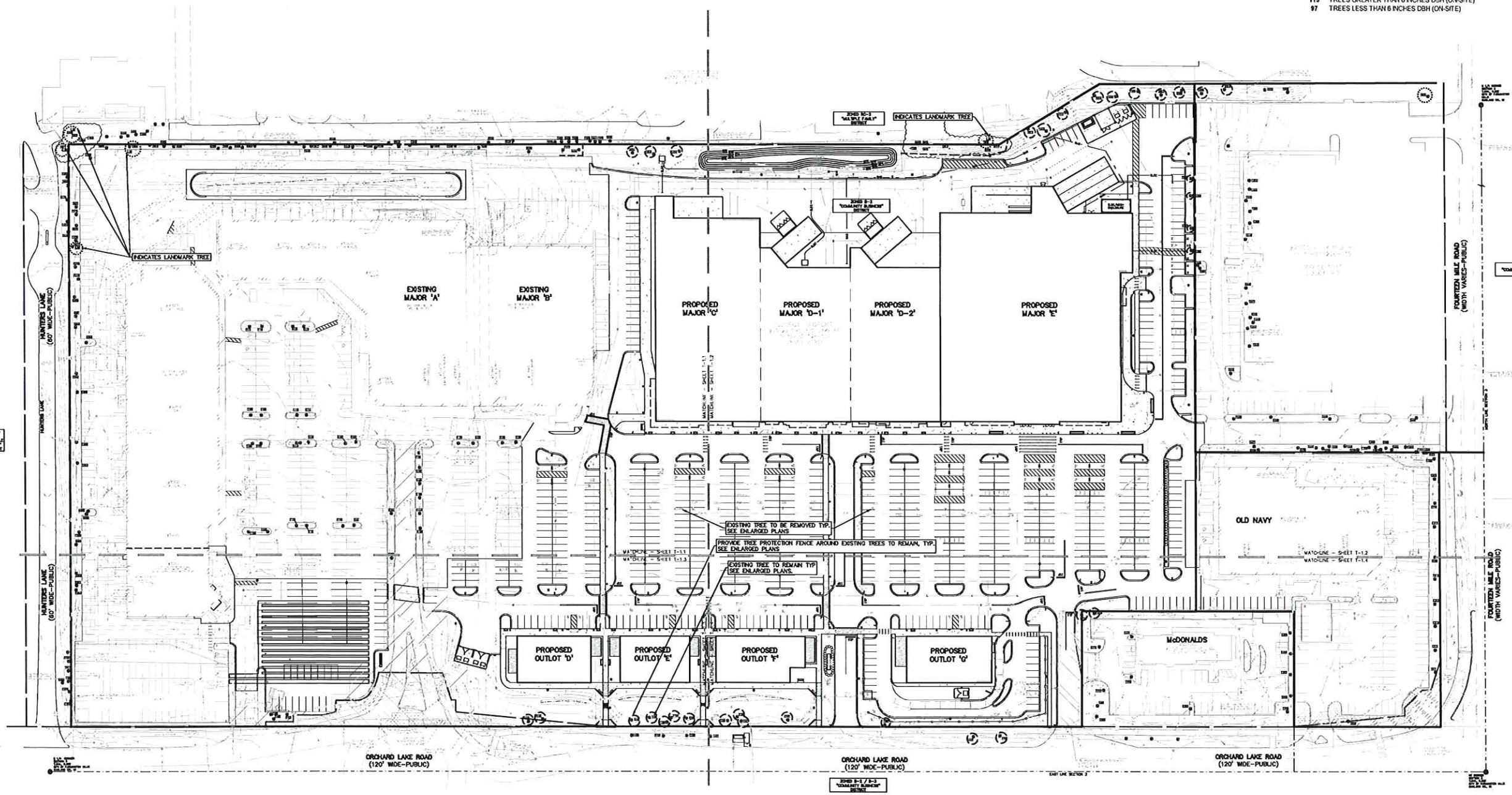
ORIGINAL ISSUE DATE:
SEPTEMBER 15, 2023

DRAWING TITLE
TREE PRESERVATION PLAN

PEA JOB NO.	22-0256
P.M.	RLS
DN.	BGG
DES.	JLE
DRAWING NUMBER	

T-1.0

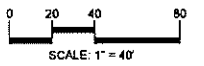
NOT FOR CONSTRUCTION



1:\PROJECTS\2022\2022-0256 - HUNTERS SQUARE REDEVELOPMENT\DWG\PEA\T-1.0-REDEVELOPMENT_PLAN\T-1.0-TREE PRESERVATION PLAN.rvt DATE: 11/27/2023 BY: JLE (JLE) DATE: 11/27/2023

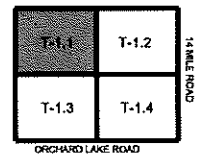
KEY	
	EXISTING TREE TO REMAIN AND CORRESPONDING TREE TAG
	EXISTING TREE TO BE REMOVED AND CORRESPONDING TREE TAG
	LANDMARK TREE
	TREE PROTECTION FENCE

PEA GROUP
 t: 844.813.2949
 www.peagroup.com



CAUTION!
 THE LOCATIONS AND PLACEMENTS OF EXISTING UNDERGROUND UTILITIES ARE FROM THE DRAWING RECORD. APPLICANTS ARE ADVISED THAT THE INFORMATION IS NOT GUARANTEED AS TO ACCURACY OR COMPLETENESS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE EXISTENCE AND DEPTH OF ALL UTILITIES PRIOR TO THE START OF CONSTRUCTION.

KEY MAP



CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN 48074

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

REVISIONS	
1 - SPAPUD RESUBMISSION	12/08/23

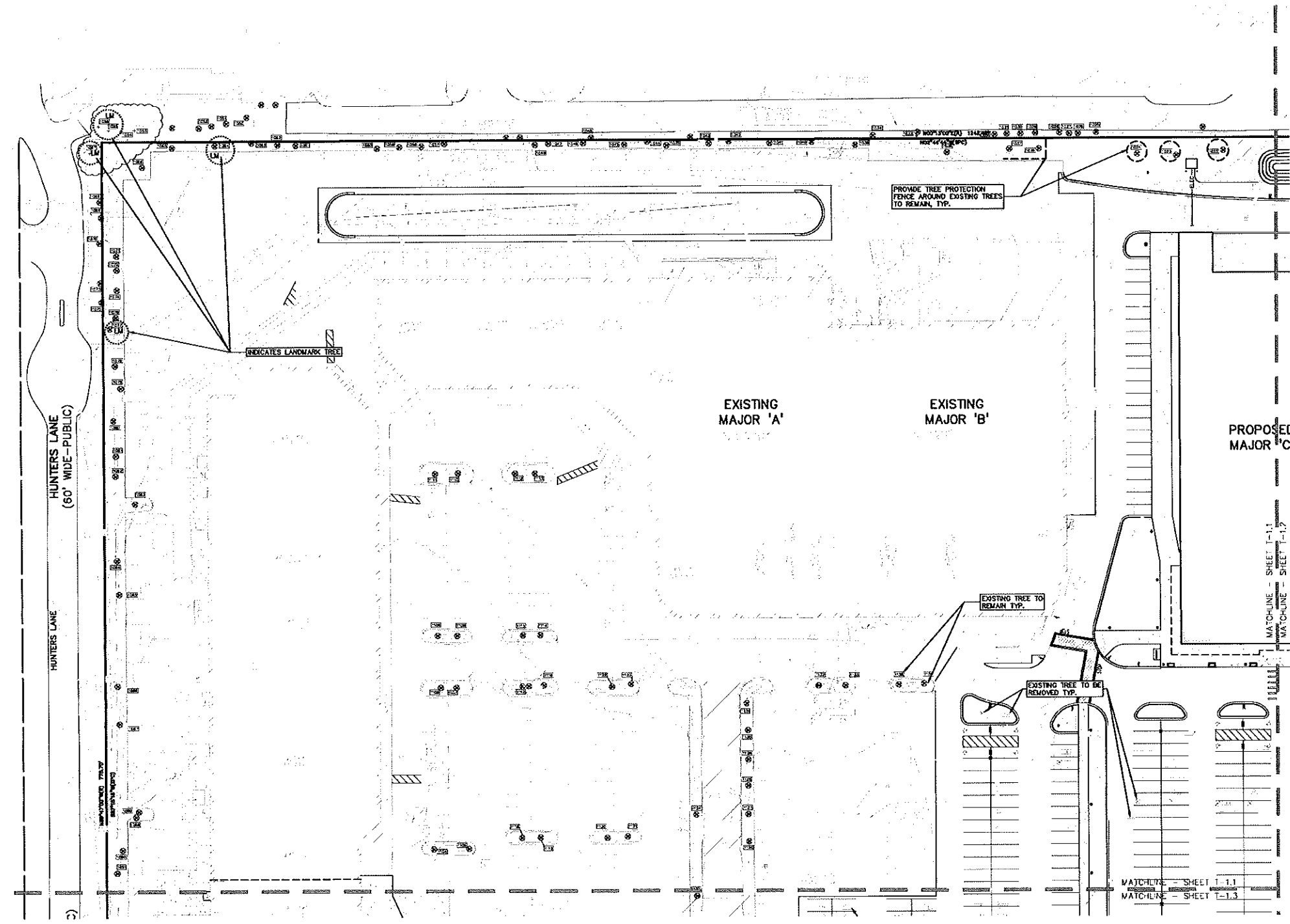
ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
TREE PRESERVATION PLAN - SW

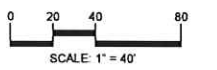
PEA JOB NO.	22-0256
P.M.	RLS
DN.	BGG
DES.	J.E.

DRAWING NUMBER

NOT FOR CONSTRUCTION T-1.1

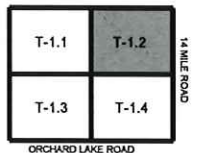


ZONED
 RC-2
 "MULTIPLE FAMILY"
 DISTRICT



CAUTION!!
 THE LOCATION AND DEPTH OF UTILITIES SHOWN ON THIS DRAWING ARE ONLY APPROXIMATE. NO WARRANTY IS GIVEN FOR THE ACCURACY OF THE INFORMATION PROVIDED. THE USER SHALL BE RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND ELEVATION OF UTILITIES PRIOR TO THE START OF CONSTRUCTION.

KEY MAP



CLIENT
RPT REALTY
 20750 CYMC CENTER DRIVE, SUITE #310
 SOUTHFIELD, MICHIGAN 48076

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

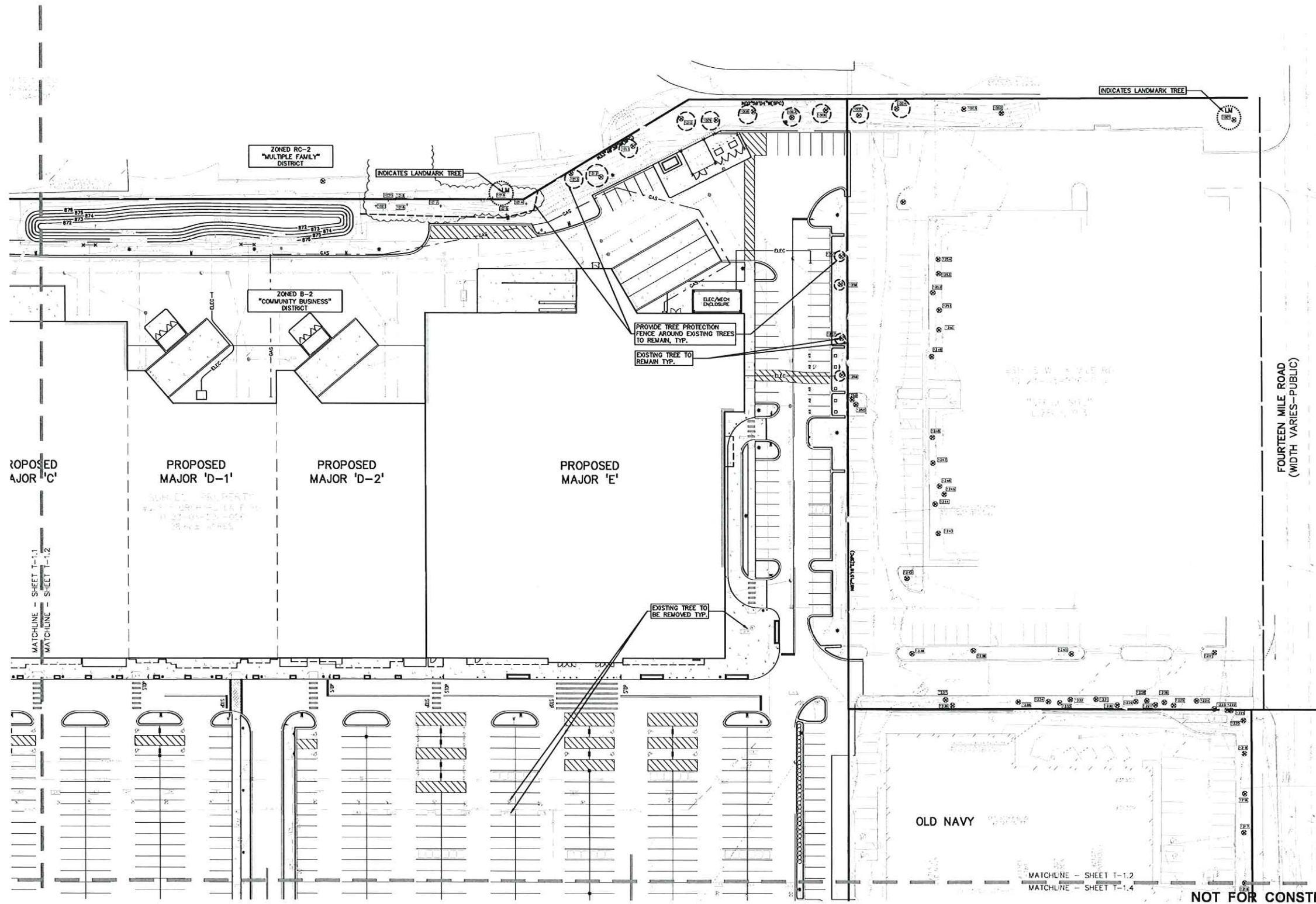
REVISIONS
 1 - SPATIAL RESUBMISSION 12/06/23

ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

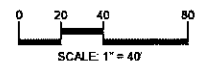
DRAWING TITLE
TREE PRESERVATION PLAN - NW

PEA JOB NO. 22-0256
 P.M. RLS
 DN. BGG
 DES. JLE

DRAWING NUMBER
T-1.2

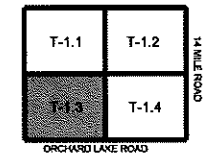


NOT FOR CONSTRUCTION



CAUTION!!
 THE LOCATION AND DEPTH OF UTILITIES ARE NOT GUARANTEED BY ANY AGENCY OR AUTHORITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF UTILITIES PRIOR TO THE START OF CONSTRUCTION.

KEY MAP



CLIENT
RPT REALTY
 20750 CIVIC CENTER DRIVE, SUITE #310
 BOSTON, MASSACHUSETTS 02116

PROJECT TITLE
HUNTER'S SQUARE REDEVELOPMENT
 ORCHARD LAKE ROAD AT 14 MILE ROAD
 FARMINGTON HILLS, MICHIGAN 48334

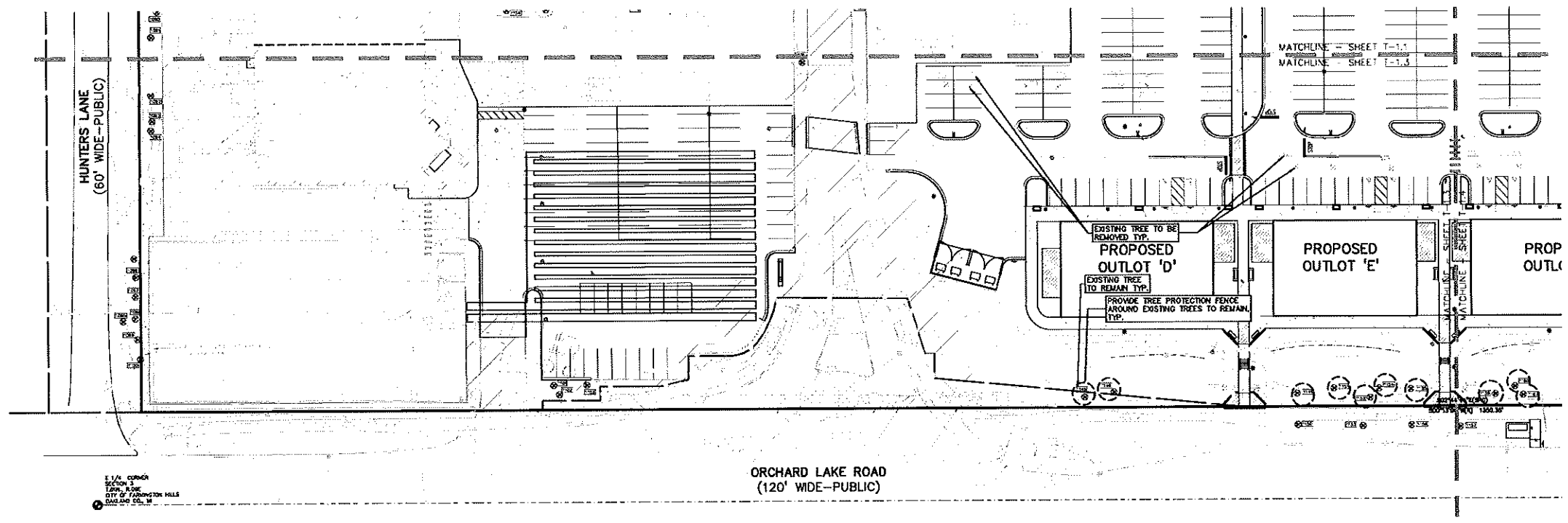
REVISIONS

1 - SPAPUD RESUBMISSION	12/06/23
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ORIGINAL ISSUE DATE:
 SEPTEMBER 15, 2023

DRAWING TITLE
TREE PRESERVATION PLAN - SE

PEA JOB NO.	22-0256
P.M.	RLS
DN.	BGG
DES.	J.L.E.
DRAWING NUMBER	



E.T.A. CONOR
 SECTION 1
 CITY OF FARMINGTON HILLS
 MICHIGAN, CO. 18

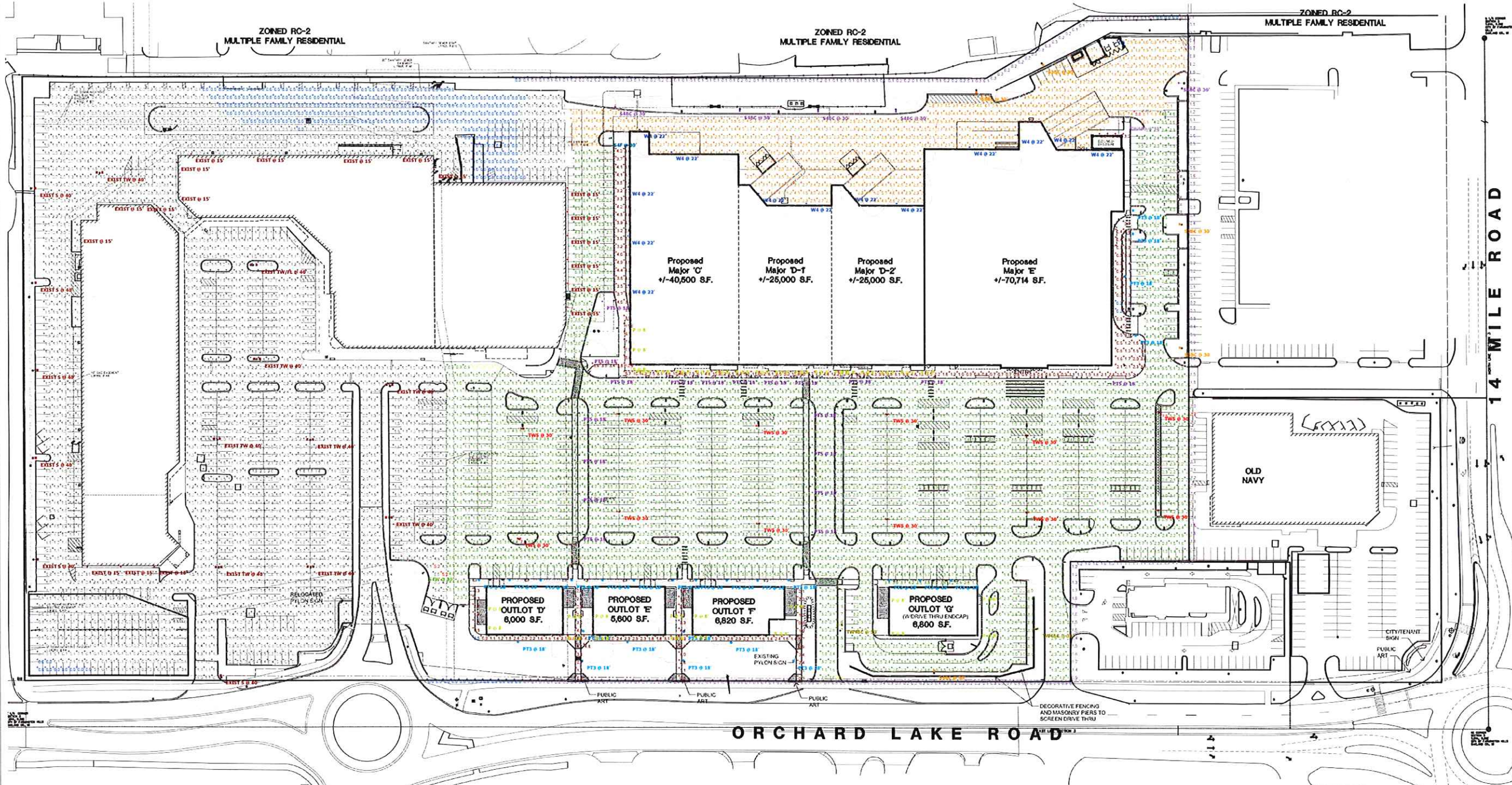
TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	EXTRA TRUNKS	CLASS	CONDITION/FACTOR					OVERALL RANK	SAVE / REMOVE	STATUS	> MN	REPLACE	ONSITE	
							trunk	growth rate	structure	pest/disease	crowd/lev							life exp
1201	NM	18	Norway Maple	Acer platanoides		LANDMARK	5	5	4	5	5	3	28	S	PROT	Y	-	OFFSITE
1202	NM	14	Norway Maple	Acer platanoides		WOODLAND	4	5	4	5	5	4	21	S	PROT	N	-	OFFSITE
1203	NM	15	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	4	21	S	PROT	N	-	OFFSITE
1204	NM	14	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	4	21	S	PROT	N	-	OFFSITE
1205	NM	11	Norway Maple	Acer platanoides		WOODLAND	4	5	5	5	4	3	26	S	PROT	Y	-	OFFSITE
1206	NM	14	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	26	S	PROT	N	-	OFFSITE
1207	NM	11	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1208	NM	13	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1209	NM	14	Norway Maple	Acer platanoides		WOODLAND	5	5	4	5	4	3	26	S	PROT	N	-	ON
1210	NM	15	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1211	NM	15	Norway Maple	Acer platanoides		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1212	NM	15	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1213	NM	15	Norway Maple	Acer platanoides		WOODLAND	4	4	4	4	5	3	24	S	PROT	N	-	ON
1214	NM	13	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1215	NM	13	Norway Maple	Acer platanoides		WOODLAND	4	3	3	3	4	3	22	S	PROT	N	-	ON
1216	NM	13	Norway Maple	Acer platanoides		LANDMARK	5	5	5	5	5	3	28	S	PROT	Y	-	ON
1217	TH	8	ThornappleHemlock	Crataegus spp.		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1218	NM	11	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1219	RP	17	Red Pine	Pinus resinosa		WOODLAND	5	4	5	5	4	1	24	S	PROT	N	-	ON
1220	RP	14	Red Pine	Pinus resinosa		WOODLAND	4	3	3	3	4	3	22	S	PROT	N	-	ON
1221	RM	12	Red Maple	Acer rubrum		WOODLAND	4	5	5	5	5	3	28	S	PROT	N	-	ON
1222	RM	6	Red Maple	Acer rubrum		WOODLAND	2	2	2	2	2	3	11	S	PROT	N	-	ON
1223	RM	12	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1224	RM	12	Red Maple	Acer rubrum		WOODLAND	5	3	3	3	3	3	18	S	PROT	N	-	ON
1225	SU	14	Sugar Maple	Acer saccharum		WOODLAND	5	5	4	5	5	3	27	S	PROT	N	-	OFFSITE
1226	SU	5	Sugar Maple	Acer saccharum	x1.4	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1227	SU	13	Sugar Maple	Acer saccharum		WOODLAND	3	3	4	2	3	18	5	PROT	N	-	OFFSITE	
1228	SU	13	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1229	SU	8	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	OFFSITE
1230	SU	10	Sugar Maple	Acer saccharum		WOODLAND	5	4	5	5	5	3	26	S	PROT	N	-	OFFSITE
1231	SU	9	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
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1234	SU	12	Sugar Maple	Acer saccharum		WOODLAND	5	5	4	5	5	3	26	S	PROT	N	-	OFFSITE
1235	SU	12	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1236	SU	6	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1237	SU	5	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1238	SU	4	Sugar Maple	Acer saccharum		WOODLAND	4	5	5	5	5	3	27	S	PROT	N	-	ON
1239	S	10	Sycamore	Platanus occidentalis		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1240	S	13	Sycamore	Platanus occidentalis		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1241	TH	9	ThornappleHemlock	Crataegus spp.		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1242	S	13	Sycamore	Platanus occidentalis		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1243	RC	7	Red Cedar	Juniperus virginiana	x1.5	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1244	EP	3	Bradford Pear	Pyrus californiana	x3.2.2.2	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1245	RC	8	Red Cedar	Juniperus virginiana		WOODLAND	4	5	5	5	5	3	27	S	PROT	N	-	ON
1246	RC	5	Red Cedar	Juniperus virginiana	x2.4.2	WOODLAND	3	3	3	3	3	3	21	S	PROT	N	-	ON
1247	EP	5	Bradford Pear	Pyrus californiana		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	OFFSITE
1248	SU	12	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1249	SU	12	Sugar Maple	Acer saccharum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1250	SM	15	Silver Maple	Acer saccharinum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE

TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	EXTRA TRUNKS	CLASS	CONDITION/FACTOR					OVERALL RANK	SAVE / REMOVE	STATUS	> MN	REPLACE	ONSITE	
							trunk	growth rate	structure	pest/disease	crowd/lev							life exp
1251	SM	15	Silver Maple	Acer saccharinum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1252	SM	14	Silver Maple	Acer saccharinum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1253	SM	17	Silver Maple	Acer saccharinum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1254	SM	17	Silver Maple	Acer saccharinum		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	OFFSITE
1255	SM	20	Silver Maple	Acer saccharinum		LANDMARK	5	5	5	5	5	3	28	S	PROT	Y	-	OFFSITE
1256	SM	25	Silver Maple	Acer saccharinum		LANDMARK	5	5	5	5	5	3	28	S	PROT	Y	-	OFFSITE
1257	JZ	14	Japanese zelkova	Zelkova serrata		WOODLAND	5	5	4	5	5	3	27	S	PROT	N	-	ON
1258	RC	5	Red Cedar	Juniperus virginiana	x2.4.3	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1259	EP	9	Bradford Pear	Pyrus californiana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1260	RC	5	Red Cedar	Juniperus virginiana	x1.4	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1261	RC	6	Red Cedar	Juniperus virginiana	x1.5	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1262	S	5	Sycamore	Platanus occidentalis	x1.3	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1263	RC	3	Red Cedar	Juniperus virginiana	x1.3.2.2.2	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1264	CT	24	Cottonwood	Populus deltoides		LANDMARK	5	5	5	5	4	3	27	S	PROT	Y	-	ON
1265	RC	7	Red Cedar	Juniperus virginiana	x5.3.3.2.2	WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1266	BS	11	Blue Spruce	Picea pungens		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1267	CT	52	Cottonwood	Populus deltoides		LANDMARK	5	5	5	5	5	3	28	S	PROT	Y	-	OFFSITE
1268	BS	11	Blue Spruce	Picea pungens		WOODLAND	5	5	5	4	5	3	27	S	PROT	N	-	OFFSITE
1269	BS	12	Blue Spruce	Picea pungens		WOODLAND	5	5	5	4	5	3	27	S	PROT	N	-	OFFSITE
1270	BS	8	Blue Spruce	Picea pungens		WOODLAND	3	3	2	3	1	3	15	S	PROT	N	-	OFFSITE
1271	BS	12	Blue Spruce	Picea pungens		WOODLAND	5	5	5	4	4	3	26	S	PROT	N	-	ON
1272	TH	5	ThornappleHemlock	Crataegus spp.		WOODLAND	4	5	4	5	3	3	24	S	PROT	N	-	ON
1273	BS	11	Blue Spruce	Picea pungens		WOODLAND	5	4	4	4	4	3	24	S	PROT	N	-	OFFSITE
1274	BS	12	Blue Spruce	Picea pungens		WOODLAND	4	4	5	5	4	3	25	S	PROT	N	-	ON
1275	BS	11	Blue Spruce	Picea pungens		WOODLAND	4	5	5	4	5	3	26	S	PROT	N	-	OFFSITE
1276	BS	9	Blue Spruce	Picea pungens		WOODLAND	4	5	4	4	4	3	24	S	PROT	N	-	ON
1277	BS	8	Blue Spruce	Picea pungens		LANDMARK	4	4	4	4	4	3	24	S	PROT	Y	-	ON
1278	BS	13	Blue Spruce	Picea pungens		WOODLAND	5	5	4	4	4	3	25	S	PROT	N	-	ON
1279	S	11	Sycamore	Platanus occidentalis		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1280	RP	8	Red Pine	Pinus resinosa		WOODLAND	5	4	4	5	3	3	24	S	PROT	N	-	ON
1281	RP	9	Red Pine	Pinus resinosa		WOODLAND	4	4	4	5	3	3	23	S	PROT	N	-	ON
1282	JZ	12	Japanese zelkova	Zelkova serrata		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1283	CA	5	Crab Apple	Malus coronata	x2.3.2	WOODLAND	3	3	3	5	2	3	19	S	PROT	N	-	ON
1284	TH	9	ThornappleHemlock	Crataegus spp.		WOODLAND	5	5	5	5	4	3	26	S	PROT	N	-	ON
1285	JZ	15	Japanese zelkova	Zelkova serrata		WOODLAND	5	5	5	5	5	3	27	S	PROT	N	-	ON
1286	EP	9	Bradford Pear	Pyrus californiana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1287	JZ	16	Japanese zelkova	Zelkova serrata		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1288	GA	6	Green Ash	Fraxinus pennsylvanica		WOODLAND	2	2	2	2	2	3	13	S	PROT	N	-	ON
1289	CA	3	Crab Apple	Malus coronata		WOODLAND	2	2	2	2	2	3	13	S	PROT	N	-	ON
1290	TH	7	ThornappleHemlock	Crataegus spp.		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1291	RP	17	Red Pine	Pinus resinosa		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1292	EP	7	Bradford Pear	Pyrus californiana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1293	RP	15	Red Pine	Pinus resinosa		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1294	RP	8	Red Pine	Pinus resinosa		WOODLAND	5											

KEY
 OVERALL RANK OF 16 OR MORE, TREE MUST BE REPLACED
 S = EXISTING TREE TO REMAIN
 R = EXISTING TREE TO BE REMOVED
 LINE WITH STRIKE OUT = EXISTING TREE TO BE REMOVED
 REFER TO SHEET T-1.4 FOR ADDITIONAL EXISTING TREE LIST
 REFER TO SHEET L-1.0 FOR TREE REPLACEMENT INFORMATION

TAG NO.	CODE	DBH	COMMON NAME	LATIN NAME	EXTRA TRUNKS	CLASS	CONDITION FACTOR					OVERALL RANK	SAVE / REMOVE	STATUS	> MN	REPLACE	ONSITE	
							trunk	growth rate	structure	insect/diseases	crown dev							life exp
1101	CA	6	Crab Apple	Malus coronaria	>16&S	WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1102	AP	5	Domestic Apple	Malus sylvestris		WOODLAND	5	4	3	5	5	3	23	S	PROT	N	-	ON
1103	NM	13	Norway Maple	Acer platanoides		WOODLAND	4	4	5	5	4	3	25	S	PROT	N	-	ON
1104	BP	8	Bradford Pear	Pyrus calleryana		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1105	BP	4	Bradford Pear	Pyrus calleryana		WOODLAND	5	4	4	5	3	3	25	S	PROT	N	-	ON
1106	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	4	5	3	3	25	S	PROT	N	-	ON
1107	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	4	4	3	25	S	PROT	N	-	ON
1108	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	4	5	5	5	4	3	25	S	PROT	N	-	ON
1109	HL	4	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	3	3	25	S	PROT	N	-	ON
1110	BP	5	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1111	BP	4	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1112	BP	4	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	5	3	26	S	PROT	N	-	ON
1113	BP	4	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	5	3	26	S	PROT	N	-	ON
1114	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1115	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	5	3	27	S	PROT	N	-	ON
1116	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	4	5	5	4	3	26	S	PROT	N	-	ON
1117	HL	7	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1118	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1119	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1120	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	4	5	4	3	26	S	PROT	N	-	ON
1121	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1122	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1123	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1124	LL	8	Litchie/Linden	Tilia cordata		WOODLAND	5	4	4	5	4	2	25	S	PROT	N	-	ON
1125	LL	6	Litchie/Linden	Tilia cordata		WOODLAND	5	4	4	5	3	3	24	S	PROT	N	-	ON
1126	LL	7	Litchie/Linden	Tilia cordata		WOODLAND	5	4	4	5	3	3	24	S	PROT	N	-	ON
1127	LL	7	Litchie/Linden	Tilia cordata		WOODLAND	4	4	4	5	3	3	23	S	PROT	N	-	ON
1128	LL	8	Litchie/Linden	Tilia cordata		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1129	LL	7	Litchie/Linden	Tilia cordata		WOODLAND	5	5	4	5	3	3	25	S	PROT	N	-	ON
1130	LL	8	Litchie/Linden	Tilia cordata		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1131	LL	8	Litchie/Linden	Tilia cordata		WOODLAND	5	4	4	5	4	3	25	S	PROT	N	-	ON
1132	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	3	4	4	5	4	3	23	S	PROT	N	-	ON
1133	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	4	4	5	3	3	24	R	PROT	N	-	ON
1134	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	4	5	5	5	4	3	26	R	PROT	N	REPLACE	ON
1135	HL	4	Honeylocust	Gleditsia triacanthos		WOODLAND	5	4	5	5	3	3	23	R	PROT	N	-	ON
1136	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	4	5	4	3	25	R	PROT	N	-	ON
1137	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	4	4	5	5	4	3	25	R	PROT	N	-	ON
1138	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	4	5	4	3	26	R	PROT	N	-	ON
1139	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	4	5	5	3	27	R	PROT	N	-	ON
1140	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	4	5	4	3	26	R	PROT	N	-	ON
1141	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	4	5	4	3	26	R	PROT	N	REPLACE	ON
1142	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	REPLACE	ON
1143	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	5	4	4	5	4	3	25	R	PROT	N	REPLACE	ON
1144	HL	5	Honeylocust	Gleditsia triacanthos		WOODLAND	4	4	3	5	3	3	22	R	PROT	N	-	ON
1145	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	3	3	26	S	PROT	N	-	ON
1146	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	3	3	26	S	PROT	N	-	ON
1147	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	REPLACE	ON
1148	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	REPLACE	ON
1149	NM	14	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	REPLACE	ON
1150	NM	11	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	3	3	26	R	PROT	N	-	ON
1151	BP	5	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	4	5	4	3	26	S	PROT	N	-	ON
1152	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	3	3	26	S	PROT	N	-	ON
1153	BP	5	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1154	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1155	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1156	RM	5	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1157	BP	5	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1158	NM	12	Norway Maple	Acer platanoides		WOODLAND	4	4	4	5	3	3	24	R	PROT	N	REPLACE	ON
1159	NM	14	Norway Maple	Acer platanoides		WOODLAND	4	5	5	5	3	3	27	S	PROT	N	-	ON
1160	BP	4	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1161	RM	3	Red Maple	Acer rubrum	DEAD	WOODLAND							0	S	PROT	N	-	ON
1162	RM	4	Red Maple	Acer rubrum		WOODLAND	5	5	5	5	4	3	27	S	PROT	N	-	ON
1163	NM	13	Norway Maple	Acer platanoides		WOODLAND	4	4	4	4	4	3	23	R	PROT	N	REPLACE	ON
1164	CA	6	Crab Apple	Malus coronaria		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	REPLACE	ON
1165	CA	2	Crab Apple	Malus coronaria		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	REPLACE	ON
1166	CA	8	Crab Apple	Malus coronaria		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	-	ON
1167	NM	7	Norway Maple	Acer platanoides		WOODLAND	4	5	5	5	5	3	27	R	PROT	N	REPLACE	ON
1168	NM	6	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	REPLACE	ON
1169	NM	6	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	-	ON
1170	NM	7	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	REPLACE	ON
1171	NM	6	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	REPLACE	ON
1172	NM	7	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	REPLACE	ON
1173	NM	7	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	5	3	28	R	PROT	N	REPLACE	ON
1174	BP	3	Bradford Pear	Pyrus calleryana		WOODLAND	5	4	4	5	4	3	25	R	PROT	N	-	ON
1175	BP	3	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	4	5	3	3	25	R	PROT	N	-	ON
1176	BP	4	Bradford Pear	Pyrus calleryana		WOODLAND	5	4	5	4	4	3	25	R	PROT	N	-	ON
1177	BP	4	Bradford Pear	Pyrus calleryana		WOODLAND	5	4	5	4	4	3	25	R	PROT	N	-	ON
1178	NM	6	Norway Maple	Acer platanoides		WOODLAND	5	5	4	5	4	3	25	R	PROT	N	-	ON
1179	NM	6	Norway Maple	Acer platanoides		WOODLAND	5	5	4	5	4	3	25	R	PROT	N	-	ON
1180	NM	4	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	3	3	26	R	PROT	N	-	ON
1181	NM	6	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	REPLACE	ON
1182	NM	6	Norway Maple	Acer platanoides		WOODLAND	4	5	5	5	4	3	26	R	PROT	N	REPLACE	ON
1183	CA	3	Crab Apple	Malus coronaria		WOODLAND	5	5	4	5	3	3	25	R	PROT	N	-	ON
1184	CA	6	Crab Apple	Malus coronaria		WOODLAND	4	4	4	5	4	3	24	R	PROT	N	-	ON
1185	CA	4	Crab Apple	Malus coronaria		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	-	ON
1186	NM	14	Norway Maple	Acer platanoides		WOODLAND	4	4	4	4	3	3	23	R	PROT	N	REPLACE	ON
1187	NM	7	Norway Maple	Acer platanoides	Dead	WOODLAND							0	S	PROT	N	-	ON
1188	RM	3	Red Maple	Acer rubrum		WOODLAND	5	4	4	4	4	3	24	R	PROT	N	-	ON
1189	NM	7	Norway Maple	Acer platanoides		WOODLAND	3	3	3	3	4	3	14	R	PROT	N	REPLACE	ON
1190	NM	12	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	REPLACE	ON
1191	NM	8	Norway Maple	Acer platanoides		WOODLAND	5	5	5	5	4	3	27	R	PROT	N	REPLACE	ON
1192	BP	7	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1193	BP	7	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
1194	BP	7	Bradford Pear	Pyrus calleryana		WOODLAND	5	5	5	5	5	3	28	S	PROT	N	-	ON
11																		

- Note**
1. New Site Fixtures to match CCT of Existing Luminaires
 2. Site Fixtures within 50FT of a Residential Property Line or Public Right-Of-Way to have External or Optic Mounted Back-Light Shielding to meet City Ordinances.
 3. Site Fixtures near public roadways to be aimed/directed to minimize light spill - per City Ordinances
 4. All fixtures to be full cut-off - any adjustable arm fixtures to be mounted at 90deg.



Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Loading & Unloading	+	3.3 fc	8.8 fc	0.8 fc	11.0:1	4.1:1
Parking Lot	+	2.9 fc	7.5 fc	0.8 fc	9.4:1	3.6:1
Property Line	+	0.6 fc	2.7 fc	0.0 fc	N/A	N/A
Walkways - Proposed Outlots	+	2.6 fc	9.6 fc	0.5 fc	19.2:1	5.2:1
Walkways Major C-E	+	3.6 fc	12.9 fc	0.2 fc	64.5:1	18.0:1
EXISTING ESTIMATED	+	1.3 fc	5.2 fc	0.0 fc	N/A	N/A

Site Plan

SCALE: 1" = 60'-0"

project:

Hunters Square Shopping Center
SWC Orchard Lake Rd. & 14 Mile Rd.
Farmington Hills, MI



Colliers Engineering & Design

20700 CIVIC CENTER DRIVE
SUITE 170
SOUTHFIELD, MICHIGAN
48076-4148

PH 248.540.7700 FX 248.540.2710
www.rogvoy.com

drawing:

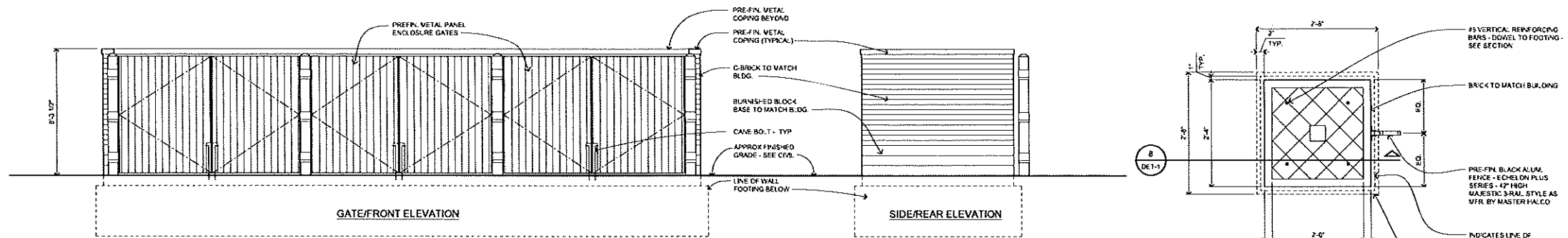
Architectural
Site Details

DO NOT SCALE DRAWING

issue date: 08 SEPT, 2023
drawn: BDB
checked: MD
approved: MD

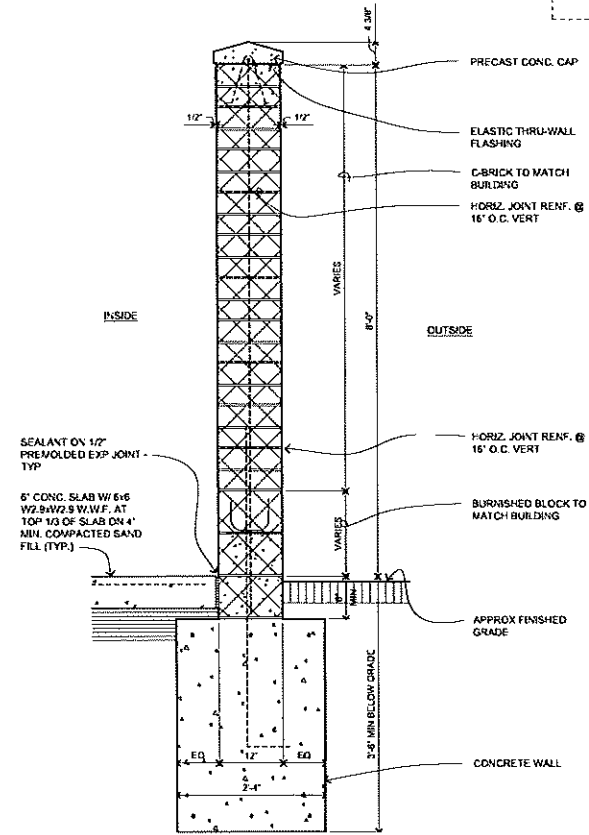
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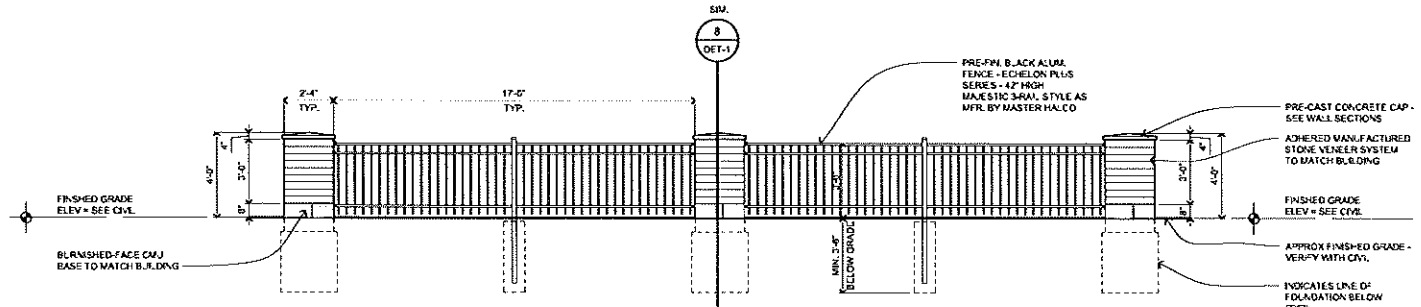


8 Dumpster Enclosure Elevations
1/4" = 1'-0"
(TYP. FOR SINGLE/DOUBLE/TRIPLE ENCLOSURES)

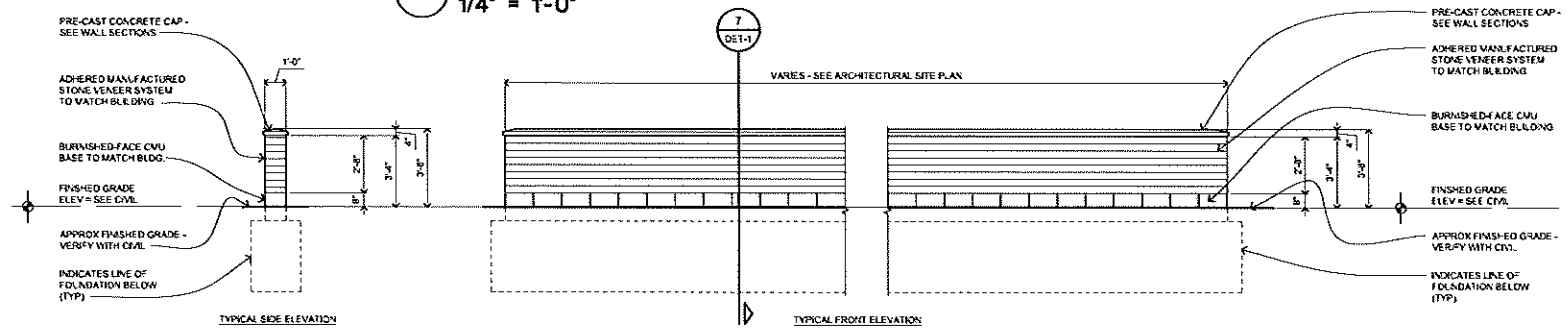
4 Typ. Pier Plan
3/4" = 1'-0"



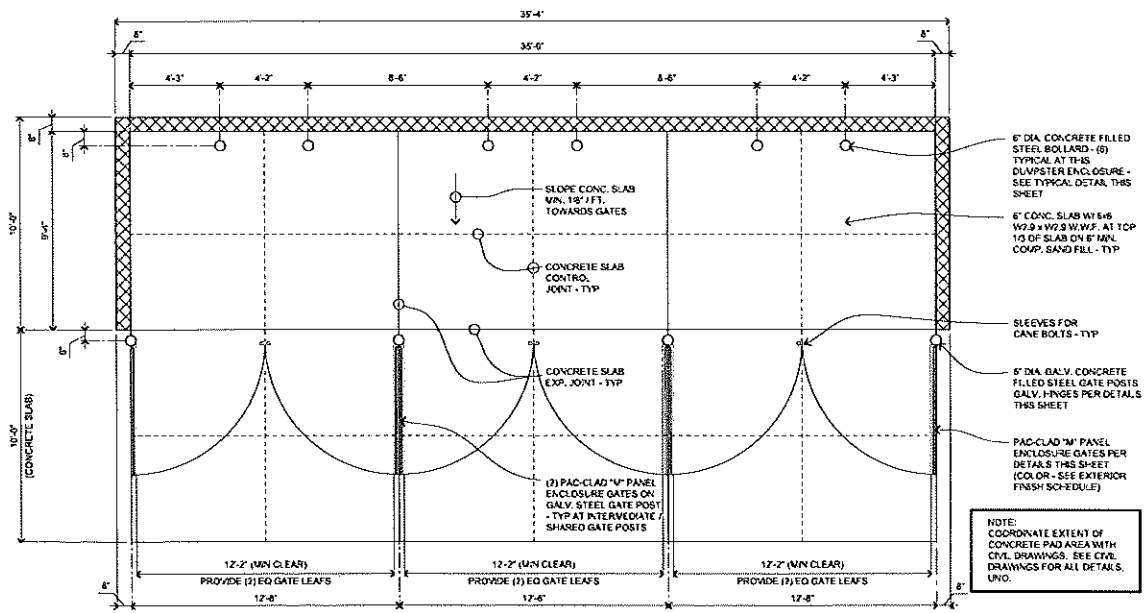
3 Dumpster Enclosure Wall Section
3/4" = 1'-0"



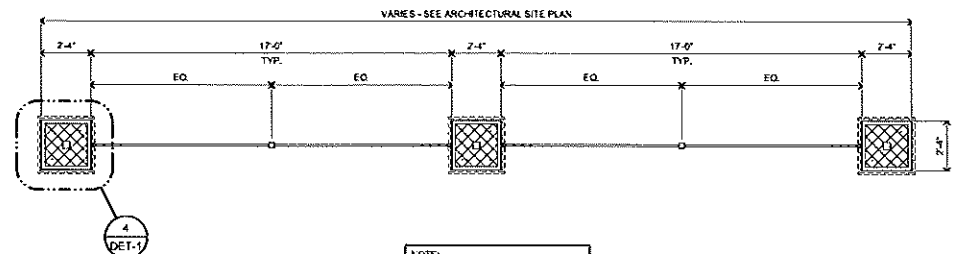
6 Fencing Screen Wall Elevation
1/4" = 1'-0"



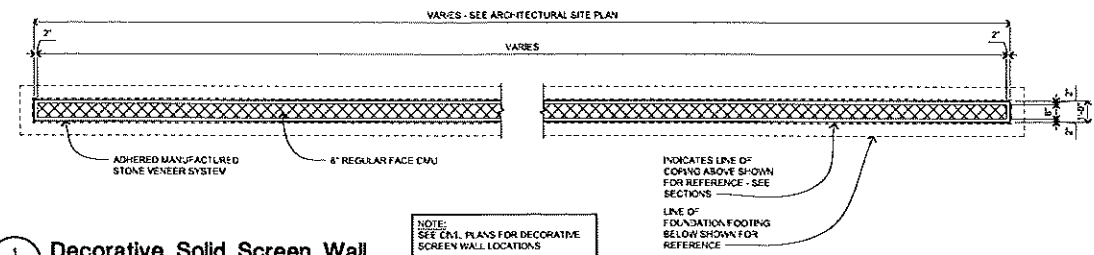
5 Decorative Screen Wall Elevation
1/4" = 1'-0"



1 Dumpster Enclosure Plan - Triple
1/4" = 1'-0"



2 Fencing Screen Wall Plan
1/4" = 1'-0"



1 Decorative Solid Screen Wall
Plan 1/4" = 1'-0"

NOT FOR CONSTRUCTION

DET-1

issued for:
 REVIEW: 27 JUNE 2023
 REVISED: 24 AUG 2023
 REVISED: 07 SEPT. 2023

project:

Hunters Square Shopping Center

SWC Orchard Lake Rd. & 14 Mile Rd.
 Farmington Hills, MI



Colliers Engineering & Design

20700 CIVIC CENTER DRIVE
 SUITE 170
 SOUTHFIELD, MICHIGAN
 48076-4148

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drawing:

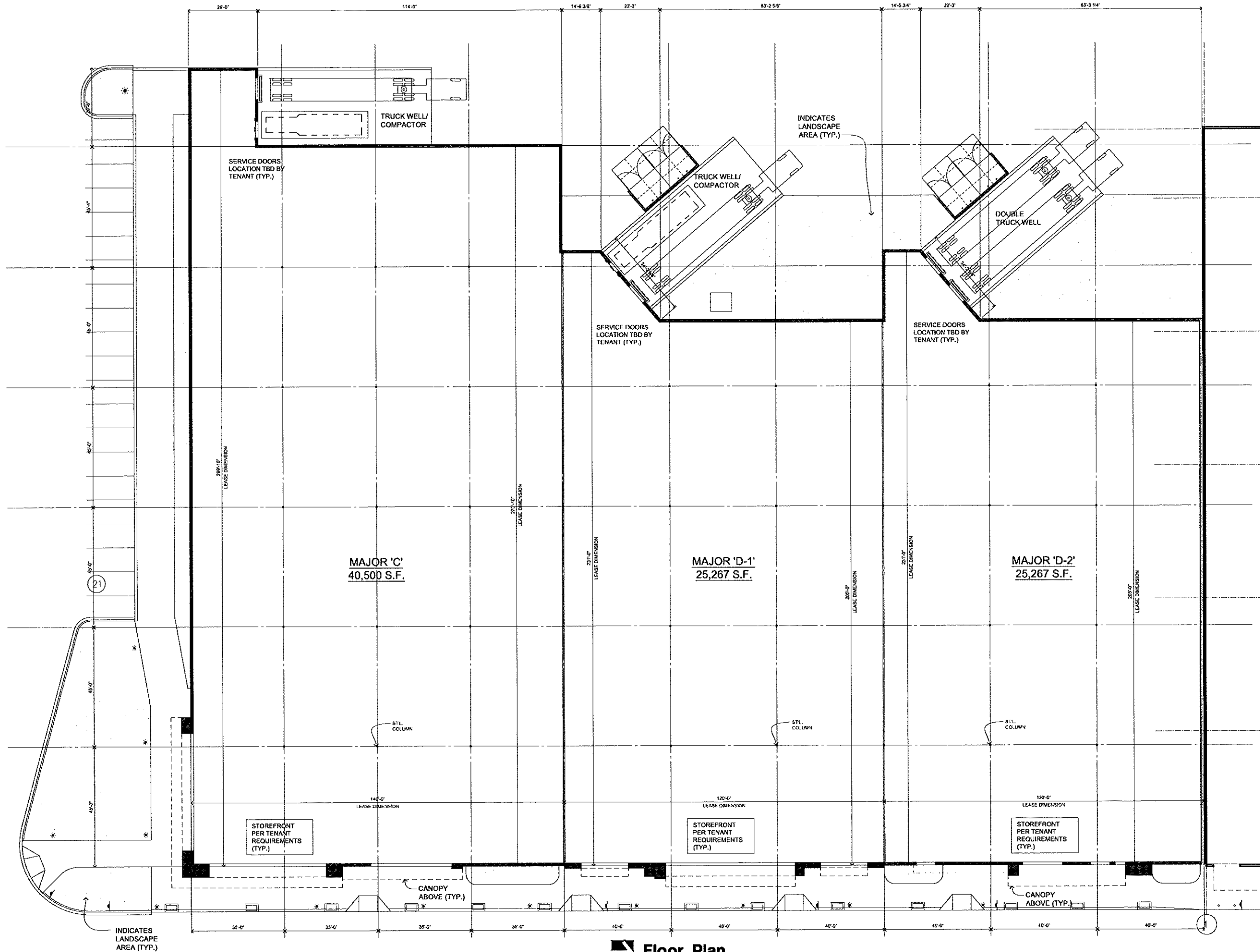
Conceptual Floor Plan

DO NOT SCALE DRAWING

issue date:
 drawn: KL
 checked: MD
 approved: MD

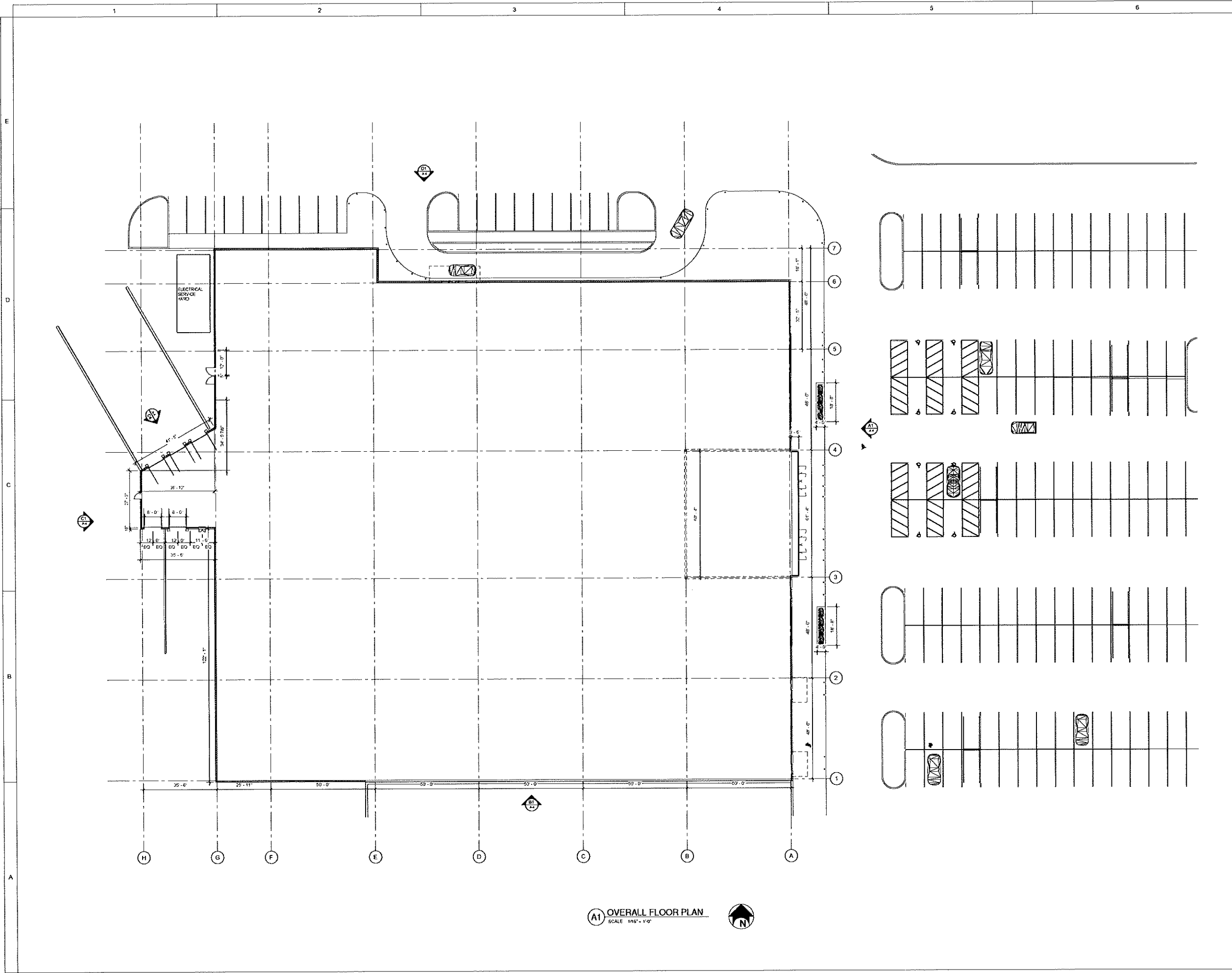
file number: **21028**

sheet



Floor Plan

Scale: 1/16"=1'-0"



B BERGMANN
 ARCHITECTS ENGINEERS PLANNERS
 2000 W. Capital Highway Suite 200
 Lansing, MI 48917
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 F 517.272.8832

meijer

REV	DATE	DESCRIPTION

FARMINGTON HILLS, MICHIGAN
 ORCHARD LAKE ROAD AND 1/4 MILE ROAD
 FARMINGTON HILLS, MI 48334

OVERALL FLOOR PLAN

ISSUED FOR	
PERMIT	
SD	
CONSTRUCTION	
TRR	K.H.

23010766A

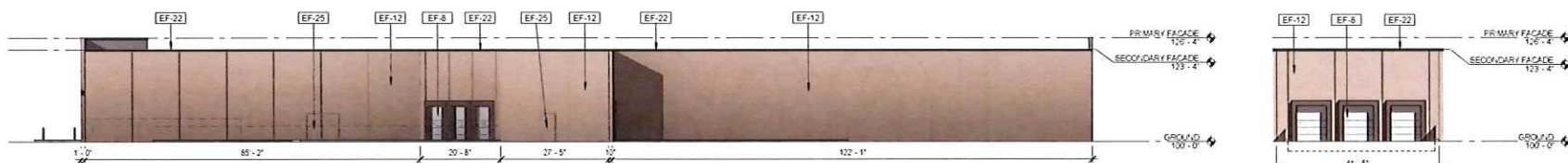
A-3

A1 OVERALL FLOOR PLAN
 SCALE 1/8" = 1'-0"



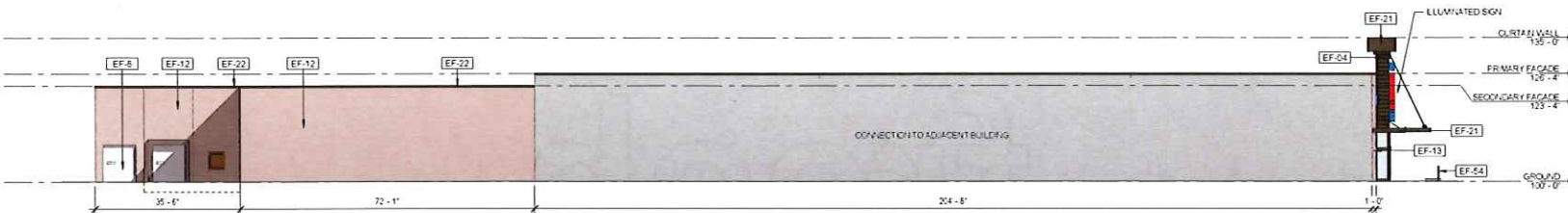


D1 NORTH ELEVATION
SCALE: 1/16" = 1'-0"



C1 WEST ELEVATION
SCALE: 1/16" = 1'-0"

C4 TRUCK DOCK ELEVATION
SCALE: 1/16" = 1'-0"



B1 SOUTH ELEVATION
SCALE: 1/16" = 1'-0"



A1 EAST ELEVATION
SCALE: 1/16" = 1'-0"



RENDERING FINISH LEGEND			
CODE	FINISH	PRODUCT	COLOR
EF-03	CORRUGATED METAL PANEL	CENTRAL CS600	SAGE BROWN
EF-04	CORRUGATED METAL PANEL	CENTRAL CS200	DARK BRONZE
EF-05	ENTRANCE AND STOREFRONT FRAMES - 1" INSULATED GUARDIAN GLASS - CRYSTAL GRAY 8963	-	DARK BRONZE ANODIZED ALUMINUM
EF-06	1" INSULATED BRANDEL PANEL WITH CRYSTAL GRAY 8963 AND WARM GRAY BRANDEL PANEL	-	WARM GRAY
EF-08	SECTIONAL OVER-HEAD DOORS	-	WHITE
EF-12	INSULATED PRECAST CONCRETE WALL	AP FORMLINER SMOOTH	TAN
EF-13	INSULATED PRECAST CONCRETE WALL - TEXTURED CONCRETE FINISH	FECKLI FORMLINER	TAN
EF-14	INSULATED PRECAST CONCRETE WALL - HORIZONTAL WOOD GRAIN	PROTO FORMLINER	DARK BROWN MATCH EF-17
EF-17	INSULATED PRECAST CONCRETE WALL	AP FORMLINER SMOOTH BAND	DARK BROWN
EF-18	INSULATED PRECAST CONCRETE WALL - BRICK IMPRINTED	AP FORMLINER MODULAR BRICK VELDUBELDUBA	LIGHT CREAM
EF-20	CANOPY FASCIA PANEL AND SOFFIT	FAC-CLAD	DARK BRONZE
EF-21	SHEET METAL FLASHING AND TRIM - EDGE FLASHING AT CANOPY	FAC-CLAD	DARK BRONZE
EF-22	SHEET METAL FLASHING AND TRIM - EDGE FLASHING AT PARAPET	FAC-CLAD	DARK BRONZE
EF-25	FIELD PAINTED STEEL - HOLLOW METAL DOORS & FRAMES	SHERWIN WILLIAMS	TAN MATCH EF-12
EF-27	FIELD PAINTED STEEL - HOLLOW METAL DOORS & FRAMES	SHERWIN WILLIAMS	DARK BRONZE
EF-30	STEEL SUPPORT	-	DARK BRONZE STEEL BEAM
EF-53	STAINLESS STEEL BOLLARD	-	BRUSHED STAINLESS STEEL
EF-54	BOLLARD	-	GREY
EF-63	VENTED SOFFIT PANEL - LINA CLAD UC-500	FIRESTONE	DARK BRONZE
EF-73	FIELD PAINTED STEEL - METAL DECKING	SHERWIN WILLIAMS	DARK BRONZE

REV. DATE DESCRIPTION

REV.	DATE	DESCRIPTION

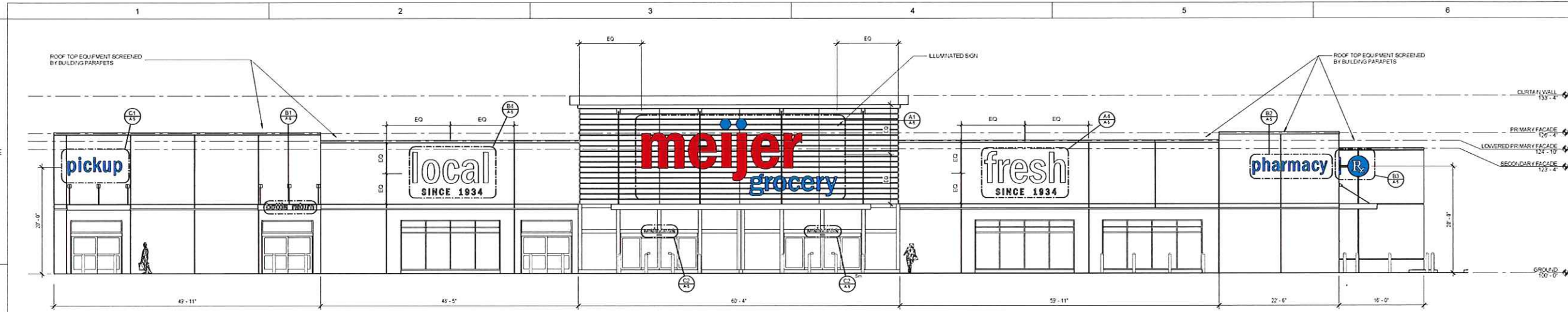
FARMINGTON HILLS, MICHIGAN
ORCHARD LAKE ROAD AND 14 MILE ROAD
FARMINGTON HILLS, MI 48324

ELEVATIONS AND 3D VIEWS

ISSUED FOR
PERMIT
ED
CONSTRUCTION

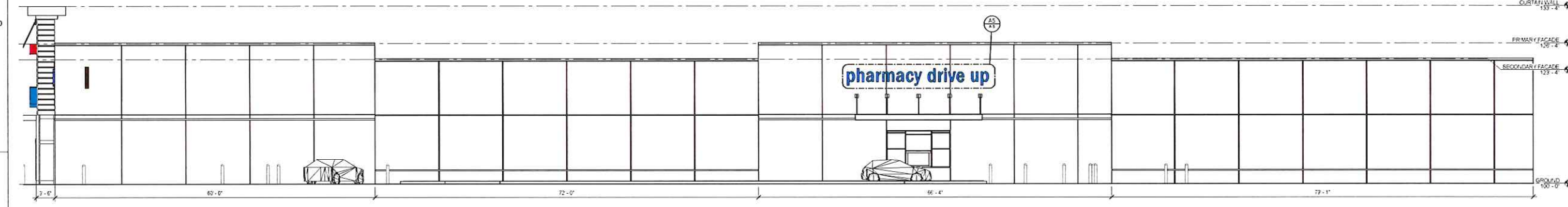
TRR KJH

23010766A



EAST ELEVATION SIGNAGE
SCALE 1/8" = 1'-0"

6,981.69 S.F.



NORTH ELEVATION SIGNAGE
SCALE 1/8" = 1'-0"

6,968.71 S.F.

pickup

C1 PICKUP SIGNAGE 20.17 S.F.
SCALE 1/2" = 1'-0"

welcome

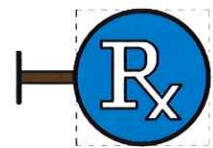
C2 WELCOME SIGNAGE 8.45 S.F.
SCALE 1/2" = 1'-0"

bottle return

B1 BOTTLE RETURN SIGNAGE 9.90 S.F.
SCALE 1/2" = 1'-0"

pharmacy

B2 PHARMACY SIGNAGE 30.84 S.F.
SCALE 1/2" = 1'-0"



B3 RX BLADE SIGNAGE 16.0 S.F.
SCALE 1/2" = 1'-0"

local
SINCE 1934

B4 LOCAL SIGNAGE 68.46 S.F.
SCALE 1/2" = 1'-0"

EAST ELEVATION SIGNAGE

TOTAL SIGNAGE: 811.2 S.F.
TOTAL ELEVATION: 6,981.69 S.F.
SIGNAGE PERCENTAGE OF ELEVATION: 11.62%

NORTH ELEVATION SIGNAGE

TOTAL SIGNAGE: 60.67 S.F.
TOTAL ELEVATION: 6,968.71 S.F.
SIGNAGE PERCENTAGE OF ELEVATION: 0.87%

meijer
grocery

A1 MEIJER GROCERY SIGNAGE 531.67 S.F.
SCALE 1/2" = 1'-0"

fresh
SINCE 1934

A4 FRESH SIGNAGE 97.26 S.F.
SCALE 1/2" = 1'-0"

pharmacy drive up

A5 PHARMACY DRIVE UP SIGNAGE 60.67 S.F.
SCALE 1/2" = 1'-0"

BERGMANN
ARCHITECTS ENGINEERS PLANNERS
7700 W. Cassman Highway Suite 200
Lansing, MI 48117
P: 313.272.8805
F: 313.272.8806

meijer

REV.	DATE	DESCRIPTION

FARMINGTON HILLS, MICHIGAN
CFCHEFD LAKE ROAD AND 14 MILE ROAD
FARMINGTON HILLS, MI 48334

ELEVATIONS AND SIGNAGE

ISSUED FOR	PERMIT	EO	CONSTRUCTION

23010766A

A-5

Issued for:
 REVIEW 28 JUN 2023
 REVIEW 07 SEPT 2023
 REVISED 05 DEC 2023

project:

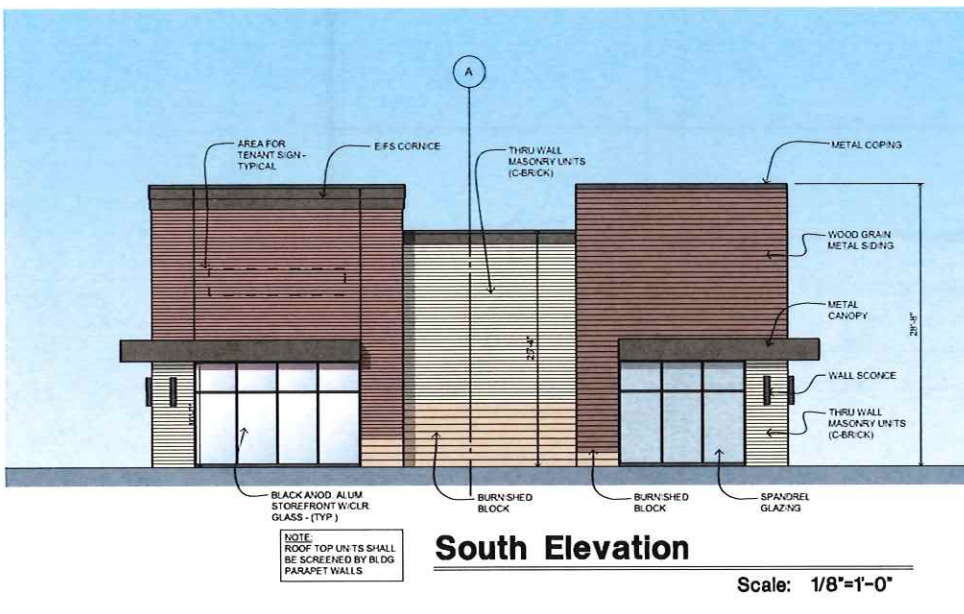
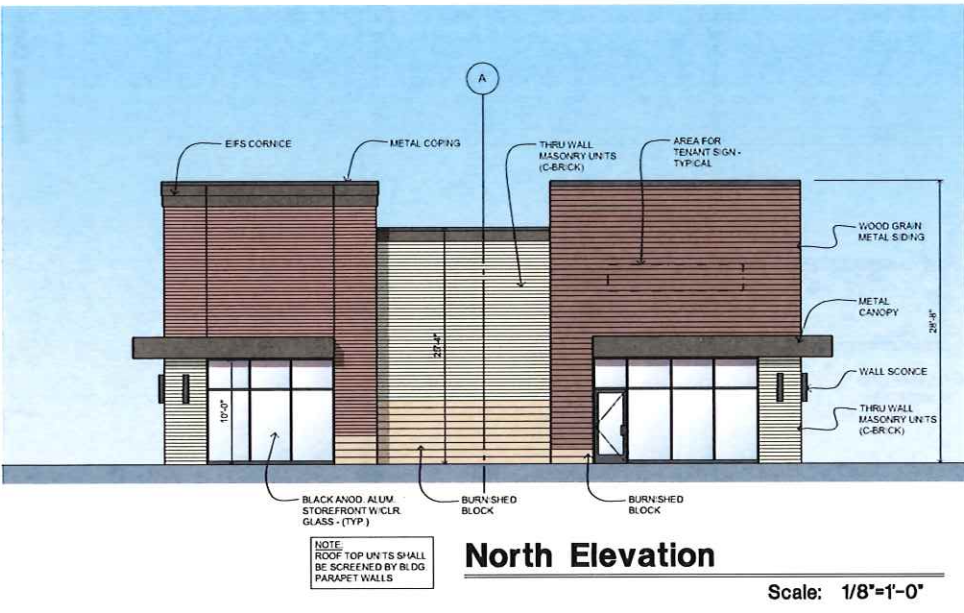
Proposed Outlot Building 'D' at
Hunters Square Shopping Center
 SWC Orchard Lake Rd. & 14 Mile Rd.
 Farmington Hills, MI



Colliers Engineering & Design

20700 CIVIC CENTER DRIVE
 SUITE 170
 SOUTHFIELD, MICHIGAN
 48076-4148

PH 248.540.7700 FX 248.540.2710
 www.rogvoy.com



drawing:

Conceptual
 Building
 Elevations

DO NOT SCALE DRAWING

Issue date: 28 JUN 2023
 drawn: BDB
 checked: MD
 approved: MD

file number: 21028-OD

sheet:

A-6

Issued for:
 REVIEW 28 JUNE 2023
 REVIEW 07 SEPT. 2023
 REVISED 06 DEC. 2023

project:

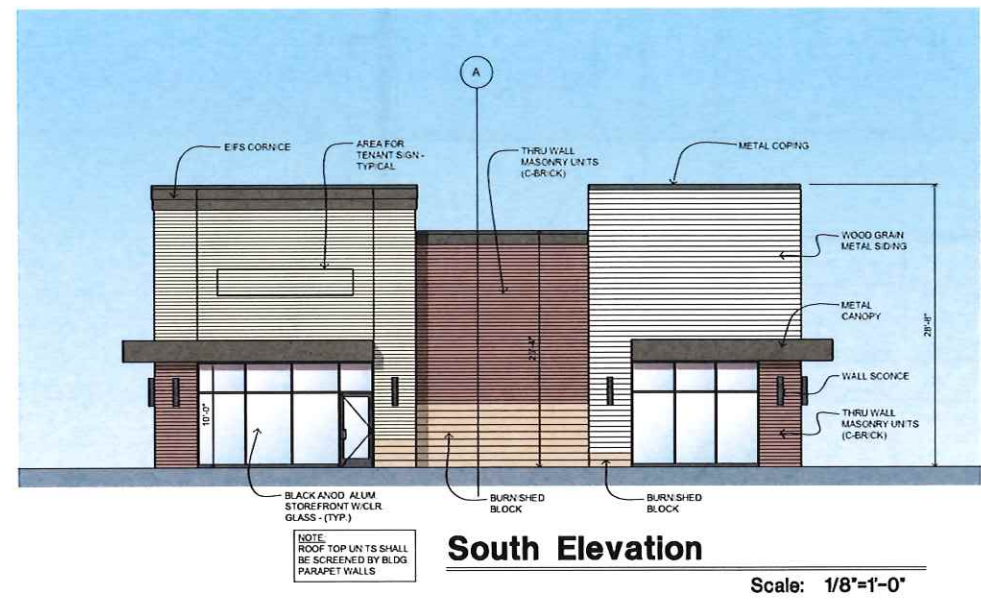
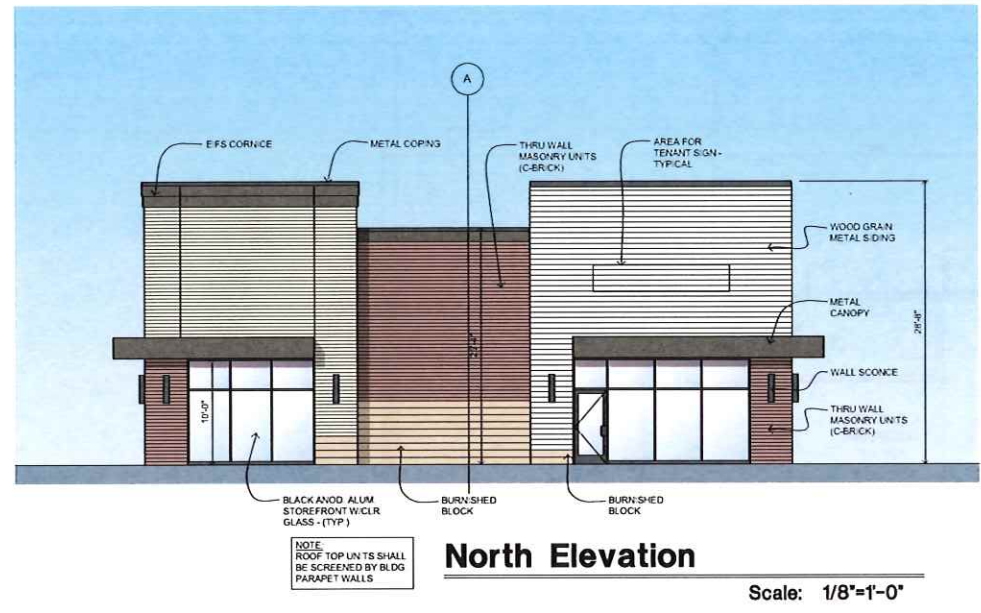
Proposed Outlot Building 'E' at
Hunters Square Shopping Center
 SWC Orchard Lake Rd. & 14 Mile Rd.
 Farmington Hills, MI

ROGVOY
 ARCHITECTS
has joined

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drawing:
Conceptual Building Elevations
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issue date: 28 JUN 2023
 drawr: BDB
 checked: MD
 approved: MD

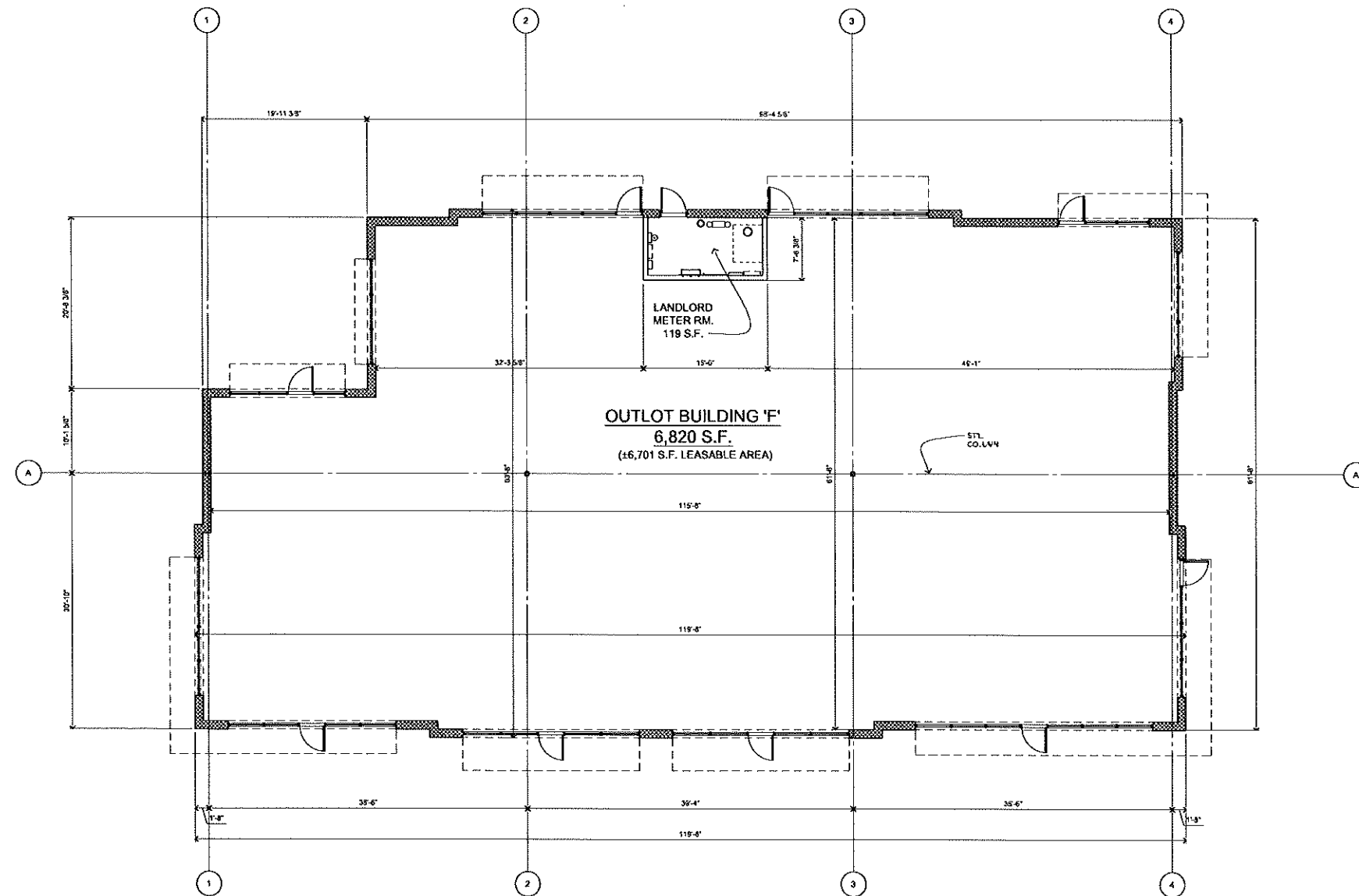
file number: 21028-OE
 sheet:

issued for:
 REVIEW: 27 JUNE 2023
 REVISED: 07 SEPT. 2023

project:

Proposed Outlot Building 'F' at
Hunters Square Shopping Center
 SWC Orchard Lake Rd. & 14 Mile Rd.
 Farmington Hills, MI

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Floor Plan - Outlot Building 'F'
 +/-6,820 S.F. Scale: 1/8"=1'-0"

drawing:

**Conceptual
 Floor
 Plan**

DO NOT SCALE DRAWING

issue date: 27 JUN 2023
 drawr: BDB
 checked: MD
 approved: MD

file number: 21028-OF

sheet:

A-9

Issued for:
 REVIEW 28 JUNE 2023
 REVISED 07 SEPT. 2023
 REVISED 05 DEC. 2023

project:

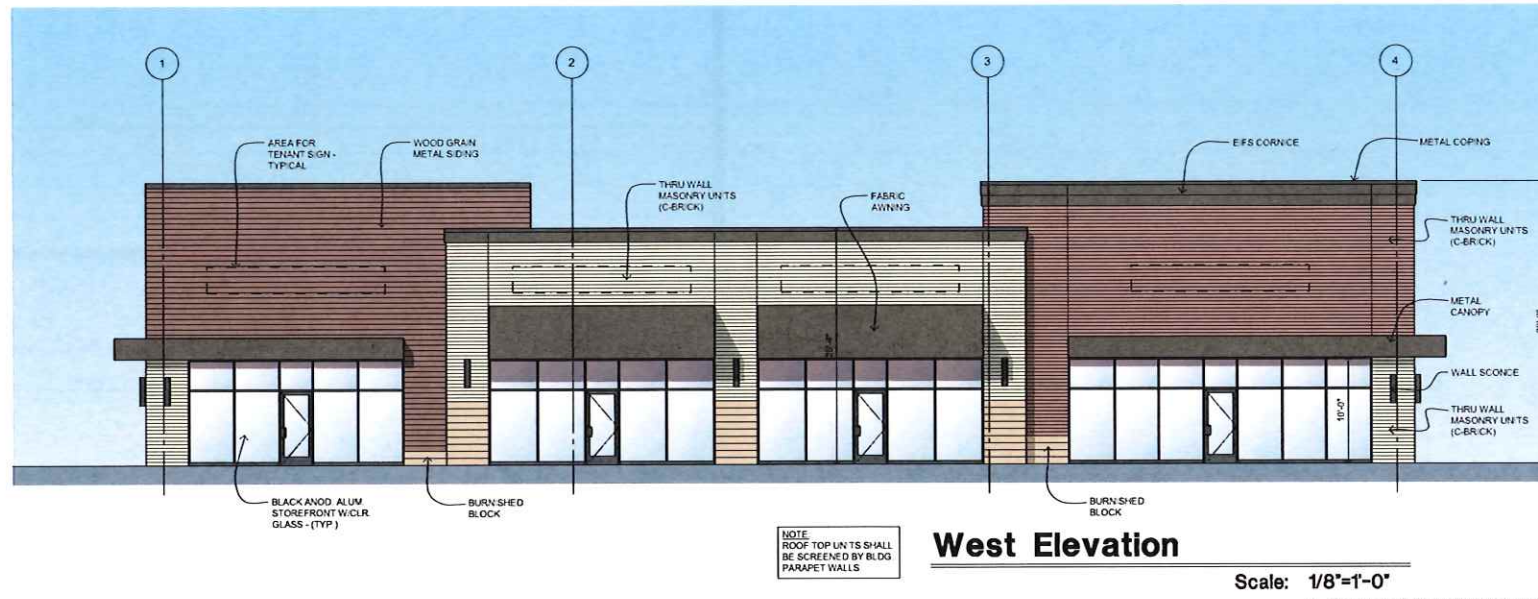
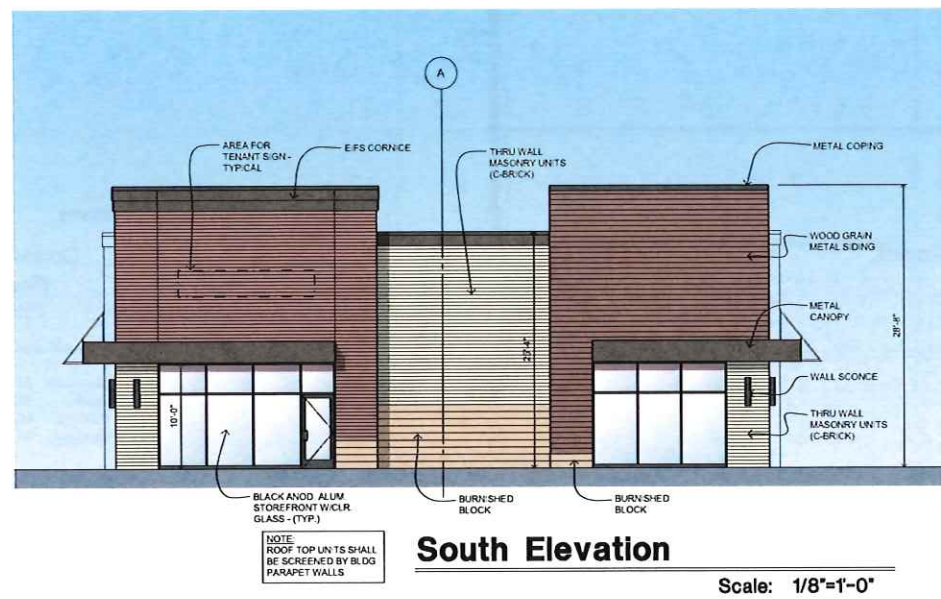
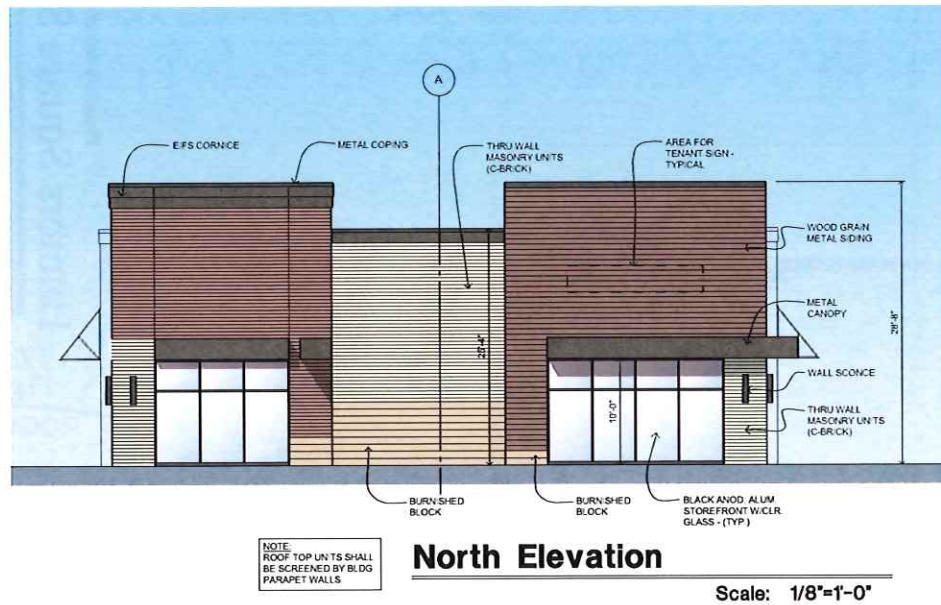
Proposed Outlot Building 'F' at
Hunters Square Shopping Center
 SWC Orchard Lake Rd. & 14 Mile Rd.
 Farmington Hills, MI



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drawing:
Conceptual Building Elevations

DO NOT SCALE DRAWING

Issue date: 28 JUN 2023
 drawn: BDB
 checked: MD
 approved: MD

file number: 21028-OF

sheet:

A-10

issued for:
 REVIEW 27 APR 2023
 REVIEW 07 SEPT 2023
 REVISED 05 DEC 2023

project:

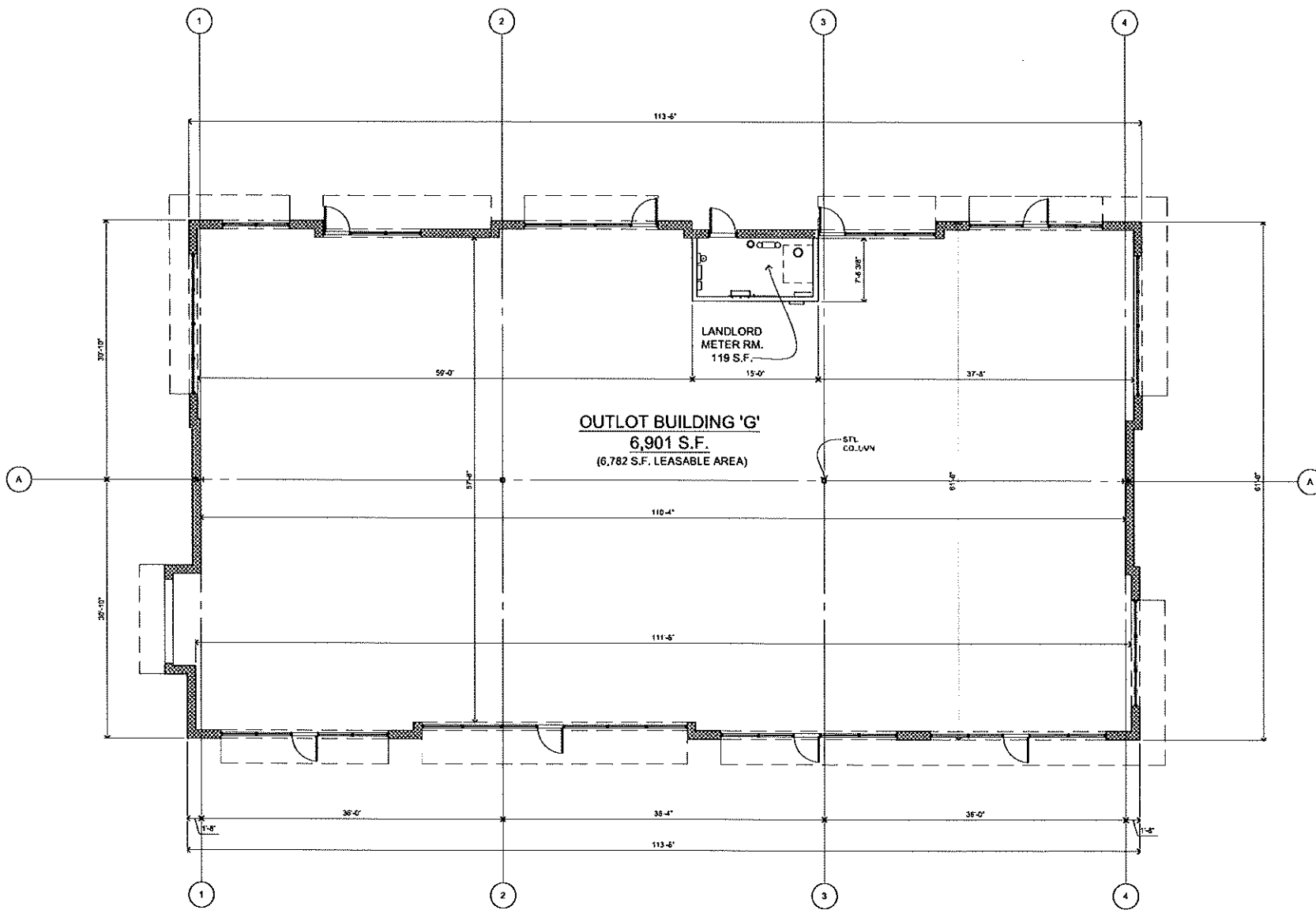
Proposed Outlot Building 'G' at
Hunters Square Shopping Center
 SWC Orchard Lake Rd. & 14 Mile Rd.
 Farmington Hills, MI

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drawing:

**Conceptual
 Floor
 Plan**

DO NOT SCALE DRAWING

issue date: 27 JUN 2023
 drawn: BDB
 checked: MD
 approved: MD

file number: 21028-OG

sheet:

Floor Plan - Outlot Building 'G'
 6,901 S.F. Scale: 1/8"=1'-0"

Issued for:
 REVIEW 28 JUNE 2023
 REVIEW 07 SEPT. 2023
 REVISED 05 DEC. 2023

project:

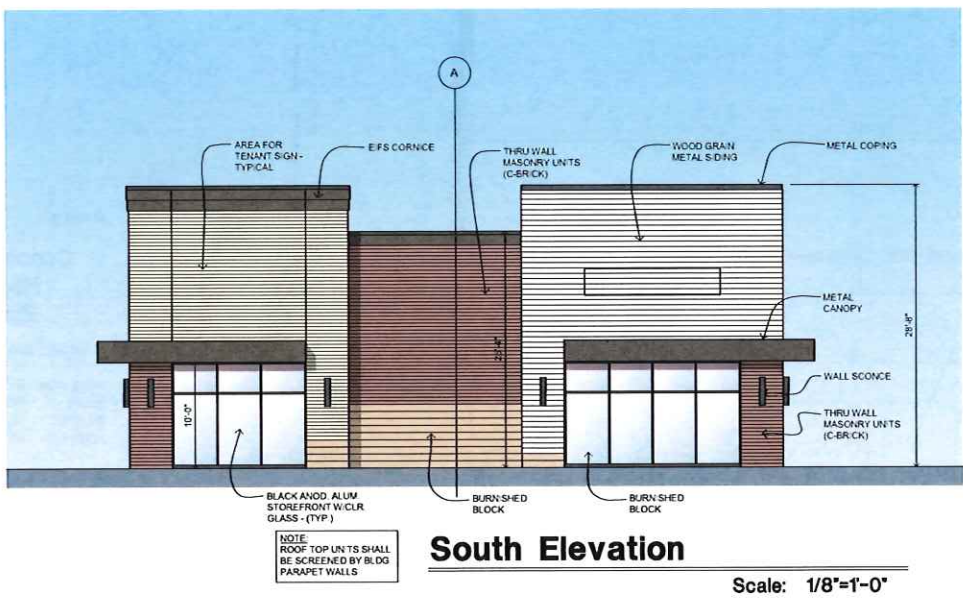
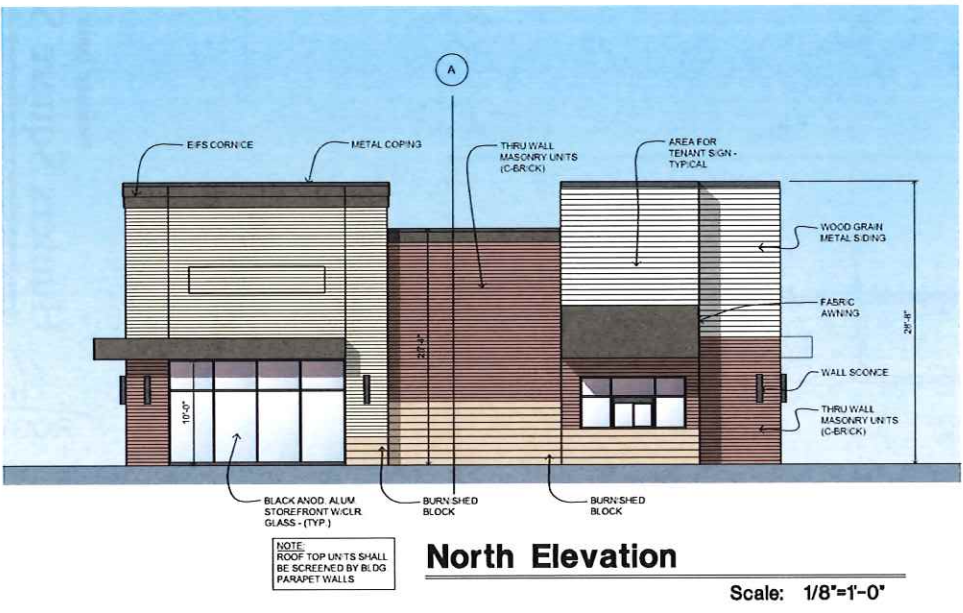
Proposed Outlot Building 'G' at
Hunters Square Shopping Center
 SWC Orchard Lake Rd. & 14 Mile Rd.
 Farmington Hills, MI

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 ARCHITECTS
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drawing:
Conceptual Building Elevations
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Issue date: 28 JUN 2023
 drawn: BDB
 checked: MD
 approved: MD

file number: 21028-OG
 sheet:

December 15, 2023

Farmington Hills Planning Commission
 31555 W 11 Mile Rd
 Farmington Hills, MI 48336

Final PUD Review

Case: PUD 2, 2023
 Site: 30825 and 31361 Orchard Lake Road (Parcel ID 22-23-03-226-028 & -027)
 Applicant: Timothy Collier
 Plan Date: 8/29/2023; revisions 12/8/2023
 Zoning: B-2 Community Business

We have completed a review of the application for PUD, site plan, and landscape plan approval referenced above and a summary of our findings is below. Items in **bold** require specific action by the Applicant. Items in *italics* can be addressed administratively.



Project Summary

The applicant proposes reconfiguration of an existing large shopping center, including a revamp of several larger tenant units, the addition of outlots, and some accompanying changes to the parking lot. The use will remain solely commercial.

Existing Conditions

- 1. **Zoning.** The site is currently zoned B-2 Community Business.
- 2. **Existing site.** The site is 28.82 acres and developed with the Hunter’s Square shopping center. The site has no wetlands or other notable natural features apart from trees.
- 3. **Adjacent Properties.**

Direction	Zoning	Land Use
North	B-2/ West Bloomfield Commercial	Commercial
RA-1	B-3/B-2	Commercial
South	RC-2	Multi-Family
West	RC-2	Multi-Family

- 4. **Site configuration and access.** The site is accessed from five driveways onto Orchard Lake Road, with an additional access point to 14 Mile Road via an easement over a neighboring property.

PUD Qualification

Under Section 34-3.20.2, the Planning Commission may make a determination that the site qualifies for a PUD based on the following criteria and procedures. On August 17, the Planning Commission approved the following motion:

“MOTION by Brickner, support by Varga, that the Planning Commission makes a preliminary finding that PUD 2, 2023, received July 18, 2023, submitted by Timothy Collier, qualifies for the Planned Unit Development Option under Section 34-3.20.2.A through D. of the Zoning Chapter. It is further determined that the proposal meets at least one of the objectives as outlined in Section 34- 3.20.2.E.i. thru viii., specifically objectives i, ii, v, vi, vii., and that it be made clear to the applicant that final granting of the PUD plan and contract requires approval by City Council, after recommendation by the Planning Commission”

This determination was based upon the following:

Criteria for qualification. In order for a zoning lot to qualify for the Planned Unit Development option, the zoning lot shall either be located within an overlay district or other area designated in this chapter as qualifying for the PUD option, or it must be demonstrated that all of the following criteria will be met as to the zoning lot:

- A. The PUD option may be effectuated in any zoning district.
- B. The use of this option shall not be for the sole purpose of avoiding the applicable zoning requirements. Any permission given for any activity or building or use not normally permitted shall result in an improvement to the public health, safety and welfare in the area affected. *The commercial use of the property is not proposed to change significantly; the development of the outlots occurs partially within the front setback of the B-2 district; drive-through restaurants are also not typically permitted in the B-2 district. The applicant notes that attempts will*

generally be made to meet B-2 standards, with additional landscaping, amenities, and architectural quality as trade-offs for any necessary deviations.

- C. The PUD shall not be utilized in situations where the same land use objectives can be accomplished by the application of conventional zoning provisions or standards. Problems or constraints presented by applicable zoning provisions shall be identified in the PUD application. Asserted financial problems shall be substantiated with appraisals of the property as currently regulated and as proposed to be regulated.

The applicant has provided a letter specifying the constraints of the zoning ordinance from which they are seeking relief, along with the degree of relief sought (the list also included at the end of this letter). More generally, the narrative notes that the applicant is seeking a flexible mix of commercial uses to support the introduction of more pedestrian amenities and a new layout.

- D. The Planned Unit Development option may be effectuated only when the proposed land use will not materially add service and facility loads beyond those contemplated in the Future Land Use Plan unless the proponent can demonstrate to the sole satisfaction of the city that such added loads will be accommodated or mitigated by the proponent as part of the Planned Unit Development.

The fundamental use of the site will not change. The applicant has provided a traffic study showing projected changes in trips to and from the site from current conditions; we defer to engineering for a full review of this study. There is some potential to close at least one if not two curb cuts, though the site plan does not show this (Planning Commission did not condition a recommendation of approval on closing any access points).

- E. The Planned Unit Development must meet, as a minimum, one of the following objectives of the city (criteria addressed by the applicant in bold):

- i. **To permanently preserve open space or natural features because of their exceptional characteristics or because they can provide a permanent transition or buffer between land uses.**

The applicant points to an expansion of green space along Orchard Lake Road; there may be additional opportunities to return paved areas to an unpaved state, or establish new gathering areas, visual focal points (such as public art pieces), and sidewalks to aid the flow of people through the large parking lot. The applicant's letter lists the following "proposed community benefits" in a letter:

- 1) Public artwork
- 2) Reduction of paved areas on site
- 3) Addition of pedestrian pathways
- 4) Traffic calming measures to increase pedestrian safety
- 5) Diversity of uses on site including grocery, restaurants, office and entertainment
- 6) Enhancement of existing landscape buffer adjacent to residential to the west
- 7) Enhancement of landscape buffer along Orchard Lake Road
- 8) Additional shrubs and perennial landscape enhancements within parking lot
- 9) Added outdoor plaza areas with benches
- 10) Addition of stormwater quality measures
- 11) Deferred/future covered bus stop if/when SMART reinstates bus line in this area
- 12) Reduction of 424 parking spaces and reduction of pavement

- ii. **To permanently establish land use patterns which are compatible or which will protect existing or planned uses.**
The applicant has highlighted and responded to this objective, referring to a more accessible overall center with better stormwater management and a more dynamic tenant mix. The plan does also show some of the buildings moving away from the residential property line to establish larger loading zones.
- iii. To accept dedication or set aside open space areas in perpetuity.
- iv. To provide alternative uses for parcels which can provide transition buffers to residential areas.
- v. **To guarantee the provision of a public improvement which could not otherwise be required that would further the public health, safety, or welfare, protect existing or future uses from the impact of a proposed use, or alleviate an existing or potential problem relating to public facilities.**
The applicant notes enhanced non-motorized access and new green spaces; see our response to objective i above; details of art and some amenities (such as the type of benches) have not yet been provided.
- vi. **To promote the goals and objectives of the Master Plan for Land Use.**
The applicant addresses this item with notes about business stabilization.
- vii. **To foster the aesthetic appearance of the city through quality building design and site development, the provision of trees and landscaping beyond minimum requirements; the preservation of unique and/or historic sites or structures; and the provision of open space or other desirable features of a site beyond minimum requirements.**
The applicant calls for higher-end design, architecture, and materials on the site, and also notes that the outlots help foster walkability and change the feel of the corridor; the new green space and plazas between these buildings area also highlighted in the applicant's response.
- viii. To bring about redevelopment of sites where an orderly change of use is determined to be desirable.

Though only one objective must be met by the plan, the applicant's narrative directly addresses objectives I, ii, v, vi, and vii.

- F. The PUD shall not be allowed solely as a means of increasing density or as a substitute for a variance request; such objectives should be pursued through the normal zoning process by requesting a zoning change or variance.
The primary reason for seeking a PUD here appears to be a broad desire for greater flexibility than the B-2 district provides, both in terms of uses and site layout.

Final determination.

The Planning Commission held a public hearing on the final PUD plan on November 16, 2023. After substantial public comment, the majority of which was in support of Marvin's Marvelous Mechanical Museum, with commenters expressing a hope that the business will not be lost.

Per Section 34-3.20.5.B, the following must be submitted when seeking final determination of PUD qualification:

a. A boundary survey of the exact acreage being requested done by a registered land surveyor or civil engineer (scale not smaller than one inch equals one hundred (100) feet).	√
b. A topographic map of the entire area at a contour interval of not more than two (2) feet. This map shall indicate all major stands of trees, bodies of water, wetlands and unbuildable areas (scale: not smaller than one inch equals one hundred (100) feet).	√
c. A proposed land use plan indicating the following at a scale no smaller than one inch equals one hundred (100) feet (1" = 100'):	√
(1) Land use areas represented by the zoning districts enumerated in Section 34-3.1.1 through Section 34-3.1.30 of this chapter.	√
(2) Vehicular circulation including major drives and location of vehicular access. Preliminary proposals as to cross sections and as to public or private streets shall be made.	√
(3) Transition treatment, including minimum building setbacks to land adjoining the PUD and between different land use areas within the PUD.	√
(4) The general location of nonresidential buildings and parking areas, estimated floor areas, building coverage and number of stories or height.	√
(5) The general location of residential unit types and densities and lot sizes by area.	√
(6) A tree location survey as set forth in Section 34-5.18, Tree Protection, Removal and Replacement.	√
(7) The location of all wetlands, water and watercourses and proposed water detention areas.	√
(8) The boundaries of open space areas that are to be preserved and reserved and an indication of the proposed ownership thereof.	√
(9) A schematic landscape treatment plan for open space areas, streets and border/transition areas to adjoining properties.	√
d. A preliminary grading plan, indicating the extent of grading and delineating any areas which are not to be graded or disturbed.	√
e. An indication of the contemplated water distribution, storm and sanitary sewer plan.	√
f. A written statement explaining in detail the full intent of the applicant, indicating the type of dwelling units or uses contemplated and resultant population, floor area, parking and supporting documentation, including the intended schedule of development.	√

Site Plan & Use:

1. **Summary of Proposed Use.** The applicant has submitted a site plan. The plan proposes three new “Major” tenant spaces for uses consistent with a commercial shopping center with corresponding site improvements. The tenant spaces, comprising 157,564 square feet, will replace a single structure that includes 190,155 square feet of space. The plans also propose four new outlots along Orchard Lake Road, one of which includes a drive-in business. Four existing buildings are to be maintained in their current condition. The total floor area of the site with the proposed improvements will be 353,223 square feet, an overall reduction of 7,270 square feet.

Uses in a shopping center are consistent with principal permitted uses listed in the B-2 District, including retail businesses, personal service establishments which perform services on the premises, fast food or carry out restaurants, and sit-down restaurants. However, drive-in businesses, or establishments that serve patrons while in a motor vehicle, are not permitted in the B-2 District.

Uses not permitted in an underlying district may be permitted as part of a PUD. The applicant is requesting that the following five uses be permitted in this development:

- Drive-in restaurant
- Pharmacy pick-up window
- Sidewalk sales
- Indoor recreation facilities
- Veterinary hospital or clinic

2. **Density.** The plans include 353,223 square feet of floor area. The lot size is 1,241,024 square feet (net). The lot coverage is 28.5%.
3. **Master Plan.** The 2009 Future Land Use Map in the City of Farmington Hills Master Plan for Future Land Use designates this site as Shopping Center Type Business. This classification is also assigned to the properties across from the subject property, along Orchard Lake Road, as well as other commercial corridors in the City. Areas identified for Shopping Center Type Business are intended to be compatible with other uses in the shopping center environment and are aligned with lots in the B-1, B-2, or b-4 Zoning districts.

The plan identified several sites as a part of the “Orchard Lake Redevelopment Area,” but the Hunter’s Square Shopping Center was not included. However, the lots directly across the street on Orchard Lake were identified as potential Mixed-Use Redevelopment, with a vision for this area similar to a central business district, including an increase in the intensity of uses and enhanced pedestrian and bicycle accommodations and amenities.

The Master Plan also cited two relevant studies, both of which included recommendations for Orchard Lake Road. The City’s 2006 Sustainability Study included a recommendation to establish a mixed-use development area along Orchard Lake between Eleven and Fourteen Mile Roads, with an emphasis on the potential to create a “Central Business District,” characterized by buildings up to five stories with “zero” setbacks from the roads. The 2002 Orchard Lake Road Corridor Study was a coordinated plan for the area along Orchard Lake extending from Grand River Avenue to Pontiac. The Master Plan cited a quote from the Corridor Study that recommends a coordinated approach to development with “common visual linkages, landscaping, calming of traffic, pedestrian friendly ways, lighting and signage.”

4. **Dimensional Standards of the B-2 District (34-3.1.24.E). The applicant is seeking relief from required setbacks in the B-2 District as follows (two of these would confirm existing nonconforming setbacks, as noted):**
- a. 34-3.1.24.E- Setback relief for front setback (reduction from 75’ to 58.49’)
 - b. 34-3.1.24.E- Setback relief for rear setback (reduction from 75’ to 69.33’)
 - c. 34-3.1.24.E- Setback relief for south side setback (reduction from 75’ to 74.88’—existing setback)
 - d. 34-3.5.2.E- Relief from parking setback (SE corner of site) (reduction from 10’ to 4.96’—existing setback)

Standard	B-2 Requirement	Proposed
Lot Size	Not Specified	28.85 acres (gross)/ 28.49 acres (net)
Lot width	Not Specified	Not Specified
Lot coverage	Not Specified	28.5%
Front setback	75 ft	58.49 ft
Rear setback ¹	75 ft	69.34 ft
Side setback (north)	20 ft	83.24 ft
Side setback (south) ²	75 ft	74.88 ft
Building height	50 ft/ 3 stories	33.17 ft
Front Yard Open Space	20% of Front Yard, or 15’	100,333 ³

Footnotes:

¹The rear setback for the B-2 District is 20 feet; however, when adjacent to residential, the required setback is 75 feet.

²The side yard setback for the B-2 District is 20 feet; however, when adjacent to residential, the required setback is 75 feet.

³The front yard open space required for this site is 20,255 square feet and the applicant indicated that 100,333 square feet of open space area was provided which exceeds 20%.

5. **Notes to Schedule of Regulations (34-35)**

- a. Landscaping for all yards abutting a street shall be provided.
- j. Parking in the front yard setback. Parking may be permitted in the front yard setback provided that the parking is set back not less than 10 feet and an area within the front yard, including that 10’, remains as lawn or landscaped area which is equal to the specified percentage of the area of the required front yard setback. This standard is met, except in the southeastern portion of the site, where the existing nonconforming setback will remain (this is included in the list of requested relief from ordinance standards).*
- n. See the loading/unloading requirements of Section 34-5.4. *See comments below on loading/ unloading.*
- o. Rooftop equipment shall be screened in accordance with Section 34-5.17. *See comments below on rooftop equipment.*
- v. Required landscaped area. In any yard abutting a street or freeway, a landscaped area not less than 10 feet deep and abutting the street or freeway shall be provided in the setback. All proposed improvements appear compliant with this standard, but existing parking on the southeast corner of the site does not. *In all areas of the site where*

changes are proposed, this standard is met. Areas in the southeastern portion of the site that are currently nonconforming with this standard are not proposed to change.

6. **B-2 and B-4 District Required Conditions (34-3.10).** The uses in the B-2 District shall require review and approval of the site plan by the planning commission. The conceptual PUD plans do not indicate the types of uses included as proposed tenants. *The applicant has provided a narrative that specifies the uses not permitted in the B-2 district that they are requesting to permit in this development:*

- Drive-in restaurant
- Pharmacy pick-up window
- Sidewalk sales
- Indoor recreation facilities
- Veterinary hospital or clinic

7. **Pedestrian Access. (34-3.24).** Pedestrian access-ways are required between public sidewalks and principal building entrances, principal building entrances and off-street parking lots, and between uses on abutting parcels.

- The site plans show where pedestrian access is proposed to be included.
- Pedestrian access is included along the frontage of the four proposed “Major” buildings, as well as the south side of Major Building C and the North side of Major Building E.
- Marked crossings are provided between the “Major” buildings and parking areas.
- Outlots D, E, and F include paved pedestrian access along all frontages, marked crossings to the parking lot, and connections to the sidewalk on Orchard Lake Road.
- No pedestrian improvements are considered for portions of the site where existing structures are maintained as-is, except for enhanced crosswalk striping.
- Sidewalks are included in the right-of-way on Orchard Lake Road and Fourteen Mile, but no sidewalks are included along Hunter Lane. This is an existing condition and no changes are proposed to the buildings or parking areas adjacent to Hunter Lane.

8. **Fast food or Carry-out Restaurants (34-4.27).** The ordinance requires that all fast food or carry out restaurants in the B-2 District are located in a building which includes at least one other principal use. The structure for the drive-in restaurant is intended for up to four tenants and meets this standard.

9. **Drive-in Restaurants (4.35).** The use standards for drive-in restaurants are specific to the B-3 and LI-1 district, where drive-in restaurants are allowed to be located. **However, since the applicant has included a drive-in as a part of a PUD application, the commission may consider these standards as a part of this review.**

The drive-in standards include the following requirements. (The plans do not comply with the regulations listed under a. and d.).

- a. Drive-in restaurants shall provide a building setback of at least 60 feet from any street right-of-way. Signs and other structures shall provide setbacks required in Section 34-3.1.

The plans propose the drive-in restaurant to be located 58.59' from the Right-of-Way, within the front yard setback and less than 60' from the Right-of-Way.

- b. No space set aside for the stacking of vehicles waiting to be served from a drive-in window shall be closer than thirty-five (35) feet to any adjacent residential zoning lot, except when such lot is occupied by use other than residential.

The proposed drive-in restaurant is not within 35' of adjacent residential.

- c. The zoning lot occupied by such use shall not abut an RA District unless the district is separated from the lot by a major or secondary thoroughfare.

The subject site is not abutting any property zoned RA.

- d. Vehicular access drives to a drive-in restaurant shall be located at least sixty (60) feet from the Right-of-Way of any Intersecting street.

Vehicular access drives are less than 60' from the right-of-way.

- 10. **Off Street Parking (34-5.2)** The applicant has not submitted detailed floor plans, and instead estimated usable floor area as 75% of the total floor area of the development. Based on the Usable Floor Area provided by the applicant, the proposed parking meets the parking requirements for a Commercial Shopping Center use.

Standard	Required Parking	Proposed
Commercial Shopping Center- First 15,000 SF	1 Space/ 150 SF UFA	15,000SF/150 SF= 100 Spaces
Commercial Shopping Center- Next 15,0001-100,000 SF	1 space/ 175 SF UFA	84,999/175= 486 Spaces
Commercial Shopping Center- UFA in excess of 100,001 SF	1 space/ 200 SF UFA	164,916/200= 825 Spaces
Total	1,411 Spaces	1,329 Spaces plus 82 Deferred Spaces (1,411 Spaces)

- 11. **Parking requirements for Drive-In uses. (34.-5.2.12).** **The plans include a proposed vehicle drive lane for waiting and stacking in the front yard of the drive-in use, but vehicle drive lanes for waiting and stacking are not allowed to be located in a front yard. The applicant has requested approval of this arrangement as part of the PUD agreement.**

- 12. **Off-Street Parking Space Layout, Standards, Construction, and Maintenance (34-5.3).** The applicant included the following minimum parking space layout dimensions.

34-5.3.3.A Minimum Off Street Parking Dimensions		
	Required	Provided
Maneuvering Lane Width	20 ft	20 ft
Parking Space Width	9 ft	9 ft

Parking Space Length	20 ft*	20 ft
*May include a maximum 2' unobstructed vehicle overhand area at the front of the parking space		

13. **Off-Street Loading and Unloading (34-5.4).** The applicant provides 15 loading spaces on the plans, including 4 temporary off-hours spaces to serve the outlots. The B-2 loading calculation is derived from total building frontage as follows:

	Frontage	Total SF of Loading
Major Building C	140	1,400 sf
Major Building D-1	120	1,200 sf
Major Building D-2	120	1,200 sf
Major Building E	241.09	2,410 sf
Outlot D	98.67	987 sf
Outlot E	93	930 sf
Outlot F	99	990 sf
Outlot G	114	1,140 sf
Total	1025.76	10,257 sf
Number of Loading Spaces at 500 SF/ Loading Space		20 Spaces

The applicant has requested a reduction in required loading spaces from 20 to 15 as part of the PUD agreement.

14. **Landscaping (34-5.14) and Screening (34-5.15).** On portions of the site that are proposed to change, proposed landscaping meets or exceeds ordinance requirements. The untouched portion of the site will continue to be nonconforming.
15. **Exterior Lighting (34-5.16).** The applicant provided five different fixture types, including three different luminaires. All fixtures are mounted at a mounting height that is below (or meets) the 30' height requirement. The applicant included three fixtures with adjustable-arm luminaires. The plan notes that lights within 50 feet of residential property will be fitted with house-side shielding refractors, and that all fixtures will be mounted at the 90-degree cut-off angle.

On portions of the site not proposed for redevelopment, the applicant is not proposing to change any lighting, but has shown that lighting on the plans.

Proposed lighting exceeds the required 4:1 uniformity ratio and should be adjusted accordingly.

We recommend a lower color temperature than the proposed color temperature of 4,000-5,000K. Color temperatures below 3,500K are less white and less disruptive to sleep cycles.

16. **Rooftop Screening (34-5.17).** Screening of rooftop equipment is required where any is provided. Mechanical equipment on the ground shall be screened per Section 34-5.1.4.D. The applicant's response letter indicates that all mechanical equipment will be screened per ordinance requirements.

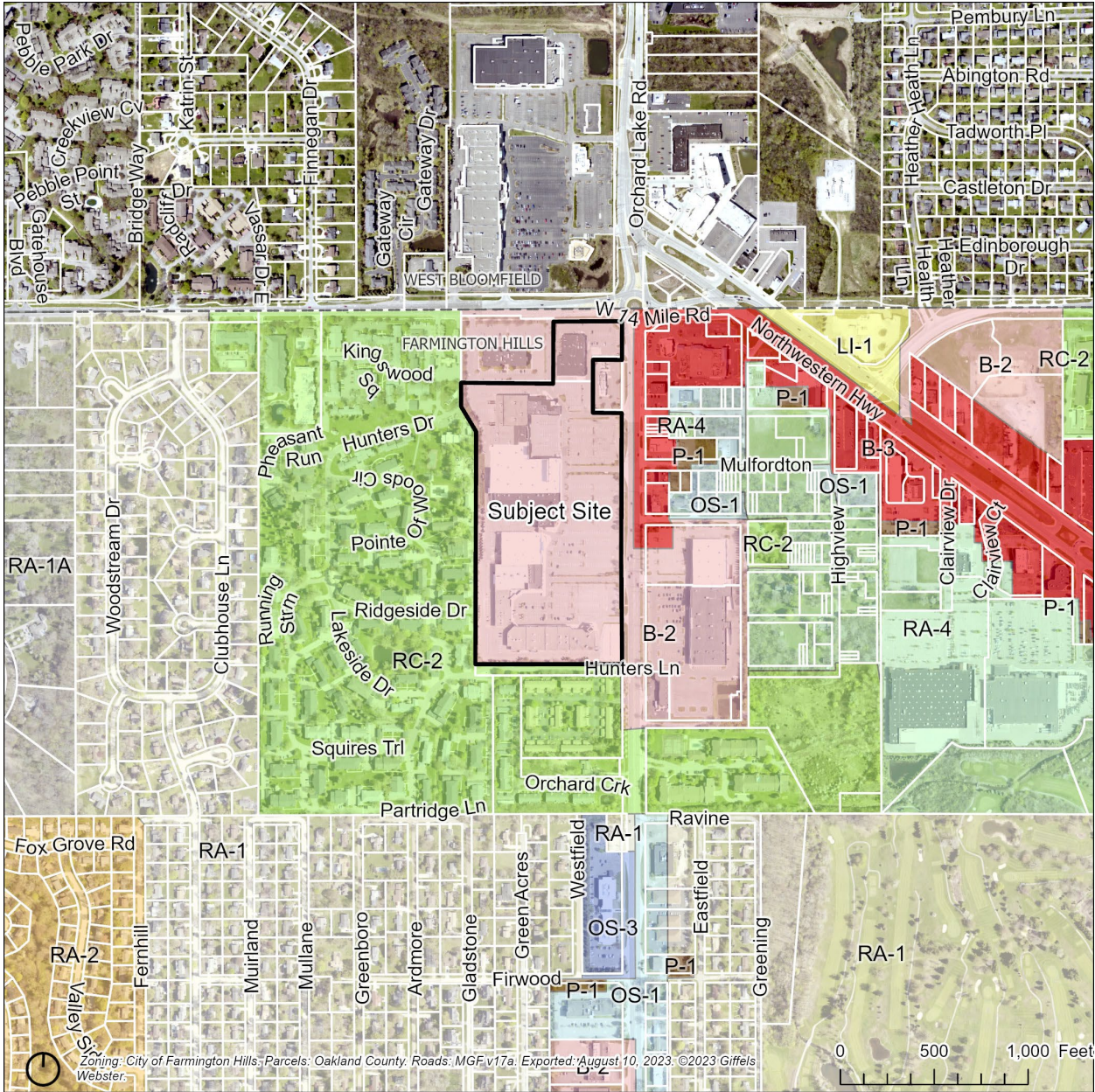
17. **Tree Removal (34-5.18).** A tree inventory was provided as required and appears to be accurate, with replacement calculations indicating that 34 standard trees will be replaced on a 1:1 basis, and no landmark trees will be removed from the site.
18. **Pedestrian access and connectivity from public rights of way (34-5.19).** Pedestrian access across the site is generally improved by the plan, with two new connections from the major buildings to Orchard Lake Road.
19. **Dumpster Enclosures. (34-5.1.3).** The applicant has provided several new dumpster enclosures. There is one behind Major Building E, one with each truckwell behind Major Buildings D-1 and D-2, one behind the drive-in lane near Outlot G, and one between the primary access drive and Outlot D. The plans also include an existing enclosure south of the primary access drive. The dumpster enclosure proposed near outlot G is located in the front yard setback, which is not allowed. In addition, the existing dumpster south of the primary access drive is also located in this front yard setback. **The applicant is requesting the front yard dumpster location as a part of the PUD approval.**
20. **Relief Sought from Ordinance Standards**
 - a. 34-3.1.24.E- Setback relief for front setback (reduction from 75' to 58.49')
 - b. 34-3.1.24.E- Setback relief for rear setback (reduction from 75' to 69.33')
 - c. 34-3.1.24.E- Setback relief for south side setback (reduction from 75' to 74.88'—existing setback)
 - d. 34-3.5.2.E- Relief from parking setback (reduction from 10' to 4.96'—existing setback)
 - e. 34-4.35- Relief from requirement that a drive-in use be set back a minimum of 60 feet from the right-of-way (building is set back 58.59 ft, and the drive-through lane is about 25 ft from the front property line) – **not listed by applicant, but applicable**
 - f. 34-3.1.24- Permit uses not permitted in the B-2 district:
 - Drive-in restaurant
 - Pharmacy pick-up window
 - Sidewalk sales
 - Indoor recreation facilities
 - Veterinary hospital or clinic
 - g. 34-3.24- Pedestrian connections between ROW and Principal Building for Outlot G
 - h. 34-5.2.12- Stacking/waiting lane for drive-in located in front yard
 - i. 34-5.1.3- Dumpster enclosure located in front yard setback
 - j. 34-5.4- Relief from loading space requirements (reduction from 20 to 15)
 - k. 34-5.5.5.A- Relief from nonconforming sign standards to allow re-use/re-location of currently existing nonconforming signs
 - l. 34-5.5.3.B.ii.a(2)- Relief from wall sign standards to allow more signs than permitted (increase from 2 to 5 on the Meijer storefront)

We are available to answer questions.

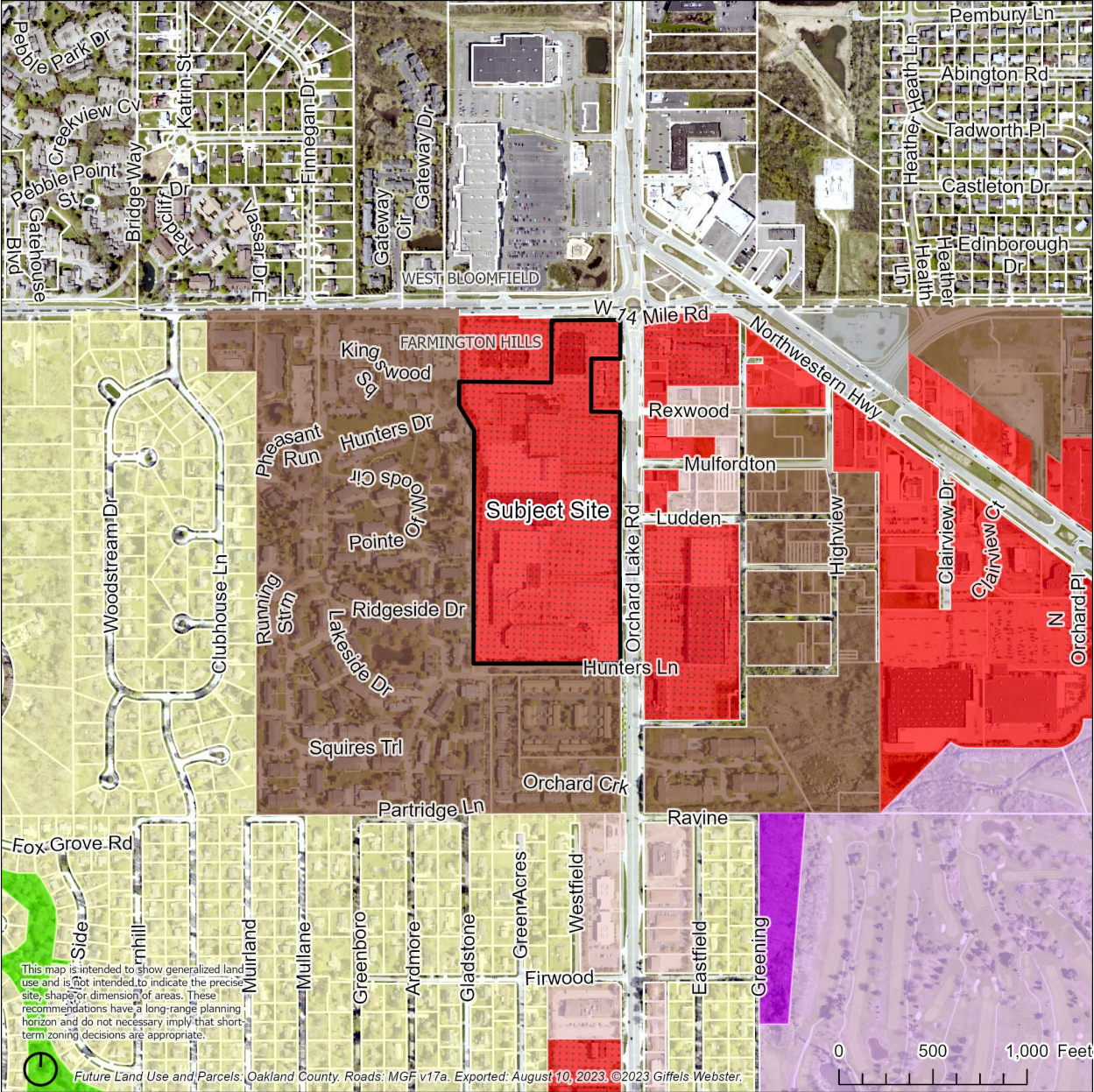
Respectfully,
Giffels Webster

Joe Tangari, AICP
Principal Planner

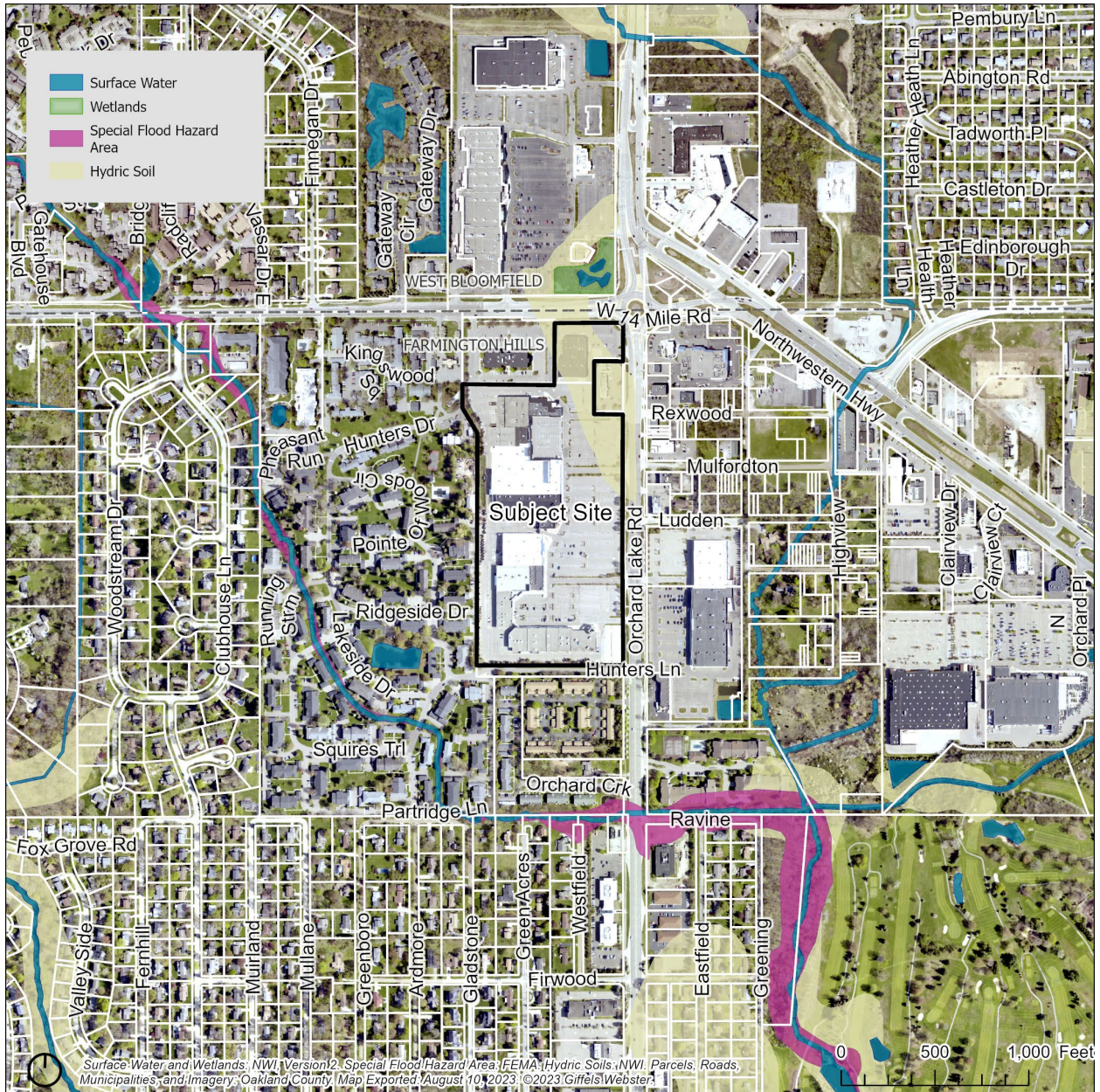
Julia Upfal, AICP
Senior Planner



Current zoning



Master Plan designations for this area. See discussion above.



Wetland map

**MINUTES
CITY OF FARMINGTON HILLS
PLANNING COMMISSION MEETING
31555 W ELEVEN MILE ROAD
FARMINGTON HILLS, MICHIGAN
August 17, 2023, 7:30 P.M.**

CALL MEETING TO ORDER

The Planning Commission Regular Meeting was called to order by Vice Chair Trafelet at 7:30 p.m.

ROLL CALL

Commissioners present: Aspinall, Brickner, Grant, Mantey, Trafelet, Stimson, Varga, Ware

Commissioners Absent: Countegan

Others Present: City Planner Perdonik, City Attorney Schultz, Planning Consultant Tangari

APPROVAL OF THE AGENDA

MOTION by Brickner, support by Ware, to approve the agenda as published.

Motion passed unanimously by voice vote.

PUBLIC HEARING

REGULAR MEETING

A. PLANNED UNITED DEVELOPMENT (PUD) QUALIFICATION 2, 2023

LOCATION: 30825 and 31361 Orchard Lake Road
PARCEL I.D.: 22-23-03-226-027 and 028
PROPOSAL: Redevelopment of shopping center in B-2, Community Business zoning district
ACTION REQUESTED: Qualification of Planned Unit Development
APPLICANT: Timothy Collier
OWNER: RPT Realty, L.P.

Referencing his August 9, 2023 memorandum, Planning Consultant Tangari gave the background and review for this request for PUD qualification, for redevelopment of a 28.82 acre site containing a shopping center in a B-2, Community Business zoning district, located at 30825 and 31361 Orchard Lake Road.

Planning Consultant Tangari made the following points:

- The applicant proposes reconfiguration of an existing large shopping center, including a revamp of several larger tenant units, the addition of outlots, and some accompanying changes to the parking lot. The use will remain solely commercial.
- Regarding the criteria for PUD qualification:
 - A. The PUD option may be effectuated in any zoning district.
 - B. The use of the property is not proposed to change; the development of the outlots appears to occur partially within the front setback of the B-2 district; drive-through restaurants are also not typically permitted in the B-2 district. The applicant notes that attempts will generally be made to meet B-2 standards, with additional landscaping, amenities, and architectural quality as trade-offs

for any necessary deviations.

- C. The applicant is not specific about constraints posed by the current zoning district – more generally, the narrative notes that the applicant is seeking a flexible mix of commercial uses to support the introduction of more pedestrian amenities and a new layout. No particular dimensional or site standard is noted.
- D. The fundamental use of the site will not change; service loads on Orchard Lake are unlikely to change; there is some potential to close at least one or two curb cuts, though the conceptual plan does not show this.
- E. The Planned Unit Development must meet, at a minimum, one of the 8 objectives of the City. The applicant’s narrative indicated they think they meet the following:

- i. To permanently preserve open space or natural features because of their exceptional characteristics or because they can provide a permanent transition or buffer between land uses.

The applicant points to an expansion of green space along Orchard Lake Road; there may be additional opportunities to return paved areas to an unpaved state, or establish new gathering areas, visual focal points (such as public art pieces), and sidewalks to aid the flow of people through the large parking lot.

- ii. To permanently establish land use patterns which are compatible or which will protect existing or planned uses.

The applicant has highlighted and responded to this objective, referring to a more accessible overall center with better stormwater management and a more dynamic tenant mix. The conceptual plan does also show some of the buildings moving away from the residential property line to establish larger loading zones.

- v. To guarantee the provision of a public improvement which could not otherwise be required that would further the public health, safety, or welfare, protect existing or future uses from the impact of a proposed use, or alleviate an existing or potential problem relating to public facilities.

The applicant notes enhanced non-motorized access and new green spaces; see the consultant’s response to objective i above; the Planning Commission should discuss what these amenities and spaces might actually look like when the applicant returns for final PUD.

- vi. To promote the goals and objectives of the Master Plan for Land Use

The applicant addresses this item with notes about business stabilization.

- vii. To foster the aesthetic appearance of the city through quality building design and site development, the provision of trees and landscaping beyond minimum requirements; the preservation of unique and/or historic sites or structures; and the provision of open space or other desirable features of a site beyond minimum requirements.

The applicant calls for higher-end design, architecture, and materials on the site, and also notes that the outlots help foster walkability and change the feel of the corridor; the new greenspace and plazas between these buildings are also highlighted in the applicant’s response.

Regarding the conceptual site plan, Planning Consultant Tangari made the following points:

- The applicant has submitted a conceptual drawing showing four new outlots, a major rebuilding of three large tenant spaces, and some other tenant spaces. Much greater site analysis and detail will be required to help the Planning Commission and City Council reach a final determination.
- Uses must be compatible with Shopping Center Type Business uses.
- Dimensional standards have not yet been provided.
- A full parking analysis will be necessary.
- Relief sought from ordinance standards included:
 - a. Permit drive-through use/potentially other uses not listed in B-2.
 - b. B-2 front setback.
 - c. Possibly others to be noted at final PUD determination.

In response to questions, Planning Consultant Tangari, explained that McDonalds was a separate site, with its own PUD agreement.

Dave Ortner, Senior Vice President of RPT Realty, was present this evening along with members of the development team. Mr. Ortner made the following points:

- The applicants were pursuing a PUD in order to provide for more comprehensive redevelopment and also attract a more dynamic mix of tenants and uses to the site.
- There were significant vacancies on the site due to changes in the retail industry.
- The intent was to maintain this site as a regional retail shopping center, by redeveloping in a way that will attract new tenants and create a more usable, realistic layout, better parking, and enhance walkability on the site.
- Everything north of what is currently the SAKS building will be new construction.

Commission discussion:

- Some City Council members want this area to work in tandem with West Bloomfield to offer a restaurant corridor.
- It was important for the applicants to include what type of uses will be in the shopping center, in terms of the PUD agreement and site plan approvals. This was also important in terms of what opportunities the developers wanted to create for this site.
- In future discussions, the applicants needed to justify why this development needed a PUD rather than being redeveloped under straight rezoning. Every component of the plan needs to be tied back to the request for a PUD.

City Planner Perdonik noted that a PUD was the most efficient land use tool to use in this situation.

- In future discussion, the applicants should be prepared to discuss more completely how they met the criterion in the PUD ordinance, and the five objectives listed above. How was the project providing enhanced green space? How many acres are being affected? Where were the exceptional characteristics of this site? How was this project protecting existing land uses? How will stormwater be improved as a result of this PUD? All elements of the request need be reviewed and explained.
- In response to questions, Mr. Ortner said that they were reconfiguring the site, providing landscaping along Orchard Lake Road and pulling the outlots closer to the road. Outlots D, E, and F were meant to create walkability, with pedestrian pauses between the uses.
- Mr. Ortner said that preserving the use by creating new designs in redevelopment will attract credit worthy and stable tenancy, which will maintain this shopping center as a reactivated regional retail center.

MOTION by Brickner, support by Varga, that the Planning Commission makes a preliminary finding that PUD 2, 2023, received July 18, 2023, submitted by Timothy Collier, qualifies for the Planned Unit Development Option under Section 34-3.20.2.A through D. of the Zoning Chapter. It is further determined that the proposal meets at least one of the objectives as outlined in Section 34-3.20.2.E.i. thru viii., specifically objectives i, ii, v, vi, vii., and that it be made clear to the applicant that final granting of the PUD plan and contract requires approval by City Council, after

recommendation by the Planning Commission.

Motion discussion:

- Commissioner Mantey said it was important that the Commission and the City ask for appropriate public benefits. For instance, a covered bus stop would be a public benefit, and should be included in the PUD approval.
- Commissioner Stimson said he would support the motion, but he was also struggling with why this application was coming forward as a PUD and not as a straight zoning development. He would need to hear more about why this is a PUD request.
- Commissioner Brickner spoke to the need to revitalize this shopping center. He understand why the request was being made for a PUD, which would allow more flexibility, in terms of allowing a drive-thru restaurant, putting restaurants close to Orchard Lake Road, and so on.

Motion passed unanimously by voice vote.

APPROVAL OF MINUTES

July 20, 2023, Special Meeting, and July 20, 2023, Regular Meeting

MOTION by Brickner, support by Ware, to approve the July 15, 2023 Special Meeting minutes and the July 15, 2023 Regular Meeting minutes as submitted.

Motion carried unanimously by voice vote.

PUBLIC COMMENT

None.

ADJOURNMENT

Motion by Grant, support by Ware, to adjourn the meeting at 11:00pm.

Motion carried unanimously by voice vote.

Respectfully Submitted,
Marisa Varga
Planning Commission Secretary
/cem

**MINUTES
CITY OF FARMINGTON HILLS
PLANNING COMMISSION MEETING
31555 W ELEVEN MILE ROAD
FARMINGTON HILLS, MICHIGAN
October 26, 2023, 7:30 P.M.**

CALL MEETING TO ORDER

The Planning Commission Regular Meeting was called to order by Chair Countegan at 7:30 p.m.

ROLL CALL

Commissioners present: Aspinall, Brickner, Countegan, Grant, Mantey, Trafelet, Stimson, Varga, Ware

Commissioners Absent: None

Others Present: City Planner Perdonik, City Attorney Schultz, Planning Consultant Bahm

APPROVAL OF THE AGENDA

MOTION by Brickner, support by Trafelet, to approve the agenda as published.

Motion passed unanimously by voice vote.

REGULAR MEETING

A. PLANNED UNIT DEVELOPMENT 2, 2023, INCLUDING SITE PLAN 67-9-2023

LOCATION: 30825 and 31361 Orchard Lake Road
PARCEL I.D.: 22-23-03-226-027 and 028
PROPOSAL: Redevelopment of shopping center in B-2, Community Business zoning district
ACTION REQUESTED: Set for public hearing
APPLICANT: Timothy Collier
OWNER: RPT Realty, L.P.

Planner's Review

Referencing the October 18, 2023 Giffels Webster memorandum, Planning Consultant Bahm gave the background and review for this request for a Planned Unit Development including site plan, for the redevelopment of the existing Hunters Square Shopping Center.

The applicants were seeking to reconfigure the site, including demolition of part of the shopping center and adding outlot buildings.

Requested relief from ordinance standards included:

- a. Setback relief for front setback (reduction from 75' to 58.49')
- b. Setback relief for rear setback (reduction from 75' to 69.34')
- c. Pedestrian connections between ROW and Principal Building for Outlot G
- d. Drive-in restaurant use (currently not permitted in B-2 Zoning District)
- e. Stacking/waiting lane for drive-in in front yard
- f. Excess lighting adjacent to residential
- g. Insufficient loading spaces

h. Dumpster enclosure located in front yard setback

A lot split request was also associated with this PUD request, and the lot split review was included in tonight's packet. The lot split could only be approved if exceptions from side setback requirements are granted by the Planning Commission as part of the PUD application review.

The purpose of the lot split is to allow consideration of a drive-in business as a part of the PUD request. There were deficiencies associated with the drive in as outlined on page 2 of the PUD review.

Applicant presentation

David Ortner, RPT Realty, was present on behalf of this request for PUD approval. Applicant engineer Rachel Smith, PEA Group, was also present, as was architect Mark Drane, Colliers. The team was available to answer questions.

Commission questions and discussion:

- Commissioner Trafelet hesitated to consider allowing a deviation for stacking at the drive through; this should be discussed further at the public hearing. He was also not comfortable with the dumpsters facing Orchard Lake Road.
- Commissioner Mantey said that he would be looking for more of an open space buffer around all front yard parking areas, as suggested in the planner's review. This was something that would affect the greater Farmington Hills community, and needed to be done right. He was also concerned about how the lighting would affect the neighboring properties.
- Commissioner Mantey emphasized that the Commission needed a reason to approve a PUD development. Significant community benefits should be offered, such as a covered bus stop.

City Planner Perdonik said that staff had suggested constructing a bus stop, only to discover that SMART was discontinuing the route to this location. Perhaps the requirement could be to construct a bus stop, should the bus route resume.

- The Commission expressed concern over the loss of Marvelous Marvin's, which is a unique historical destination in the City.

Mr. Ortner said they were open to discussing another location with the owners of Marvelous Marvin's.

- In response to questions, Mr. Ortner provided the following further information:
 - Everything from Marvelous Marvin's through what is currently Bed Bath and Beyond will be demolished.
 - They were actively negotiating with Meijer to put a "small Meijer" in this location.
 - The outbuildings will provide for a mix of tenants, including a drive through restaurant and soft goods retail.
 - The applicants were in active lease negotiations with tenants, contingent on approvals going through.
 - Retailers are adjusting to the current climate; brick and mortar retail stores are not going away.
 - The lot split will allow for flexibility of development. They were not planning on selling portions of the shopping center. In any event there will be cross access agreements so that the center will continue to function as a unified center as it does today.

After further discussion of the proposed changes to the shopping center, the following motion was offered:

MOTION by Brickner, support by Aspinall, that Planned Unit Development 2, 2023, including Site Plan 67-9-2023, both dated September 15, 2023, submitted by Timothy Collier, be set for public hearing on November 16, 2023.

Motion passed unanimously by voice vote.

APPROVAL OF MINUTES **September 21, 2023, Special Meeting (master plan study),
September 21, 2023, Special Meeting (PUD 1, 2023 study), and
September 21, 2023, Regular Meeting**

MOTION by Mantey, support by Trafelet, to approve the minutes of the September 21, 2023 Special Meeting (master plan study), Special Meeting (PUD 1, 2023 study), and Regular Meeting, as submitted.

Motion carried unanimously by voice vote.

PUBLIC COMMENT

None.

COMMISSIONER/STAFF COMMENTS

ADJOURNMENT

Motion by Mantey, support by Ware, to adjourn the meeting at 9:15pm.

Motion carried unanimously by voice vote.

Respectfully Submitted,
Marisa Varga
Planning Commission Secretary

/cem

**MINUTES
CITY OF FARMINGTON HILLS
PLANNING COMMISSION MEETING
31555 W ELEVEN MILE ROAD
FARMINGTON HILLS, MICHIGAN
NOVEMBER 16, 2023, 7:30 P.M.**

CALL MEETING TO ORDER

The Planning Commission Regular Meeting was called to order by Chair Countegan at 7:34 p.m.

ROLL CALL

Commissioners present: Aspinall, Brickner, Countegan, Grant, Mantey, Trafelet, Stimson, Varga, Ware

Commissioners Absent: None

Others Present: City Planner Perdonik, City Attorney Schultz, Planning Consultants Tangari and Upfal, Director of Planning and Community Development Kettler-Schmult

APPROVAL OF THE AGENDA

MOTION by Brickner, support by Trafelet, to approve the agenda as published.

Motion passed unanimously by voice vote.

PUBLIC HEARING

A. PLANNED UNIT DEVELOPMENT 2, 2023, INCLUDING SITE PLAN 67-9-2023

LOCATION: 30825 and 31361 Orchard Lake Road
PARCEL I.D.: 22-23-03-226-027 and 028
PROPOSAL: Redevelopment of shopping center in B-2, Community Business zoning district
ACTION REQUESTED: Recommendation to City Council
APPLICANT: Timothy Collier
OWNER: RPT Realty, L.P.

Applicant presentation

David Ortner, RPT Realty, was present on behalf of this request for a recommendation to City Council for planned unit development and site plan. Mark Drane, Rogvov Architects, was also present.

Mr. Ortner provided the following information:

- The Meijer proposed for this location is a smaller grocer format, will be located on the north end of the project, and will not impede Marvin's Marvelous Mechanical Museum ("Marvin's"). The two locations are unrelated.
- Hunters Square and RPT Realty have a long standing relationship with Marvin's, and they are in regular communication with Marvin's. RPT Realty will work with Marvin's and look for opportunities to maintain their presence at the center, whether in their current space or in a

different space. RPT has worked with Marvin's as a landlord and partner through a number of setbacks, and appreciates Marvin's presence at the center.

- Hunters Square has almost 200,000sf of vacant space. A major tenant had relocated across Orchard Lake Road because the current building was obsolete; the buildings do not work on a going-forward basis.
- RPT wants to be responsible owners, landlords, and community members, as it develops a center that works into the future and does not turn into a blight in the community.
- RPT is making a significant investment in the center, trying to be thoughtful as it puts together a plan that the city will be proud of that will add to the community. RPT will work with all their tenants including Marvin's, to find a home for those who want to stay and to make sure the redevelopment is a net positive for the community.
- The redevelopment/modernization of Hunters Square includes four new development "boxes", one of which will be the proposed Meijer. There will be four new outlots as well. The plan creates additional walkability, a restaurant, a quick service restaurant, and new retail opportunities.

In response to questions, Mr. Ortner and Mr. Drane provided the following information:

- The lot split had been removed from the plan.
- The architecture will be four sided, with all entrances to the outlot buildings on the parking lot side (west side). However, the buildings will be designed to look like front of building facing Orchard Lake Road, including signs, canopies, and decorative building materials.
- Headlights of cars using the drive-through will be shielded by screen walls, hedgerows, and decorative fencing. These elements will also buffer the sidewalk.

Planner's review

Referencing the October 18, 2023 Giffels Webster review, Planning Consultant Tangari highlighted the following:

- The 29-acre Hunter's Square Shopping Center is located in a B-2 Community Business District at the southwest corner of 14 Mile and Orchard Lake Road. The developer is seeking a planned unit development.
- Properties to the north and east, including in West Bloomfield, are zoned commercial. Properties to the south and west have RC-2 multi-family zoning.
- The applicant is seeking to reconfigure a good portion of the existing shopping center, and rebuild some of the large tenant spaces, as well as add some outlots along Orchard Lake Road. The use will remain commercial.
- On August 17, 2023, the Planning Commission had found that the project qualified for a PUD development under Section 34-3.20.2, citing criteria i, ii, v, vi, and viii under Section 34-3.20.2.E. of the Zoning Ordinance.
- A set of criteria – items to be submitted – applied relative to final determination of a planned unit development. All required items had been submitted.

Regarding the site plan:

- Four new major tenant spaces were proposed at approximately 157,000 total square feet, with an additional four new outlots (one of which includes a drive-through).
- Four existing buildings on the site will be maintained in their current location. The total floor area of the site with all the proposed improvements will be about 353,000sf. Most of the uses are consistent with B-2 uses, and almost all zoning standards are met. The shopping center complies with the 2009 Future Land Use Map and Master Plan.

- The 2006 sustainability study included a recommendation to establish a mixed use development area along Orchard Lake between 11 and 14 Mile Roads, with an emphasis on the potential to create a central business district characterized by buildings up to five stories with zero setbacks from the roads.
- The 2002 Orchard Lake Road Corridor Study was a coordinated plan for the area along Orchard Lake extending from Grand River Avenue to Pontiac. The Master Plan cites a quote from the Corridor Study that recommends a coordinated approach to development with common visual linkages, landscaping, traffic calming, pedestrian friendly ways, lighting and signage.
- As part of this PUD request, the applicants are seeking the following relief from ordinance standards:
 - a. Setback relief for front setback (reduction from 75' to 58.49')
 - b. Setback relief for rear setback (reduction from 75' to 69.34')
 - c. Pedestrian connections between ROW and Principal Building for Outlot G
 - d. Drive-in restaurant use (currently not permitted in B-2 Zoning District)
 - e. Stacking/waiting lane for drive-in in front yard
 - f. Excess lighting adjacent to residential
 - g. Insufficient loading spaces
 - h. Dumpster enclosure located in front yard setback
- The Planning Commission might want to discuss whether additional open space should be provided to buffer all the front yard parking areas, whether existing or proposed, and whether additional landscaped areas should be included along Hunters Lane and Orchard Lake Road at the southeast corner of the site.
- Regarding pedestrian access, the project adds significant pedestrian cross connectivity across the site. However, there should be more pedestrian markings on the plan. No pedestrian access or markings across drive-through lanes were provided.
- The lighting plan shows a temperature between 4000k-5000k; a lower color temperature might provide a less intense feeling. Lighting also needs to be adjusted to meet the 1/3 of a footcandle standard along the rear property line.

In response to questions, Planning Consultant Tangari provided the following:

- The applicants have offered what they consider to be additional landscaping and open space. The Planning Commission needs to assess whether or not what the applicant is offering is a fair trade off with the relief that they're requesting from the ordinance.

Commission discussion:

- Bicycle parking should be added to the plan.
- The Commission had discussed the need for common gathering areas on this site, but this was not shown on the plans. Planning Consultant Tangari said this situation should be discussed with the applicant.
- Commissioner Brickner pointed out that the Planning Commission reviews plans for compliance with the zoning ordinance. In the case of a planned unit development, the Planning Commission evaluates whether the plans fit the character of the neighborhood, among other things. The Planning Commission does not make decisions as to tenants nor does it have control on who a landlord wants to have in their in their complex. Such components cannot be used to make a determination regarding zoning compliance.

Public comment

63 letters had been received relative to this proposal, specifically in opposition to demolition of Marvin's Marvelous Mechanical Museum, and also to share concerns regarding traffic.

Chair Countegan opened the public hearing at approximately 8:15pm.

65 members of the public spoke regarding this PUD request, with most comments focusing on how the proposed redevelopment of Hunter's Square Shopping Center will impact the future of Marvin's Marvelous Mechanical Museum, which was located within the area of the shopping center that was proposed for demolition. Speakers were concerned about the added traffic a Meijer store might bring to this location. 24 speakers said they lived in Farmington Hills. Other speakers did not give an address, often saying they grew up in the City, had family in Farmington Hills, or that they lived elsewhere. Some speakers said they had been visiting Marvin's since the site was Tally Hall.

Comments regarding Marvin's included:

- Friends from across the country and international visitors state the first place they want to visit is Marvin's Marvelous Mechanical Museum. Marvin's is a unique destination that brings people to the city from all over the world.
- In just 48 hours, a change.org petition drew 27,000 signatures supporting Marvin's.
- Marvin's was written about in various publications, including *Atlas Obscura*, *Curious and Wondrous Travel Destinations*, and other publications. An area blogger that hosts international travelers always takes people to Marvin's, at their request.
- The Ann Arbor rock band Tally Hall's first album was titled *Marvin's Marvelous Mechanical Museum*.
- Many people commented that Marvin's was a "safe place" for them when younger, and today. Marvin's provided several speakers with their first jobs. Commentors referred to Marvin's as magical, unique, one of a kind, culturally significant, a place to have birthday parties, to take dates, a community/historical icon, a whacky place, brings people of all demographics together, life-changing, the opposite of the "epidemic of sameness" and "placelessness", a favorite place, a destination gem, the heart of Farmington Hills, supported by a wide community as evidenced by a successful Go Fund Me effort during the pandemic, an accessible place that welcomes everyone, including groups from Community Work Opportunities in Canton, and student tours from Farmington Public Schools.
- Speakers spoke of going to Marvin's when they were children, then later with adult friends, and then with their own children and grandchildren. Marvin's is an inter-generational attraction.
- Several speakers pointed out that no one had memories of shopping in generic big box stores, but Marvin's creates memories that last a lifetime. Marvin's was a century old, had remained through difficult times, including recessions and COVID, and was the last remaining business from Tally Hall.
- One speaker noted she had written her college admission essay about Marvin's.

Concerns regarding traffic and other issues included:

- A Meijer would bring additional intense traffic to an area where traffic was already difficult.
- Had traffic/noise/parking studies been done?
- There were several grocery stores already in this area; another one was not needed.
- Several speakers wondered why Meijer couldn't be located in the old Sam's Club location on Northwestern Highway.
- Several speakers also supported the renovation of this shopping space, noting that an update was badly needed. Several spoke of the way Meijer supported its local communities and the state, and pointed to the Frederick Meijer Gardens and Sculpture Park in Grand Rapids.
- Other commentors spoke of their desire for smaller, locally owned businesses.

- The rear docks at Meijer meant more semi-trucks/trailers will need to access this site.
- The redevelopment represented a car-centric design, thereby prioritizing people with money for cars. People are trapped unless they have a car. Empathetic and intelligent planning will solve this and similar issues.
- Several people did not support having a drive-through restaurant at this location.

In response to comments, Chair Countegan explained that that planning commissioners were appointed volunteers from the community. When proposals came to the Commission, the Commission heard them. Commissioners had taken an oath to uphold the laws of the community, the state, and the constitution. Some commissioners had also been to Marvin's, and had their own histories there. There was obviously wide community support for this business, and the Commission appreciated the passion people brought to the meeting tonight. However, the Commission needed to base its decisions on the City's Master Plan, and had to follow due process as outlined in the Zoning Ordinance. It was also important to note that the relationship between Marvin's as a tenant and the shopping center as a landlord was a contractual relationship between two private parties, and the City was not part of that contractual relationship.

Chair Countegan closed the public hearing at approximately 10:27 pm.

Chair Countegan invited Mr. Ortner to respond to public comment.

Mr. Ortner said he was "blown away" and appreciated the community's commitment and support of Marvin's. Several people had talked about Marvin's surviving the great recession and the pandemic; this survival was due in no small part to the shopping center's partnership and commitment to Marvin's in the past. RPT will continue to work with Marvin's and any of the impacted tenants in the shopping center who want to move forward. RPT was not closing Marvin's down, and will continue to work with them.

In response to other comments, Mr. Ortner said traffic, parking, and noise studies had been done, and would be presented as the process for the redevelopment of the shopping center moved forward.

Commissioner Ware pointed out that previous tenants of the shopping center had been national names and big box stores. If all the current spaces were filled, or if all the new spaces would be filled, including the small Meijer, the traffic and noise would be the same. The redevelopment of Hunter's Square would not make things worse than if every storefront were filled now.

Mr. Ortner said that was correct. The planned uses so far were the Meijer, the drive-through use, a large form entertainment space, a veterinary and medical space, and others, all totaling about the same square footage as currently exists.

Mr. Ortner emphasized that the new Meijer will not impact Marvin's; it would be located on the opposite end of the redeveloped portion of the center. He wanted to make sure the community was not confused – Meijer was not displacing Marvin's.

In response to a question from Commissioner Brickner, Mr. Ortner said they would work with all their existing tenants, including Marvin's, who wanted to stay within the shopping center complex. RPT was not closing them down or throwing them out.

Chair Countegan summarized that the proposal before the Commission for a planned unit development represented an investment in Farmington Hills relative to a shopping center that had significant vacancies. The owner's representative was stating that they were in discussion with all the tenants that were located in the portion of the center that will be redeveloped. The Planning Commission is considering a PUD plan that has B-2 as the underlying zoning, with some B-3 implications in order to allow a drive-through and some setback deviations.

Tonight's public comments had been very informative and passionate in support of a long-standing business with a long history in the community.

On the positive side, the proposal represented an owner who is making a significant investment in the Farmington Hills community, thereby strengthening the tax base that supports schools, public services, and law enforcement.

Commissioner Grant supported Marvin's remaining in its current location. Was this possible?

Mr. Ortner said he did not know the answer to that question tonight.

In response to questions, City Attorney Schultz explained that:

- The Planning Commission does not have authority to stipulate a contractual condition of tenancy between an owner and a tenant.
- The Planning Commission is looking at the physical redevelopment of a property, and is not considering use, whether that be a grocery store or an amusement center.

For the benefit of the audience present, Chair Countegan further explained that the job of a planner is to evaluate proposals for conformance with what is allowed in the zoning ordinance. Private businesses make arrangements among themselves.

Commissioner Brickner pointed out that as part of an approving motion, the Commission is required to list components of the project that meet requirements of the PUD ordinance, and to add any conditions attached to the approval.

Commissioner Ware asked if RPT was willing to include a common area within the redevelopment so that people could congregate. Walkable areas were important to the city, not just to allow people to walk, but to also to provide spaces to sit and enjoy being with friends in the outdoors.

Mr. Ortner said they were increasing walkability and connectivity throughout the site, including connecting the sidewalk along Orchard Lake Road to the plazas between the outparcels, where there will be potential locations for public art and seating, as well as restaurant and retail spaces. The areas between the outparcels will be common space, not owned by the tenant. Some restaurant tenants might want outdoor space and that would also be accommodated, but the common areas are meant to be areas for the use of all visitors.

In response to a question, City Planner Perdonik explained that outdoor seating in this instance would be a permitted accessory use, as long as there is a building between the outdoor seating and the nearby residential.

In response to a question, Mr. Ortner said Marvin's currently occupied an approximate 5300sf space.

After discussion and amendment, the following motion was offered:

MOTION by Brickner, support by Trafelet, to recommend to City Council that Planned Unit Development 2, 2023, including Site Plan 67-9-2023, both dated September 15, 2023, submitted by Timothy Collier, be approved because the plans are consistent with the goals, objectives, and policies of the Master Plan, and applicable provisions of the Planned Unit Development Option in Section 34-3-20 of the Zoning Ordinance, specifically sections i, ii, v, vi, and vii, subject to the following findings and conditions:

Finding:

The Commission has no objections to the proposed property lines which will be addressed in conjunction with the final PUD Plan and the PUD Agreement.

Conditions:

- 1. Modifications of the Zoning Ordinance as indicated on the proposed plan.**
- 2. That the proponent clarify the distance between the order window and the pickup window in Outlot G, as recommended by Giffels Webster in their review.**
- 3. Compliance with ordinance stacking requirements.**
- 4. That there be less intense lighting as recommended by Giffels Webster, and that the lighting near the residential area be downlit and shielded.**
- 5. That there be a hedge along Orchard Lake Road near the drive-through restaurant to block headlights along Orchard Lake Road.**
- 6. That there be bicycle racks placed as a part of the plan.**
- 7. That all recommendations included in the October 18, 2023 Giffels Webster review letter be included in the PUD agreement, and all outstanding issues as listed in the October 18 letter be resolved.**

Motion discussion:

- Mr. Ortner was on record as saying RPT is still in discussion with Marvin's and with other existing tenants. No lease has been terminated.
- Commissioner Mantey wished there was a way to formally recognize Marvin's as a culturally valuable asset to the City, but what he was hearing was that this desire cannot affect the Planning Commission's decision regarding this proposed PUD and site plan.
- Chair Countegan agreed. The Planning Commission could not dictate the landlord's relationship with Marvin's via the PUD process, as everyone should now be aware.

Motion passed unanimously by voice vote.

Chair Countegan thanked everyone who came tonight. This proposal will next go to City Council.

Chair Countegan called a brief break at approximately 10:40pm and reconvened the meeting at approximately 10:55pm.

REGULAR MEETING

PUBLIC COMMENT

None.

COMMISSIONER/STAFF COMMENTS

Commissioner Brickner noted that the cargo containers and portable toilets were still on the property at Drake and Grand River.

ADJOURNMENT

Motion by Grant, support by Trafelet, to adjourn the meeting at 11:57pm.

Motion carried unanimously by voice vote.


Respectfully Submitted,
Marisa Varga
Planning Commission Secretary

/cem



DEPARTMENT OF PUBLIC SERVICES
KAREN MONDORA, P.E., DIRECTOR

INTEROFFICE CORRESPONDENCE

DATE: December 18, 2023
TO: Erik Perdonik, Planning & Community Development
FROM: James Cubera, Engineering 
SUBJECT: Hunter's Square Redevelopment
Revised PUD Plan 2, 2023
PJ#: 3-23-77
SP #67-9-2023
22-23-03-226-027 & 028

This office has performed a preliminary review of the above referenced revised PUD plans submitted to the Planning Department on December 7, 2023. Our preliminary comments are as follows.

1. We note that the submittal letter indicates that the proposed lot splits are no longer being sought. The need to provide public watermain and public sewer to the resultant parcels is therefore no longer in effect. The preliminary layout is identified on Sheet C7.0. Conceptionally, it appears appropriate for the water main and sanitary sewer requirements for this development. This will be further addressed and reviewed during construction review.
2. As per our Planning Commission memo dated October 12, 2023, the proponent will be obligated to confirm that the existing detention and restriction system is operating properly. An engineering assessment of its current conditions must be provided. Any repairs or necessary cleanout must be attended to and it must be verified as meeting City requirements for this site. In addition, as we stated previously, the City recently adopted the Oakland County Storm Water Quality Standards and with this renovation, the proponent will be obligated to comply with this as well. This will necessitate additional storm detention and storm water quality improvements.

We note that the proponent's cover letter with this resubmittal indicates that we have had discussions with proponents' engineers as to what the engineering requirements for detention restriction and storm water quality will be.



DEPARTMENT OF PUBLIC SERVICES
KAREN MONDORA, P.E., DIRECTOR

Their plan sheet C-12.0 identifies detention and restriction. This will be further reviewed during construction review.

3. We previously requested a traffic impact statement/study. This has been provided. Our City Traffic Engineer is working with our consultant on providing a review of this document. The review and results will be provided under separate cover.
4. This redevelopment includes significant site work. Although it has not been identified, if utility vacations are necessary, they will require City Council action prior to any construction approval.



INTEROFFICE CORRESPONDENCE

TO: Planning Commission

FROM: Jason Balog, Fire Marshal

DATE: December 12, 2023

RE: Revised PUD 2-2023 & Site Plan 67-9-2023 (Hunter's Square Development)

The Fire Department has no objection to approval contingent upon compliance with the following requirements:

1. Hydrant coverage shall be in accordance with Chapter 12 Section 12-11(2) of the City Code. The proponent and Fire Marshal discussed that any adjustments could occur during Engineering review.
2. Entire site, including delivery routes must be designed to accommodate fire apparatus with a fifty-foot turning radius in accordance with Chapter 12, Section 12-11(1){a} of the City Code. The proponent has committed to providing further clarification.
3. The minimum clearance between the finished roadway surface and any overhang shall be 13'6".
4. The building(s) and site shall be maintained in accordance with minimum Fire Prevention Code requirements.

A handwritten signature in blue ink, appearing to read "Jason Baloga".

Jason Baloga, Fire Marshal

**CITY OF FARMINGTON HILLS
CITY COUNCIL PUBLIC HEARING NOTICE**

www.fhgov.com

DATE: January 8, 2024
TIME: 7:30 P.M.
PLACE: Farmington Hills City Hall, City Council Chambers
31555 Eleven Mile Rd., Farmington Hills, MI 48336
ITEM: Planned Unit Development 2, 2023,
Including Site Plan 67-9-2023

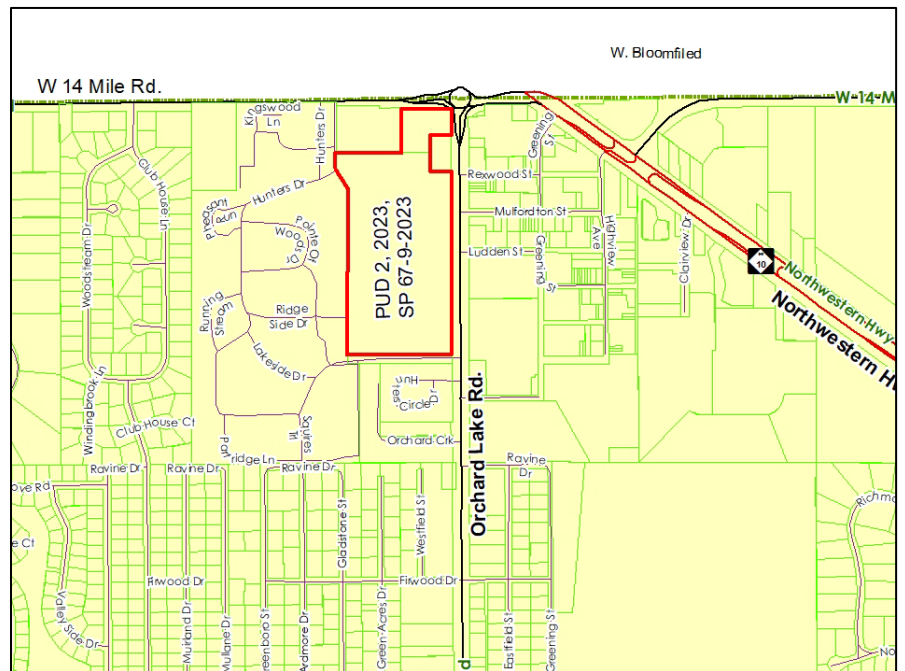
The City of Farmington Hills City Council shall consider two (2) applications, both received by the Planning Office on September 15, 2023, submitted by Timothy Collier, RPT Realty, L.P., for approval of a Planned Unit Development (“PUD”) and Site Plan to redevelop a shopping center in the B-2, Community Business zoning district.

The subject property is located at 30825 and 31361 Orchard Lake Road on the west side of Orchard Lake Road between Thirteen Mile and Fourteen Mile Roads and being more particularly described as: Parcel Identification Numbers: 22-23-03-226-027 and 028, City of Farmington Hills, Oakland County, Michigan.

Any interested person is invited to comment on the request in person at the scheduled hearing, or written comments may be submitted to the City of Farmington Hills Planning Office at 31555 W. Eleven Mile Road, Farmington Hills, Michigan 48336, or ckettler@fhgov.com, prior to the hearing. The application may be reviewed at the Planning Office between 8:30 a.m. and 4:30 p.m., Monday through Friday.

Charmaine Kettler-Schmult
Director of Planning &
Community Development

Phone: (248) 871-2540
Email: ckettler@fhgov.com
Publish: December 22, 2023



Procedures for accommodations for persons with disabilities:

The City will be following its normal procedures for accommodation of persons with disabilities. Those individuals needing accommodations for effective participation in this meeting should contact the City Clerk (248) 871-2410 at least two (2) working days in advance of the meeting. An attempt will be made to make reasonable accommodations.



INTEROFFICE CORRESPONDENCE

DATE: January 8, 2024
TO: City Council
FROM: Theresa Rich, Mayor
SUBJECT: Recommendation for appointment.

I would like to recommend the following appointments at the January 8, 2024 City Council meeting.

Master Plan Steering Committee

Mayor Theresa Rich & Randy Bruce

Council Liaisons for Boards & Commissions

Mayor Theresa Rich	Board of Review, Grand River Corridor Improvement Authority, Mayors Youth Council.
Mayor Pro Tem Michael Bridges	Farmington Area Commission on Aging, Economic Development Corporation.
Valerie Knol	Beautification Commission, Historical Commission, Historic District Commission.
Jackie Boleware	Brownfield Redevelopment Authority, Commission on Community Health, Committee to Increase Voter Participation, Farmington Area Arts Commission.
William Dwyer	Commission on Children, Youth & Families, Retirement Board.
Jon Aldred	Emergency Preparedness Commission, Parks & Recreation Commission.

Committee to Increase Voter Participation

Catherine Brown	Length of Term: 3 years	Term ending: February 1, 2027
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Catherine will fill the vacancy left by Marc Thomas who resigned. Attached, please find Catherine's resume.

Adam Harrell	Length of Term: 3 years	Term ending: February 1, 2027
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Adam will fill the vacancy left by Ashley Riley who resigned. Attached, please find Adam's resume.

Thomas Engle	Length of Term: 3 years	Term ending: February 1, 2027
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Thomas will fill the vacancy left by Deb Kendzierski who resigned. Attached, please find Thomas resume.

Beautification Commission

Dr. Robert Levine	Length of Term: 3 years	Term ending: February 1, 2027
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Dr. Levine will fill the alternate vacancy. Attached, please find Dr. Levine's resume.

Grand River Corridor Improvement Authority

Jerry Price	Length of Term: 4 years	Term ending: February 1, 2028
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Jerry will fill the vacancy left by Michael Gendjar. Attached, please find Jerry's resume.

Zoning Board of Appeals

Lloyd Banks	Length of Term: Unexpired Term	Term ending: February 1, 2026
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Lloyd will fill the alternate vacancy left by Asim Khan. Attached, please find Lloyd's resume.

Commission on Aging

Vivek Das	Length of Term: 3 years	Term ending: February 1, 2027
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Vivek will fill the alternate vacancy left by Karina Weglarz. Attached, please find Vivek's resume.

Commission on Community Health

Ikramuddin Khan

Length of Term:
3 years

Term ending:
February 1, 2027

Ikramuddin will fill the vacancy left by La Keshia Young who resigned in 2022. Attached, please find Ikramuddin's resume.

Danielle King

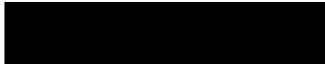
From: postmaster@muniweb.com
Sent: Friday, September 15, 2023 4:29 PM
To: CMO
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

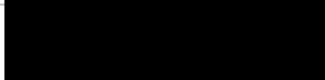
Submittor's Name

Catherine Brown

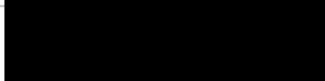
Submittor's Address



Submittor's Phone



Submittor's Email



Interest(s) Brownfield Redevelopment Authority|Commission on Community Health|Economic Development Corporation|Housing Rehabilitation Loan Board|Multicultural/Multiracial Community Council|Parks Recreation Commission|Planning Commission|Zoning Board of Appeals

Resume

Most of my professional experience is working in homeless services and housing development, particularly low income housing. I currently work for the Corporation for Supportive Housing which provides lending and technical assistance to affordable housing and supportive housing developers.

References

9/15/2023
4:27:24 PM

Danielle King

From: postmaster@muniweb.com
Sent: Friday, December 8, 2023 2:02 PM
To: DistributionList-CityManagerOffice
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Submittor's Name	Adam Harrell
Submittor's Address	[REDACTED]
Submittor's Phone	[REDACTED]
Submittor's Email	[REDACTED]
Interest(s)	Committee to Increase Voter Participation

Resume

Adam Jason Harrell [REDACTED] Cell: [REDACTED] Email: [REDACTED]
Objective: Goal-oriented Customer Service Professional dedicated to high levels of customer satisfaction and meeting aggressive business goals. Friendly, enthusiastic, and able to learn quickly and proficiently while growing key customer relationships. Always represent establishment with friendly and professional demeanor. Work Experience Supervisor of Cure Department 8/2022- Present • Ensure effective and appropriate communication with team members to increase team unity and operative flow. • Supporting the Manager of Cure to guide and train Cure department members to organize and process the dried cannabis product to be available promptly for the Packaging department. • Maintain thorough, consistent, and professional communication with other Processing and Production sub-departments as necessary to achieve company objectives. • Support the Manager of Cure with training, coaching, supervision, and disciplining appropriately within the department. Be prepared to communicate, document, and report towards employees' performance, behavior, and attendance. • Be prepared to change technique, training, and instruction based on variables, R&D codes, and strain attributes. Shri Thanedar 4/2022 – 8/2022 Assistant Campaign Director Responsible for Payroll Conduct interviews and handling recruiting Also handle training, hiring team leads. Attending meeting as well as fund raising campaign event for potential voters. Fields Works 04/2022 – 08/2022 Team Leader • Recruit, train, and manage • Collaborate with senior staff to set and accomplish campaign goals. • Talking to voters • Helps progressive organizations, advocacy groups, and members • Finding solutions to problems, and hitting goals • Run reports daily

References

Melvin Henry- [REDACTED] Former mentor Chris Allen- [REDACTED] Former coworker Nora Matti - [REDACTED] Former coworker

12/8/2023 2:01:35 PM

Danielle King

From: postmaster@muniweb.com
Sent: Friday, April 7, 2023 1:49 PM
To: CMO
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Submittor's Name	Thomas Engle
Submittor's Address	[REDACTED]
Submittor's Phone	[REDACTED]
Submittor's Email	[REDACTED]
Interest(s)	Commission on Community Health Committee to Increase Voter Participation

Resume

THOMAS W. ENGLE, PA-C [REDACTED] | [REDACTED] | [REDACTED]

SUMMARY Versatile medical provider with extensive experience providing acute inpatient care throughout multiple hospitals of varying acuity. Goal-oriented, strategic leader with experience developing and managing an inpatient medicine/rapid response advanced practice provider service line. Passionate about community outreach. PROFESSIONAL EXPERIENCE Manager, APP Internal Medicine Service, 08/2022 – Present Corewell Health Farmington Hills Hospital, Farmington Hills, MI • Responsible for the management and development of the inpatient internal medicine/rapid response APP team, consisting of 20 nurse practitioners and physician assistants. Management includes all activities related to direct patient care, service line development, budget preparation, quality improvement program goals, strategic planning, and protocol or policy review/development. • Responsible for completing all administrative duties including interviewing/hiring, onboarding, scheduling, provider evaluations, maintenance of employee files, conducting disciplinary action, and assistance with payroll. • Attend and participate in a number of hospital and system-wide committee meetings, including the Safety Event Review Committee, APP Governance Council, Code Blue Review Committee, Medical Quality Improvement and Patient Safety Committee, and others. • Clinical duties include performing medical admissions and providing urgent care to stabilize decompensating patients in rapid response situations. Hepatology Physician Assistant, 01/2022 – 08/2022 Vanderbilt University Medical Center, Nashville, TN • Functioned largely independently as a billing provider of the Vanderbilt Hepatology and Liver Transplant Center. • In the clinic setting, diagnosed and managed patients with various liver conditions including compensated and decompensated cirrhosis, autoimmune hepatitis, viral hepatitis, NAFLD, and others. Hospital Medicine Physician Assistant, 11/2020 – 01/2022 Vanderbilt University Medical Center, Nashville, TN • Primary responsibility was providing overnight cross-cover care to patients admitted to the hospital medicine services. Patient load ranged from 50-100 patients per night. Additional duties included performing admissions, consultations, and rounding on an observation service. Hospital Medicine Physician Assistant, 11/2017 – 10/2020 Bronson Healthcare Group, Kalamazoo, MI • Was responsible for the work-up and treatment of patients admitted to Bronson Methodist Hospital, a tertiary center, in collaboration with a supervising physician. Provided care on a variety of hospital units including general medical, intermediate care, cardiology, neurology, and oncology floors. • Assisted in piloting and fully implementing a mid-level provider medical consulting service for patients admitted to Bronson Fieldstone, an inpatient psychiatric hospital. Worked collaboratively with leadership to optimize the psychiatric and medical care of patients through data analysis and patient feedback.

EDUCATION Southern Illinois University School of Medicine Physician Assistant Program, Carbondale, IL M.S. Physician Assistant Studies, 06/2015 – 08/2017 Central Michigan University, Mt. Pleasant, MI Bachelor of Science, Exercise Science, 08/2009 – 12/2013

References

Available upon request

4/7/2023 1:48:50 PM

Danielle King

From: postmaster@muniweb.com
Sent: Wednesday, June 21, 2023 10:12 PM
To: CMO
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Submittor's Name Robert Levine, MD

Submittor's Address

Submittor's Phone

Submittor's Email

Interest(s) Beautification Commission

Resume

I am writing to express my interest in becoming an alternate member of the City Beautification Commission. My family and I have resided in Farmington Hills for 30 years, and this is an opportunity to give back to the community and make it a more beautiful place to live. I am experienced in committee work, have a keen eye for detail as an artist who has exhibited in City Hall, and am a former eye surgeon. I have a strong background in gardening, primarily with local native plants, and believe they are essential for creating a healthy and sustainable environment. For many years I have used them to design an ecologically-balanced, landscaped habitat that is both beautiful and sustains pollinators, birds, and amphibians. I am a member of the Michigan Botanical Society. The city and the Beautification Commission were inspiring examples for me, with ideas such as planting the grassy median of Northwestern with native plants as well as a rain garden at City Hall. When I spoke with Ashlie Smith at the Nature Center, she recommended this commission as a way to become more involved. I am eager to use my skills and experience to help the City Beautification Commission achieve its goals improving the overall appearance of our city's spaces, and hope to be a valuable asset to the City Beautification Commission. Thank you for your time and consideration. Sincerely, Robert Levine, MD

References

Beth Brandvain,

6/21/2023 10:12:20 PM

Danielle King

To: Theresa Rich
Subject: RE: yes to Beautification Committee recommendation

Begin forwarded message:

From: Robert Levine <[REDACTED]>
Date: December 11, 2023 at 7:48:25 PM EST
To: Theresa Rich <TRich@fhgov.com>
Subject: yes to Beautification Committee recommendation

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Dear Mayor Rich,

I'd be delighted to serve on the Beautification Committee, and appreciate your call about putting me on the Council agenda and recommending me for this position . Please proceed.

I hope to meet you in person at some point, perhaps during the art installation ceremony at City Hall on January 12, or some other event or meeting.

Respectfully,
Robert M. Levine, MD

Danielle King

From: postmaster@muniweb.com
Sent: Monday, December 11, 2023 7:35 AM
To: DistributionList-CityManagerOffice
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Submittor's Name	Jerry Price
Submittor's Address	[REDACTED]
Submittor's Phone	[REDACTED]
Submittor's Email	[REDACTED]
Interest(s)	Grand River Corridor Improvement Authority

Resume

Jerry Price Advisor Diversity, Equity, and Inclusion Partner for Corewell East a \$15 billion not-for-profit integrated health system in Michigan. In his role, Jerry is responsible for implementing Corewell Health's five areas of focus framework; cultural competency, equity of care, workforce diversity, community engagement, and supplier diversity to over 30,000 team members at Corewell Health East. Before joining Corewell Health, Mr. Price was Fund Development Director at the Greater Michiana YMCA. He was responsible for developing inclusive community engagement strategies, identifying and implementing strategic sustainability plans for donor development, and building inclusive community partnerships. Mr. Price holds an A.A. and B.S. degree in Organizational Leadership/ Diversity and Inclusion from Waldorf University. He is also a Certified Diversity professional, a national trainer for Vital Smarts, and former national trainer for the Y of the USA. A community-focused person Mr. Price has served on the boards of; Michigan's Great Southwest Strategic Leadership Council, Horizon Bank Diversity Business Council, Harbor Habitat for Humanity, and Cornerstone Alliance Economic Development-Community Ambassador Program. Mr. Price currently serves on the Executive Committee for the Kitchenaid Senior PGA Championship and Chairs the board of his high school alma mater School Foundation.

References

Carlos Cubia- Chief Inclusion, Equity, Diversity, and Sustainability Officer Corewell Health-
[REDACTED] Dr. Lynn Todman-Vice President Health Equity Corewell Health-
[REDACTED] Valissa Armstead- Senior Director Diversity, Equity, and Inclusion Corewell
Health-[REDACTED]

12/11/2023 7:34:11 AM

Danielle King

From: postmaster@muniweb.com
Sent: Wednesday, December 27, 2023 1:42 PM
To: DistributionList-CityManagerOffice
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Submittor's Name Lloyd Banks

Submittor's Address [REDACTED]

Submittor's Phone [REDACTED]

Submittor's Email [REDACTED]

Interest(s) Commission on Community Health|Planning Commission|Zoning Board of Appeals

Resume

Lloyd L. Banks, III is a distinguished professional with a rich background in business and community leadership. As the CEO of Banks & Company, he has shown exceptional leadership in public affairs, government relations, marketing, and diversity initiatives. Previously, he held a significant role at Blue Cross Blue Shield of Michigan, enhancing the company's community and market presence. His achievements include successfully leading campaigns in the cannabis industry and substantial community fundraising. Banks is a proud Central Michigan University alumnus, enjoys golf and motivational speaking, and is an imminent author. He holds multiple board positions, exemplifying his commitment to community and professional excellence.

References

Diana C. Jones [REDACTED] The Honorable Mary Waters Detroit City Council Member
At Large mary.waters@detroitmi.gov 313-628-2363 The Honorable Alisha bell Wayne County Commission
Chair abell3@waynecounty.com

12/27/2023 1:41:04 PM

Vivek Das

3/9/2023

Grand River Corridor Improvement Authority (CIA)
City Hall
Farmington Hills

Dear Grand River Corridor Improvement Authority (CIA)

I am a longtime resident of Farmington Hills, having stayed in the area for over 20 years. My Children have graduated from Farmington Hills High School. I am an Indian American Community leader. I believe in giving back to the community and I have organized multiple volunteering and charitable activities at Gleaners food bank, Soup Kitchen and the like. I also mentor youth and young adults in preparing for job interviews, getting placement, providing advise on essential job skills and the like. I am a Director at a large Information Technology Company. You can see my profile at <https://www.linkedin.com/in/dasvivek/>

I value getting involved with the local community. I enjoy community service and volunteering activities. I am looking to make a positive impact through my volunteering activities.

I have a keen interest in Grand River Corridor Improvement Authority as I live very close to it. I have advised large corporations in USA in Business and Information Technology development as part of my job . I have also attached my resume to the email.

I would like to join the Grand River Corridor Improvement Authority (CIA) and assist you in your mission as a volunteer and perform community service. I can contribute some time and effort as a volunteer every week or month as required. I hope to hear favorably back from you.

Sincerely,

Vivek Das



VIVEK DAS

Farmington Hills, Mi [REDACTED]

[REDACTED] [REDACTED] <https://www.linkedin.com/in/dasvivek/>

Enterprise Information Architect

Vivek Das is a Functional and Technical Enterprise Data Management leader with a proven record of delivering business value by the application of data to solve Business problems. Vivek is skilled in all facets of Enterprise Data Management including Data Architecture, Data Integration, Master Data Management, Data Governance, Data Classification, Data Dictionary, Data Catalog and Lineage, Database Administration, Data Modeling, Data Analytics, Data Mining, Machine Learning, Data Science, and Artificial Intelligence. He has enabled multiple CRM, BRM, Mainframe applications, Cloud Applications, SAP ECC and S/4 Hana systems by executing the Data Conversion and Migration activities as well as coding the required Business Functionality. Vivek has Analyzed, Architected, Designed, and built CRM and BRM systems as well as Data warehouses and Data Lakes on the Cloud and on premise. Vivek specializes in Integrating data from multiple sources, formats, volumes, to fulfil the business need. He collaborates well with stakeholders, defining technology strategy and drives execution to achieve desired objectives. Vivek holds multiple certifications and has demonstrated hands on ability to lead delivery teams in enabling enterprise-wide Cloud systems on GCP, AWS and Microsoft Azure Cloud. He has expertise in Data Security and applying Cybersecurity measures to Business applications. Vivek has deep Functional knowledge in Banking, Financial Services, Insurance, Health Care, Manufacturing, Life Sciences, Pharmaceuticals, Electronics, Oil and Gas, as well as Federal and Local Government sectors. Vivek has deployed multiple Data Mining, Data Sciences, Machine Learning, Business Intelligence (BI), Data Analytics, Data Science, Data Warehouse, Data Security, Encryption, Fraud Management and Data Integration systems to support essential business operations. He has proven expertise to help organizations improve Operational efficiency, reduce costs, improve customer centricity, and maximize revenue streams by unleashing the Full potential of their Information systems. Expertise in:

- Data Integration in real time, Data Security, Data Classification, MDM, Design and Architecture of Databases, Designing Data Quality at Source, Data Engineering, Building Data Pipelines to Ingest, Curate, and Consume big data, Designing Data Analytics and Reporting Solutions.
- Enabling CRM, MDM and ERP systems on Cloud like Sales Force, Microsoft Dynamics, SAP both on Premise and on Cloud, Artificial Intelligence, Data Mining, IOT, Data Analytics, Reporting applications. Expertise in Data Security, Database encryption, Database Administration, Data Architecture, Data Integration, Data Warehouses and Solution Architecture.
- BI Analytics, Data Integration, SAP Data Migration, SAP Data Conversion, Data Quality, SAP Data Management with various tools like SAP Data Services (BODS), SAP S/4 HANA, SAP BW, SAP MDG, BOBJ, Information Analyzer, IBM Information Server, INFORMATICA and Collibra.

Experienced in SAP Data Conversion/SAP Data Migration for SAP ECC and SAP S/4 Hana implementations.

- Specialized in connecting the data to enable clarity in big picture. Expertise in analysis, extraction, schema, lineage, governance, working with large and complex datasets. Skilled in Cloud applications with Cloud Storage, Dataproc, Cloud Dataflow, Rest API development, API gateway management, big query, Oracle R12 EBS. Skilled in AWS Essentials and Cloud Practitioner skills.
- IT and Data Strategy, Application Digital Transformation, Technology Roadmap, Development, Ecommerce, Web applications, Program & Project Management, Dev Ops & Legacy Modernization, Automation. Excellent team building, mentoring and leadership abilities. Excellent verbal and written communication skills; able to build strong rapport across all functional areas.
- Data Science, ML, Data Mining, NLP, NLC, ESB, Java, MDM, Salesforce, SAP OTC, SCM, SD, MM, SPSS, SAS, Minitab, Python, Mainframes, CICS, Cobol, Db2 and Distributed systems.
- Deep knowledge in Claims Processing, Property and Casualty, Provider and Payor, Pharmaceutical data models, Membership, Loan Origination, Mortgage operations, AML, Credit and Lending Operations, Wealth Management services and similar functional areas. Expertise in Data Security operations in a banking setup.
- Create a digital technology infrastructure to capture large sets of data and find insights to drive business success. Define, analyze, and document business requirement features to produce technology solutions/alternatives; communicate with users on solutions, scope, and implementation details. Develop functional software solutions for new processes or process enhancements.
- Lead, coach and develop an enthusiastic, engaged team. Provide thought leadership, direction, and best practices to the IT team. Lead with integrity and model for others a collaborative and inclusive leadership style.

PROFESSIONAL EXPERIENCE

Enterprise Data Architect, Director, TCS

Mar 2021 to date

Designed Data Catalog, Defined Business Terms, Designed Enterprise Data Dictionary to quickly find the Business Terms and locate the physical instances of data storage to enable regulatory, traceability and Compliance systems in Life Sciences. Delivered Data Security including Data Encryption measures at Large Enterprises. Set up Data Encryption and Data Security for Data at Rest and Data in Transit. This included implementing Transport Layer Security, LDAP, Total database encryption, Identity and Access Management systems as well as enhanced Key Management measures including HSM and Multi Factor Authentication.

Solution / Cloud / Data Architect EQUIFAX Digital Transformation Solution/Data Engineer

Aug 2020 to Mar 2021

- Implemented enterprise scale digital transformation on Oracle R12 EBS for CRM and BRM solutions. Analyzed, designed, Built and Deployed Cloud based data lakes, data warehouses on Google Cloud and AWS. Implemented large scale Data Ingestion, Data Curation and Data Reporting Solutions including building Data Pipelines, Cloud based Data Analytics, Data Lakes and Reporting solutions. Designed Solutions to enable easier business operations, scalability, data integration, and enable information exchange with multiple agencies.
- Improved the detection of Fraud and enabled increased Revenue Recognition by enabling integration across multiple systems like Contract Management, Billing and Revenue Management, Customer and Product Onboarding systems. Through the connected data, there was better tracking of business processes and data flows.

Architect Technology & Business Integration, Accenture

2018 to June 2020

Digital Transformation Data Engineer

- Deployed Cloud based Data Analytics, Data Lakes and Reporting solutions based on CI/CD devops pipeline. Solutioned for multiple clients to enable easier business operations, scalability, data integration, and enable information exchange with multiple agencies.
- Delivery Lead and Solution Architect for Big Data and Cloud Based Data Lakes and CRM systems on AWS, GCP and Microsoft Azure Cloud. Worked with Business Stakeholders in scoping Sales Functionality as well as all other Stakeholders and Vendors in successful delivery of the CRM system.
- Led transformation methodology development and planning for digital transformation using API, IaaS & PaaS for base applications. Advised Clients with modernization strategy, data migration, target state architecture, and implementation roadmap to transform a legacy application to Cloud based applications. Worked on Multiple AWS service such as databases, computing, application services, IOT and worked on Redshift, Lambda, EMR, EC2, Identity Management, IOT applications.

Sr. Technical Architect, HCL

Mar to June 2018

Created Marketing Campaigns for Retail Consumers

- Conducted digital marketing campaigns, database marketing, data mining and analytics to increase Sales and improve ROI of marketing efforts for clients.

Technical Delivery Manager/Solution & Data Architect, IBM 2007 to 2018

Created Innovative Data Products and services to enhance Business

- Enabled multiple SAP systems in transformational projects with Client spend of over \$ 50 M/yr. and benefits in multiple times that by migrating the master and transactional data needed. SAP Lead Consultant for Data Migration and Conversion for Multiple SAP systems across Multiple Clients and Industries. Led the Data Migration and Conversion activities for Customer, Vendor Master, Purchase Info Records, A/R, A/P systems,

Profit, Cost Centers, Material Master using Migration Cockpit, LSMW, BAPIs, SAP Bods, RFC, IDOCs, Abap. Skilled in transformation, configuration and enablement of the required Master and Transactional data to enable SAP systems.

- Designed and Created a Data Warehouse by integrating data from over 130 global sources for IBM's Blue Harmony Program. Combined fragmented datasets and collaborated to improve processes and produce benefit of over \$ 3M / yr.
- Enabled the monetization of big data and increased revenue by \$ 2 M at a bank by enhancing analytics, governance, and audit functions on the data. Through this framework, the Enterprise Data Provisioning group was able to provide superior quality data to downstream users.
- Set up Data Governance framework for validating and correcting Sensitive Personal Information (SPI) as well as Payment Card Industry (PCI) data. Enabled an Enterprise-Wide Glossary and linked the Business Metadata and Business Terms with the Technical Data. This resulted in an improvement in Productivity and Efficiency by about 30% and helped monetize the data as a service model.
- Setup an Authentication, Encryption, and Secured Access system for a banking client for some of their global customer database. This helped the bank meet its regulatory compliance requirements.
- Enabled a Cloud based Real Time Statistical Solution using IBM ESB/SOA Architecture, Db2, SPSS, Node Red, IBM Bluemix, and a web based Customized dashboard measuring the composite risk score of appliances on an IOT platform which saved \$5 M in product recall costs
- Led the assessment of strategic options for the consolidation of multiple Customer services for a large Insurance Client on a cloud and big data platform. Enabled processing of member services, benefits, claims, and provider and payer services. Integrated the web based front end with many mainframe applications and databases at the back end. Resulted in savings cost by about 25% and an improvement of 30% in Customer Satisfaction index.
- Measured and correlated effectiveness of Marketing Campaigns by comparing, enhancing, and storing campaign, sales and web activity of customers, discount coupons used, timing to understand and analyze which campaigns were effective. This led to 10% improvement in marketing activity for client and better tracking of money spent and results achieved.
- Applied Machine Learning, Natural language processing. Proposed actionable insights using Watson Knowledge Services and Discovery in a financial institution setting to determine propensity to buy a product/service using Web services and Watson APIs. Defined Entities and Intents for task automation and classification. Results helped agents focus on prospective buyers based on written and verbal discussions.
- Created Info packages for Full, Initial, Delta loads for both Master data and Transaction data. Enhanced LIS data sources to create end-to-end reporting solution for many logistics areas. Participated in design reviews, spec reviews, code reviews, test plan reviews. Presented proposals to executive management. Reconciled BI reports with the ECC & legacy data. Worked on Modeling and Data provisioning and reporting in Hana

Studio & SAP BW on HANA database landscape and increased efficiency of business operations.

- Involved in data migration to Cloud using Amazon RDS, S3 buckets and AWS EC2. Import and export My SQL database and contents through RDS, and static files to S3.
- Implemented multiple Data Integration systems having worked extensively in Health Care, Pharmaceuticals and Life sciences industry in Claims, Provider, Facility, Member Services, Pharmacy areas.
- Incorporated Customer feedback and worked with Product development groups within IBM to develop products such as Infosphere Information suite, Watson explorer, Watson Apls such as Speech to text, Tone Analyzer, Personality Insight.
- Managed teams of up to twenty people, developed staffing plans, managed workloads, defined deliverables, hired resources, conducted performance reviews, and ensured compliance with established company policies.
- Developed Data Strategy roadmaps including Current state assessment, Analysis, Prioritization and Change Management.
- Clients include Citibank, MetLife, TD Bank, AIG, Blue Cross Blue Shield, Ford Motor Company, Caterpillar, Western Digital, Convatec, A O Smith, Vanguard, Northern Trust, HCSC, Otsuka Pharmaceuticals, Integra, Bristol Myers Squib, State Farm, BP, Shell, Cigna, and IBM internal projects.

Solution Architect & Programmer Analyst, BCBS 2006 to 2007

Solution and Data Architect integrating business applications

- Designed the transfer of data between Medimpact and Blue Cross Blue Shield (BCBS). Documented the System Architecture Document (SAD), which captured all the Use Cases, the Class diagrams, Program specs and well as all Functional requirements.

Database Administrator, Ford Motor Company 1999 to 2006

Create and maintain Mission critical database systems

- Created Logical and Physical database design. Created Database objects such as Tables, Views, Stored Procedures, MQTs. Maintained Production db2 and Oracle databases for business-critical global applications. Set up image copies and online reorganization system for Customer Knowledge Systems databases. This being a multi-terabyte global call center application, the database had to be available 24 x 7 x 365. Resolved production abends, carried a pager, and mentored new dbas. Performed database maintenance functions such as Reorgs, Run stats etc. Performed application and database tuning work to improve performance.
- Reduced web application performance time drastically (more than 90%) for an order entry system and saved more than \$ 1 million per year to the business. The Average Processing time dropped from 1.741 seconds to 0.657 seconds. The Average CPU time dropped from 1.102 seconds to 0.026 seconds because of the performance tuning

improvement. The transaction is executed 1500 times a day on the average by customers on the web. This was achieved by extensive database and application redesign.

CERTIFICATIONS AND TRAINING

- **AWS Certified Cloud Practitioner**
- **Open Group** Master Certified Solution Architect specializing in Information Architecture.
- **SAP:** Key Functional topics in a System Conversion to SAP S/4 Hana.
- **SAP:** Migrate your Business Data to SAP S/4 Hana.
- **Databricks:** Databricks and Delta Lake.
- **Google:** Google Cloud Platform Big Data and Machine Learning Fundamentals.
- **Google:** Modernizing Data Lakes and Data Warehouses with Google Cloud Platform (GCP).
- **IBM - IBM Certified Solutions expert DB2/UDB V 7.1 UDB Database Administration for UNIX/Linux/Windows and OS/2**
- **IBM - IBM Certified Solutions Expert DB2 UDB v 7.1 Database Administration for OS/390**
- **PMI** – Certified Project Management Professional (PMP)
- **MS Systems Engineering (RMIT, Australia)**
- **Bachelor** of Science in Industrial Engineering

SKILLS

- Skilled in: Sap Data Migration and Conversion, SAP S/4 Hana, SAP BI, SAP ECC, BODS, BOBJ, BEX, WEBI, IBM DataStage, IBM Quality Stage, IBM Information Analyzer, AWS, GCP, Azure, Redshift, Talend, Solution Architecture, Information Architecture.
- Skilled in Implementing Cybersecurity measures to protect data in Rest and transit.
- REST APIs, SOA, JSON, Xml, Defining Data Catalog, Data Dictionary, Data Glossary, IBM Information Governance, Collibra.
- Proficiency with CDH, CDP, Hadoop, HDFS, Map-Reduce, Hive, Pig, Spark, Impala, Sqoop, SPSS, Palantir, Python

- Proficiency with Cloud - AWS Cloud, GCP, Microsoft Azure, Tibco Scribe, Databricks, Delta Lake, EC2, RDS, Salesforce (SFDC), IBM Bluemix, BDM
- DevOps & Automation – Jira, IBM RTC, IBM Urban Code, SonarQube, GitHub
- Others – TIBCO, MULE, IBM SOA Platform, Oracle SOA platform, Java, Oracle, DB2 / UDB, SQL Server, Watson APIs, Crystal Reports, Cobol, CICS, IMS, DB2
- Framework – TOGAF, ZACHMAN
- ETL: Infosphere Information Server (DataStage /Quality stage/Information Analyzer), SQL loader, Informatica, PL/SQL, Skilled with SAP data packs, Informatica BDM
- OLAP and reporting: Business Objects, Cognos, Composite, Tableau, Spotfire
- RDBMS: Db2 9.x, Db2/400, Db2/UDB, IMS, Oracle 8i / 9i, PL / SQL, Teradata, Netezza
- Languages: Abap/4, CICS-COBOL-Db2, C++, Java, JSON, BASIC, C, HTML, XML
- NOSQL Databases: Postgres, Mongo dB, Cassandra
- Software: SPSS, SAS, UML, RUP, Erwin, BMC Change Manager/Catalog Manager, Strobe, Omegamon, DB Explain, ETL, SPUFI, QMF, TSO, Move4Db2, File-Aid, Abend-Aid, IBM Tivoli, Endeavor, Expeditor, COBOL, CICS, Basic, JCL, REXX, MQ Series, PL/SQL, Transact/SQL, C, MS office, Visio, Share Point, Endeavor, Xpeditor, SyncSort, File aid.
- Hardware: IBM Mainframe, ES/9000. AS/400, IBM RS/6000, HP UNIX server, DELL
- Operating Systems: Z/OS, OS/390, MVS, S/400, IBM RS/6000, UNIX (HP/UX, Solaris, AIX), Windows
- ERP: SAP S/4 Hana, SAP ECC 6.0, SAP BI 7.3, ABAP/4, Bex Analyzer, Query Designer, Web Intelligence, SAP BPC 7.0, SAP Business Objects 4.0, Dashboard Design (Xcelsius), SAP BW, ECC with focus on tools used in integrating Information Server with SAP. Familiar with IDOC, BAPI, SAP BW and LSMW, HANA STUDIO in SAP HANA SPS5 & SAP BW 7.3 on HANA DATABASE landscape.
- MDM: Skilled in deployment of MDM especially Customer, Vendor, Contracts, Purchase Orders, GL Accts, Material. Creation of Master Record.

Danielle King

From: postmaster@muniweb.com
Sent: Monday, December 18, 2023 12:04 PM
To: DistributionList-CityManagerOffice
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Submittor's Name Vivek Das

Submittor's Address

Submittor's Phone

Submittor's Email

Beautification Commission|Commission on Aging|Committee to Increase Voter Participation|Economic Development Corporation|Multicultural/Multiracial Community Council|Planning Commission|Retirement Board of Trustees

Resume

Hi, I am a longtime resident of Farmington Hills and I live here with my wife and two kids for more than 25 years. I like Farmington Hills and would like to see it prosper, vibrant and happy. I currently volunteer at the midday meals on wheels program at the Costick Center and I work with Teresa Bryant, Marsha Koet, Andrea Ali, Anna Durham and others. So they can all be my references as I am volunteering for a couple of months now. I have some ideas for economic development and for attracting new business as I have many years of experience as a Software Consultant and have traveled the country. I look forward to joining these boards and the new mayor made out a call recently for people to join the commission/boards. I am applying to help in that effort. I am an Indian American community leader. Thank you . Kind regards Vivek

References

You can take reference from Nancy Cook at [REDACTED] or Teresa Bryant at [REDACTED]. I work with Teresa as well as Marsha Koet and Andrea Ali of Special services department.

12/18/2023
12:04:06 PM

Danielle King

From: postmaster@muniweb.com
Sent: Thursday, December 28, 2023 4:04 PM
To: DistributionList-CityManagerOffice
Subject: Boards and Commissions Questionnaire

CAUTION: This email originated from outside your organization. Exercise caution when opening attachments or clicking links, especially from unknown senders.

Submittor's Name Ikramuddin Khan

Submittor's Address [REDACTED]

Submittor's Phone [REDACTED]

Submittor's Email [REDACTED]

Interest(s) Commission on Community Health|Emergency Preparedness Commission|Parks Recreation Commission|Planning Commission

Resume

Ikramuddin Khan [REDACTED] / C: [REDACTED]
SUMMARY Physical Therapist seeking managerial opportunity skills. Highly motivated, energetic and self-confident individual with rich experiences coupled with exemplary background in physical therapy and Customer service. Passionate about learning and implementing proven strategies to improve productivity and enhance performance. Reserve Deputy Sheriff Oakland County 2023. SKILLS: Licensed Physical Therapist - Michigan # • 5501014419. • Weekend Supervisor at Select Rehabilitation Services. EXPERIENCE Certified Taping Practitioner. MS word, Rehab Optima, McKessen, Matrix, Cerner. Work Experience: Reserve Deputy Oakland County Sheriff's Office 2023. Clinical PT Specialist Harper- Hutzel University Hospital (Detroit Medical Center) 2022- Current. Staff Physical Therapist Harper- Hutzel University Hospital (Detroit Medical Center) 2019-2021. Staff Physical Therapist / Select Rehabilitation Services - West Bloomfield, MI 07/2015 - 01/2016. Staff Physical Therapist / West Bloomfield Nursing Center (MHRS) - West Bloomfield, MI 02/2013 - 07/2015. Staff Physical Therapist / Encore Rehabilitation at Regency Health Care Center - Taylor, MI 02/2011 - 03/2013. Staff Physical Therapist / Henry Ford Home Health Care - Detroit, MI 02/2011 - 02/2012. Staff Physical Therapist / Oakwood Skilled Nursing and Rehabilitation Center - Dearborn, MI 05/2009 - 02/2011. Physical Therapy Aide / Health Bridges - Lexington, MA 12/2005 - 10/2007 Worked for Health Bridges, Lexington, MA (Lexington Health Care) as a Physical Therapy Aide in 2006-07. Sales Associate / Sears - Burlington, MA 12/2005 - 02/2009 Physical Therapy Aide / Blair House Skilled Nursing Facility - Tewksbury, MA. 07/2005 - 11/2005. Physiotherapist / Self-Employed -, India 02/2002 - 06/2005 Owned and operated two physiotherapy clinics in India, 2002-2005. Owned and operated Orthotics and Physiotherapy modalities manufacturing facility. 2003-2005. EDUCATION AND TRAINING A.T. Still University: Doctor of Physical Therapy 2019. Masters: Allahabad Deemed University - India, Masters Orthopedic Physiotherapy 2005 Bachelors: Dr. MGR Medical University 2002 Bachelors: Physiotherapy Diploma: Health & Hospital Management 2003 AI India Institute of Health & Hospital Administration. University of Michigan 2018 Certificate Successful Negotiation: Essential Strategies and Skills. Certificate: Certified Kinesiology Taping practitioner to improve movement and function. ACTIVITIES AND HONORS Employee of the month - Henry Ford Home Healthcare 9/2011. No way, please volunteer in public safety (VIPS) Reserve Deputy Oakland County Sheriffs Office 2023.

References

Azam Masood [REDACTED] (Served on Zoning Board) Asim Khan [REDACTED] (Serves on Zoning Board)

12/28/2023 4:02:41 PM

REPORT FROM THE CITY MANAGER TO CITY COUNCIL – January 8, 2023

SUBJECT: Memorandum of Understanding and Non-Disclosure Agreement with Mitsubishi Electric Automotive America

ADMINISTRATIVE SUMMARY

- Opportunity exists for the city to partner with Mitsubishi Electric Automotive America (Mitsubishi) for the purposes of developing innovative tools for potential optimization and automation of road asset data collection and work order management.
- Mitsubishi and city staff have been in discussions regarding the potential for mutually beneficial outcomes and believe the partnership is worth pursuing.
- The pilot will be for a 90-day period and will be at no cost to the city.
- To move forward with the 90-day project, Mitsubishi has asked the city to agree to a Memorandum of Understanding and a Mutual Non-Disclosure Agreement. These documents have been reviewed as to form and content by the City Attorney's office.

RECOMMENDATION

Resolve that the City Council of the City of Farmington Hills hereby authorizes the City Manager to execute the Memorandum of Understanding (MOU) and Mutual Non-Disclosure Agreement (NDA) with Mitsubishi Electric Automotive America, Inc.

Prepared by: Karen Mondora, P.E., Assistant City Manager
Approval by: Gary Mekjian, P.E. City Manager

REPORT FROM THE CITY MANAGER TO CITY COUNCIL
January 8, 2024

SUBJECT: PURCHASE OF JOHN DEERE EQUIPMENT

ADMINISTRATIVE SUMMARY

- Sealed proposals were advertised, publicly opened and read aloud by Oakland County for Agricultural, Grounds & Maintenance Equipment. The awarded contract with Deere & Company is offered to the City as a cooperative bid. Participating in a cooperative purchase provides cost savings for the city due to the buying power of a cooperative.
- The one (1) each John Deere TX Deluxe Cargo Gator with all terrain tires and electric lift will be used for general maintenance and trail repairs at Heritage Park and Woodland Hills Park.
- The one (1) each John Deere TX Deluxe Cargo Gator with turf tires will be used for general park maintenance and athletic field maintenance at Founders Sports Park.
- The equipment will be stored at Heritage Park and Founders Sports Park, respectively.
- A two-year warranty for all parts & labor was required as part of the specifications.

Funding for the two (2) John Deere TX Gators is budgeted and available in the Special Services Parks Millage Fund.

RECOMMENDATION

In view of the above, it is recommended that City Council authorize the City Manager to approve the purchase order in the amount of \$19,724.93, including one (1) each John Deere TX Turf Gator with all terrain tires and electric lift for \$10,309.22 and one (1) each John Deere TX Deluxe Cargo Gator with turf tires for \$9,415.71, to Deere & Company and to take delivery of the equipment via Revels Turf & Tractor as an authorized dealer.

Prepared by: April Heier, Parks Maintenance Supervisor
Reviewed by: Nikki Lumpkin, Senior Buyer
Reviewed by: Michelle Aranowski, Director of Central Services
Reviewed by: Ellen Schnackel, Director of Special Services
Approved by: Gary Mekjian, City Manager





OFFICE OF CITY CLERK

INTEROFFICE CORRESPONDENCE

TO: Mayor and City Council
FROM: Carly Lindahl, Interim City Clerk
DATE: January 8, 2024
SUBJECT: Rescheduling the February 26th City Council meeting

It is requested that City Council consider rescheduling the regular City Council meeting of February 26th to February 5th, due to the need to utilize the Chamber and other meeting space that evening in preparation for the February 27, 2024 Presidential Primary Election.

The regular meeting schedule will resume with the March 11th City Council meeting.

RECOMMENDATION:

IT IS RESOLVED, that City Council hereby reschedules the regular City Council meeting of February 26, 2024 to February 5, 2024.

REPORT FROM THE CITY MANAGER TO CITY COUNCIL – January 8, 2024

SUBJECT: Consideration of a Resolution for Acceptance of the 2023 Transportation Asset Management Plan

ADMINISTRATIVE SUMMARY

- Asset management is a strategic and systematic process of operating, maintaining, and improving physical assets based on engineering and economic analysis to identify a cost-effective sequence of maintenance, preservation, repair, rehabilitation, and replacement.
- The State of Michigan has been actively pursuing asset management since 1998 when the Michigan Legislature established the ACT 51 Transportation Funding Committee. The State has continued to support Asset Management by establishing the Transportation Asset Management Council (TAMC) in 2002. To further advance the goals of transportation asset management, the Michigan Legislature enacted Public Act 325 of 2018, requiring that governmental agencies submit a detailed Asset Management Plan to the Transportation Asset Management Council.
- The TAMC requirements for the Asset Management Plan includes an inventory of assets, identification of performance goals, risk of failure analysis, anticipated revenues and expenses and performance measures.
- Public Act 325 of 2018 requires a resolution from City Council accepting the Transportation Asset Management Plan prior to submittal to the TAMC.

RECOMMENDATION

IT IS THEREFORE RECOMMENDED, that the City Council of the City of Farmington Hills accepts the 2023 Transportation Asset Management Plan and provide a copy of the resolution as well as the Transportation Asset Management Plan to the Transportation Asset Management Council as required by Public Act 325 of 2018.

SUPPORT DOCUMENTATION

Michigan Public Act 325 of 2018 requires local agencies responsible for 100 or more certified miles of road to develop and submit an asset management plan to the Transportation Asset Management Council (TAMC) that includes an asset inventory, performance goals, risk of failure analysis, anticipated revenues and expenses and performance outcomes. This plan is required to be updated and resubmitted every three years.

By adopting the attached resolution, the City of Farmington Hills will meet the obligations of the Transportation Asset Management Plan set forth in Public Act 325 of 2018.

Prepared by: Mark S. Saksewski, P.E., Senior Traffic Engineer
Departmental Authorization by: Tammy Gushard, P.E., Interim Director of Public Services
Approval by: Gary Mekjian, P.E. City Manager

City of Farmington Hills

RESOLUTION FOR ACCEPTANCE OF THE 2023 TRANSPORTATION ASSET MANAGEMENT PLAN

R-__-24

At a regular meeting of the City Council of the City of Farmington Hills, County of Oakland, State of Michigan, held on _____, at 7:30 p.m., Eastern Standard Time, with those present and absent being;

PRESENT:

ABSENT:

the following resolution was offered by Councilmember _____ and supported by Councilmember _____:

WHEREAS, the State of Michigan has been actively promoting Transportation Asset Management since 1998 and as such established the Transportation Asset Management Council (TAMC) in 2002; and

WHEREAS, the State Legislature enacted Public Act 325 of 2018 that requires local road agencies to develop and submit an asset management plan to the Transportation Asset Management Council (TAMC) that includes an asset inventory, performance goals, risk of failure analysis, anticipated revenues and expenses and performance outcomes to be submitted to TAMC; and

WHEREAS Public Act 325 of 2018 requires that the Transportation Asset Management Plan receive City Council approval prior to submittal; and

NOW, THEREFORE BE IT RESOLVED,

1. The City Council of Farmington Hills hereby agrees to accept the 2023 Farmington Hills Department of Public Services Transportation Asset Management Plan; and
2. The City Manager and the City's Finance Director are authorized to sign the "Proof of Acceptance" in order to provide certification of the City's Transportation Asset Management Plan.
3. The 2023 Transportation Asset Management Plan be submitted to the Transportation Asset Management Council (TAMC) with a copy of this resolution to fulfill the requirements of Public Act 325 of 2018.

AYES:

NAYS:

RESOLUTION DECLARED ADOPTED ON _____.

I, Carly Lindahl, the duly authorized City Clerk of the City of Farmington Hills, Oakland County, Michigan, do hereby certify that the foregoing is a true copy of a resolution adopted by the City Council of the City of Farmington Hills at the regular City Council meeting held on _____.

CARLY LINDAHL, City Clerk

MINUTES
CITY OF FARMINGTON HILLS
FARMINGTON HILLS CITY COUNCIL
CITY HALL – COUNCIL CHAMBER
DECEMBER 11, 2023 – 6:00PM

The study session meeting of the Farmington Hills City Council was called to order by Mayor Rich at 6:00pm

Council Members Present: Aldred, Boleware, Bridges, Bruce, Dwyer, Knol (arrived at 6:12pm), and Rich

Council Members Absent: None

Others Present: City Manager Mekjian, Interim City Clerk Lindahl, Assistant City Manager Mondora, Directors Kettler-Schmult and Skrobola, Police Chief King, and City Attorney Joppich

ORDINANCE AMENDMENTS – NOISE AND LOADING/UNLOADING TRUCKS

Charmaine Kettler-Schmult, Director of Planning and Community Development, stated that a corporate office in Farmington Hills brought to City’s attention that there were two sections of the city code that deal with noise and the loading and unloading of trucks in commercial districts adjacent to residential districts. Staff has been working with the city attorney's office and found that the older section of the code, Section 18-165, which was enacted in 1981, duplicates what is in the newer portion of the code and is proposed to be repealed so as to defer noise regulation to the more modern noise standards of Section 17-101. She added that Section 17-101 was enacted in 2004, to address maximum permitted noise levels in various zoning districts at specified times of the day.

Director Kettler-Schmult explained that the old section of the code did not allow for loading or unloading of vehicles adjacent to residential districts within 500 feet from 8:00 PM. until 7:00 AM, and the repeal would limit the sound between 7:00 PM and 7:00 AM. but allow for the loading and unloading of vehicles within that time as long as they are kept within the sound limitations that are preserved in Chapter 17. She added that the amendment would now allow for loading and unloading on Sundays and carry over the section of 18 that references noise from a running engine of a truck or trailer that is stopped from that sort of activity for more than 20 minutes.

Discussion was held on Section 18-165(d), unnecessary noise by persons, and how that would be addressed in other portions of the code. Staff explained that would be subject to the decibel level in Section 17.

(Councilmember Knol arrived at 6:12pm)

Council expressed concerns over the idling of trucks, not loading or unloading, in commercial areas close to residential areas for extended periods of time and questioned if the proposed amendment would address that.

In response to Council, Steve Joppich, City Attorney, stated that this ordinance focuses on trucks specifically loading and unloading, however, the amendment will address the Sunday issue and restrict the time trucks are allowed to wait idling to 20 minutes and if the engine is running for more than 20 minutes, and audible on adjacent residential property, they are in violation of the ordinance. He added that under the noise restriction portion of the code, the city may have an enforcement ability on vehicles

DRAFT

idling, not for the purpose of loading or unloading, for extended periods depending on the decibel levels, whether it can be heard across the property line on residential property, and the time frame that is involved.

Council questioned the need for enforcement on trucks blocking ingress lanes and driveways, restricting public access to businesses while loading and unloading.

Director Kettler-Schmult explained that most commercial sites have designated loading and unloading areas and if they are not utilizing that area, the city can notify them that they need to incorporate that into their plans of operations and if it continues to be an issue, they may be subject to a violation, however, staff would need to be notified and see it happening.

Council had questions regarding how decibel levels are measured and if inspectors or officers carry measuring tools and how noise violations are enforced.

Director Kettler-Schmult indicated that if the city is notified of a concern, code enforcement inspectors will inquire about the time of day, duration of the issue and will then go out utilizing a sound meter to observe what is heard at the property boundary and in the event it is off hours and a recurring issue overtime may be authorize for the inspectors to go out and measure during that time.

Police Chief King explained that officers do not carry sound meters and typically by the time they arrive, the trucks are packing up to leave. He noted that the officers will submit a report for follow up, but try to work with the business informing them that they need to implement safeguards to ensure that the delivery trucks are operating according to the city ordinance.

Attorney Joppich clarified that the loading and unloading aspect of the ordinance does not require sound measurement; if the officer can hear the truck at the property line of a residential property, and it has been running more than 20 minutes and there is a witness that can testify to that, the officer will be in good shape to enforce this new section of the ordinance.

The consensus of Council was to move forward with the ordinance amendments.

POLICE DEPARTMENT AXON PROJECT PRESENTATION

Police Chief King thanked Council for fitting this item in to a study session and noted that they are under a time constraint with it being the end of the year. He explained that law enforcement sees rapid advancement of technology in modern policing that covers a variety of areas, and they have difficulty meeting those advances, whether it be with body cameras, tasers, redaction software, computers, technology and vehicles. He added that there is an increasing dependency on subscription-based services, and they have identified an opportunity to advance the departments capability and resources in technology, take advantage of cost savings, consolidate multiple services and technologies into one vendor sole source provider to make the department more effective and efficient.

A video was presented to Council from Axon, highlighting the benefits of their multiple service package for both the police department and the community they serve.

Staff discussed the technologies and services that Axon can provide to increase the effectiveness, efficiency and transparency of the agency, safety of the officers and community and to reduce liability:

- Safer and more effective tasers

- Adding to the LPR network
- Record after the fact services
- Automatic BWC activation with weapon deployment to combat officer activation failures
- GPS location and remote access to command or dispatch center
- Increase services such as evidence processing, warrant submissions, investigative review, and supervisor review and investigations
- Sympathetic activation
- AI assisted video and document redaction, auditing, and transcription services
- Streaming capability to monitor officers involved in pursuits, active restraint or attacks on officers and provide officers with assisted medical response

Chief King informed Council of his concerns with the performance of the conglomerate of vendors and service providers the department currently has, as well as the state and functionality of their current equipment. He added that Axon's services are supported by the city's IT department, collective bargaining units and the Oakland County Prosecutors office.

Staff discussed the projected annual expenditures over five years both staying with what the department has now and bundling services with Axon. It was noted that with Axon as a sole source provider and their 2023 contract pricing, the savings would be \$823,000 over the course of the five-year contract and the department has the capability in the current fiscal year budget to satisfy the initial cost.

Council inquired about options for the annual funding moving forward, if the city was financially able to handle the additional costs and the risk of having to competitively bid on these services separately.

Staff discussed funding options noting that the city was in good financial shape to fund the additional \$243,000 annually and if this project were to be broke up and bid in various pieces there is the potential of paying a lot more.

Council questioned the urgency to approve this now and why it was not presented in the budget last year. Chief King explained that last year they were still in contract with Watchguard and hopeful to work through the issues they were experiencing. He added that some of the technology and bundling was not yet available at that time, and Axon is trying to push this as quickly as possible before the 2023 contract pricing expires.

Discussion was held on the volume of Freedom of Information requests the police department receives, time it takes for staff to process the requests, turnover rate of the staff reviewing FOIA requests, and how the AI software would save a tremendous amount of time and cut down or possibly eliminate the overtime hours for those reviewing video footage.

Further discussion was held on the urgency to have a letter of commitment or contract signed by the end of the year to avoid increase in costs. Staff explained that they would attempt to contact Axon to see if they would accept a verbal commitment with a contract to follow.

The consensus was to put the contract on the agenda for the first meeting in January subject to review by the City Attorney and City Manager.

ADJOURNMENT

DRAFT

The study session meeting adjourned at 7:26pm.

Respectfully submitted,

Carly Lindahl, Interim City Clerk

MINUTES
CITY OF FARMINGTON HILLS
CITY COUNCIL MEETING
CITY HALL – COUNCIL CHAMBER
DECEMBER 11, 2023 – 7:30 PM

The regular session meeting of the Farmington Hills City Council was called to order by Mayor Rich at 7:34pm.

Council Members Present: Aldred, Boleware, Bridges, Bruce, Dwyer, Knol and Rich

Council Members Absent: None

Others Present: City Manager Mekjian, Interim City Clerk Lindahl, Assistant City Manager Mondora, Directors Aranowski, Kettler-Schmult, Schnackel and Skrobola and City Attorney Joppich

PLEDGE OF ALLEGIANCE

Rachel Timlin, Cultural Arts Supervisor, led the pledge of allegiance.

APPROVAL OF REGULAR SESSION MEETING AGENDA

MOTION by Bruce, support by Bridges to approve the agenda as published.

MOTION CARRIED 7-0.

SELECTION OF MAYOR PRO-TEM

MOTION by Boleware, support by Bruce to nominate Councilmember Michael Bridges as Mayor Pro-Tem.

MOTION by Knol, support by Boleware to close nominations.

MOTION CARRIED 7-0.

PROCLAMATION RECOGNIZING THE 20TH ANNIVERSARY OF THE FARMINGTON HILLS YOUTH THEATRE

The following Proclamation was read by Councilmember Boleware, and accepted by Rachel Timlin, Cultural Arts Supervisor:

**PROCLAMATION
Honoring the 20th Anniversary of
the Farmington Hills Youth Theatre
December 2023**

WHEREAS, the arts enrich the lives of all community members, and exposure to the arts is a vital part of young people’s educational experience and personal development; and,

WHEREAS, youth theater provides lifelong skills in collaboration, creativity and social awareness, and is especially successful in boosting confidence, self-esteem and concentration in children; and,

WHEREAS, since its inaugural season in 2003-2004, the Farmington Hills Youth Theatre (FHYT) has believed in the intrinsic value of access to children's theater and the power of the arts; and,

WHEREAS, the FHYT was created as a program of the Cultural Arts Division of the City's Special Services Department to support actors from the ages of four to 18, and has produced 55 productions, including recent productions of Disney's Little Mermaid, Matilda Jr., Beauty and the Beast Jr., Newsies, Mary Poppins and many more, in addition to a full roster of summer camps; and,

WHEREAS, the FHYT is committed not only to the craft, but also to creating an inclusive environment that supports the development of young people as valuable, contributing members of the community; and,

WHEREAS, for 20 years, the FHYT has impacted 7 thousand youth performers from the City of Farmington Hills and across metro Detroit and has provided countless hours of accessible entertainment to more than 100 thousand audience members of all ages.

NOW, THEREFORE, I, Theresa Rich, Mayor of the City of Farmington Hills, on behalf of the City Council, do hereby recognize all former and current Farmington Hills Youth Theatre performers, parents, guardians and instructors in acknowledgment of the **20th anniversary of the Farmington Hills Youth Theatre** and urge all residents to expose themselves and their children to the arts for the benefit of our community.

PRESENTATION AND ACCEPTANCE OF THE CITY'S FY 2022/2023 AUDIT RESULTS

Jamie Rivette of Yeo and Yeo presented the city's audit results to City Council discussing the purpose of the audit and noting that the city received an unmodified opinion, which is the highest level of assurance. She reviewed the city's general fund, state shared revenue, capitol investments, capital assets, retirement and retiree health care contributions,

Discussion was held on the city's pension and retiree health care fund balance, percentage of unassigned revenue in the general fund, outstanding bonds and the future of property tax revenue.

CORRESPONDENCE

There was no correspondence acknowledged.

CONSENT AGENDA

MOTION by Bridges, support by Boleware, to approve consent agenda items #9 through #13 and #15.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

MOTION by Knol, support by Boleware, to approve consent agenda item #14.

Roll Call Vote:

Yeas: BOLEWARE, BRIDGES, BRUCE AND KNOL

Nays: NONE

Absent: NONE

Abstentions: ALDRED, DWYER AND RICH

MOTION CARRIED 4-0-0-3.

PUBLIC QUESTIONS AND COMMENTS

Michael Sweeney, EPC member, provided an Emergency Preparedness Commission Tip of the Month.

Pam Gerald, resident, thanked gas station owners for allowing her to put up flyers for the Domestic Violence community forum event.

COUNCIL MEMBERS COMMENTS AND ANNOUNCEMENTS

The following Councilmember comments or announcements were made:

- Council wished everyone Happy Holidays
- Congratulated the teams within the city for putting together a great holiday lighting event
- Reception for the 2024-25 Farmington Hills Public Art Program Exhibit at City Hall will be held on January 12th
- Congratulations to Kola Restaurant and Ultra Lounge on their 10 year anniversary
- Congratulations to Iskcon Temple on their end of Diwali food donation dedication

CITY MANAGER UPDATE

City Manager Mekjian provided an update on the following:

- Thanked everyone for attending the holiday lighting event
- Yard waste pick up ends this week
- Wished everyone Happy Holidays and a safe and prosperous New Year

PUBLIC HEARING

PUBLIC HEARING AND CONSIDERATION OF THE CONSTRUCTION OF A CELLULAR TOWER LOCATED AT 30691 GRAND RIVER AVENUE, SITE PLAN 52-4-2023

Charmaine Kettler-Schmult, Planning and Community Development Director, explained that the request before council is for a certificate of need for SP-4-2023, the construction of a 120 foot cellular tower and associated 50'x50' fenced equipment enclosure on the site of an existing motel at 30691 Grand River Ave.

Jill Bahm, Planning Consultant from Giffels Webster, stated that the applicant is proposing a new, 120 foot cellular tower wireless communication facility located inside a fenced equipment enclosure which is a permitted use in the B-3 district, subject to the standards of Section 34-4.24, however, it requires public hearings at both the planning commission and city council.

Planning Consultant Bahm explained that there are several standards for the specific demonstration of need for cellular towers, and the applicant has demonstrated that the proposed facility is needed because of the following:

- Proximity to an interstate highway or major thoroughfare, or its proximity to areas of population concentration, or concentrations of commercial, industrial and/or business centers.

- There are areas where signal interference has occurred due to tall buildings, masses of trees or other obstructions.
- The telecommunications provider is unable to co-locate its proposed facility with another provider.
- To complete its grid as it relates to the needs of Farmington Hills and its surrounding communities and that there are no suitable sites in any of said surrounding communities.
- Designed to operate within the requirements for radio frequency emissions of the Federal Communications Commission and applicant has operated similar facilities within these requirements consistently.
- The tower is a monopole design, as required.
- The tower is set among existing vegetation screening two sides of the enclosure, and arborvitae are proposed along the other two sides.
- A decommissioning agreement has been provided, per the applicant's letter dated June 12, 2023.

Bill Williams of Atlas Tower, applicant, stated that T Mobile will be the primary anchor tenant and AT&T has already expressed interest co-locating on the tower. He added that the proposed tower will replace the 100 foot T Mobile tower currently located on the smokestack at the Farmington Junction building which will be removed upon installation and equipment testing.

Mayor Rich opened the public hearing.

Pam Gerald, resident, expressed concerns over the electromagnetic fields from cellular towers and the risk of interference with medical equipment due to the close proximity to Beaumont Hospital and she questioned what type of fence is proposed.

There being no further comments, Mayor Rich closed the public hearing.

MOTION by Bruce, support by Knol, that the City Council of Farmington Hills hereby approves to issue a certificate of need approving Site Plan 52-4-2023, because the applicable standards of Section 34-4.24 of the Zoning Ordinance are met, subject to the following conditions:

- 1) The applicant shall make adequate provision for removal of all or part of the facility by users and owners when the facility has not been used for one-hundred-and-eighty (180) days or more; for purposes of this condition, the removal of antennas or other equipment from the facility, or the cessation of operations (transmission and/or reception of radio signals) shall be considered as the beginning of a period of nonuse; and
- 2) The applicant shall deposit with the City a performance guarantee of \$5,000 as security for the removal of the tower if abandoned for use of cellular facilities.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

In response to the questions asked, Bill Willimas noted the following:

- Telecommunications Act of 1996 specifies that the governing authority on health and safety of cellular towers is the FCC, and the FCC and FDA and all governing authorities have deemed that cellular communication facilities as safe for human use.
- A chain link fence with no barbed wire is proposed and it will be shielded by vegetation.

NEW BUSINESS

CONSIDERATION OF APPOINTMENTS TO VARIOUS BOARDS AND COMMISSIONS.

MOTION by Boleware, support by Bridges, that the City Council of Farmington Hills hereby confirms the Mayor’s recommendation to appoint the following individuals:

Commission on Community Health

Daniel Konja	Length of Term: 3 years	Term ending: February 1, 2027
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Emergency Preparedness Commission

Sean Thomas	Length of Term: 3 years	Term ending: February 1, 2027
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Economic Development Corporation

Gerrard Allen	Length of Term: 6 years	Term ending: February 1, 2030
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Brownfield Redevelopment Authority

Alex Meyers	Length of Term: 3 years	Term ending: February 1, 2027
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Grand River Corridor Improvement Authority

Mary Newlin	Length of Term: 4 years	Term ending: February 1, 2028
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Commission on Children, Youth & Families

Ashley Gabb	Length of Term: 3 years	Term ending: February 1, 2027
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Zoning Board of Appeals

Asim Khan	Length of Term: Unexpired Term	Term ending: February 1, 2025
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Commission on Aging

Tiffany Tuttle	Length of Term: Unexpired Term	Term ending: February 1, 2025
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MOTION CARRIED 7-0.

CONSIDERATION OF APPROVAL OF THE SECOND AMENDMENT TO PLANNED UNIT DEVELOPMENT 5, 1993 AGREEMENT WITH FARMINGTON HILLS CORPORATE INVESTORS FOR THE CONSTRUCTION OF TWO NEW OFFICE BUILDINGS.

Erik Perdonik, City Planner, explained that the purpose of the amendment is to revise the agreement to permit the construction of two new 150,000-square foot office buildings on a vacant parcel on the south

side of Twelve Mile Road which has been recommended for Council approval by the Planning Commission and Council then approved the plan associated with the draft amendment following a public hearing on August 22, 2022. He stated that the question before Council tonight is whether the proposed agreement is consistent with the specifics of the PUD Plan approved by Council in August 2022, noting that the draft amendment has been reviewed by the City Attorney and staff.

MOTION by Bridges, support by Bruce, that the City Council approves and authorizes the City Manager to execute the Second Amendment to Planned Unit Development 5, 1993 Agreement with Farmington Hills Corporate Investors for the construction of two new office buildings.

MOTION CARRIED 7-0.

CONSIDERATION OF APPROVAL OF THE THIRD AMENDMENT TO PLANNED UNIT DEVELOPMENT 5, 1993 AGREEMENT WITH FARMINGTON HILLS CORPORATE INVESTORS FOR COMERICA SIGNAGE.

Erik Perdonik, City Planner, explained that the purpose of the amendment is to revise the agreement to allow for new wall and monument signage oriented towards I-696 at the future site of Comerica Bank which has been recommended for Council approval by the Planning Commission and Council then approved the plan associated with the draft amendment following a public hearing on August 28, 2023. He stated that the question before Council tonight is whether the proposed agreement is consistent with the previously approved signage plan and he noted that the draft amendment has been reviewed by the City Attorney and Planning and Community Development Department staff.

MOTION by Knol, support by Boleware, that the City Council approves and authorizes the City Manager to execute the Third Amendment to Planned Unit Development 5, 1993 Agreement with Farmington Hills Corporate Investors for Comerica Signage.

MOTION CARRIED 7-0.

CONSENT AGENDA
RECOMMENDED APPROVAL OF CONSULTING SERVICES AGREEMENT FOR BUILDING DEPARTMENT DATABASE MANAGEMENT AND SYSTEM WITH MUNIVATE, LLC IN AN AMOUNT NOT TO EXCEED \$75,000. CMR 12-23-125

MOTION by Bridges, support by Boleware, that the City Council of Farmington Hills hereby approves waiving the sealed process and authorizes the City Manager to sign an agreement and issue a purchase order for purchasing sole source from Munivate, LLC, to provide Consulting Services to manage the Building system transition to BS&A Cloud for the City of Farmington Hills, in an amount not to exceed \$75,000.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

RECOMMENDED APPROVAL OF AS NEEDED PURCHASE OF ASPHALT MATERIALS FROM AJAX MATERIALS CORPORATION IN THE AMOUNT NOT-TO-EXCEED \$50,000 PER YEAR, WITH EXTENSIONS. CMR 12-23-126

MOTION by Bridges, support by Boleware, that the City Council of Farmington Hills hereby authorizes the City Manager to approve the required contract and purchase orders to Ajax Materials Corporation for As-Needed Asphalt Materials in the amount not-to-exceed \$50,000 per year with one or more administration approved extensions not-to-exceed a total of four (4) additional years.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

RECOMMENDED APPROVAL OF PURCHASE OF TWO 5-YARD DUMP TRUCK CHASSIS WITH SWAP LOADER HOIST SYSTEMS FROM WOLVERINE FREIGHTLINER IN THE AMOUNT OF \$231,492. CMR 12-23-127

MOTION by Bridges, support by Boleware, that the City Council of Farmington Hills hereby authorizes the City Manager to issue a purchase order to Wolverine Freightliner located in Mt. Clemens, Michigan, in the amount of \$231,492.00 for the purchase of two (2) 5-yard Swap Loader dump truck chassis.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

RECOMMENDED APPROVAL OF AS NEEDED PURCHASE OF OEM PARTS FOR EMERGENCY VEHICLES FROM LIQUID SPRING LLC IN THE ESTIMATED AMOUNTS OF \$15,000, ROSENBAUR IN THE AMOUNT OF \$30,000 AND MACQUEEN IN THE AMOUNT OF \$15,000. CMR 12-23-128

MOTION by Bridges, support by Boleware, that the City Council of Farmington Hills hereby authorizes the City Manager to approve purchase orders for as-needed OEM parts for emergency vehicles to the following three (3) vendors for the estimated annual amounts. Liquid Spring LLC. - \$15,000.00, Rosenbauer - \$30,000.00 and MacQueen - \$15,000.00.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

RECOMMENDED APPROVAL OF SETTING THE CITY COUNCIL GOALS SESSION DATE OF JANUARY 6, 2024.

MOTION by Bridges, support by Boleware, that the City Council of Farmington Hills hereby schedules the annual goals study session meeting for Saturday, January 6, 2024, beginning at 9:00 a.m. in the Community Room at City Hall.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

RECOMMENDED APPROVAL OF CITY COUNCIL STUDY SESSION MEETING MINUTES OF NOVEMBER 27, 2023

MOTION by Knol, support by Boleware, that the City Council of Farmington Hills hereby approves the study session meeting minutes of November 27, 2023.

Roll Call Vote:

Yeas: BOLEWARE, BRIDGES, BRUCE AND KNOL
Nays: NONE
Absent: NONE
Abstentions: ALDRED, DWYER AND RICH

MOTION CARRIED 4-0-0-3

RECOMMENDED APPROVAL OF CITY COUNCIL REGULAR SESSION MEETING MINUTES OF NOVEMBER 27, 2023

MOTION by Bridges, support by Boleware, that the City Council of Farmington Hills hereby approves the regular session meeting minutes of October 23, 2023.

Roll Call Vote:

Yeas: ALDRED, BOLEWARE, BRIDGES, BRUCE, DWYER, KNOL AND RICH
Nays: NONE
Absent: NONE
Abstentions: NONE

MOTION CARRIED 7-0.

ADDITIONS TO AGENDA

The attorney's report was received.

ADJOURNMENT

MOTION by Aldred, support by Dwyer, to adjourn the regular session City Council meeting at 8:50pm.

MOTION CARRIED 7-0.

Respectfully submitted,

Carly Lindahl, Interim City Clerk

MINUTES
CITY OF FARMINGTON HILLS
CITY COUNCIL SPECIAL MEETING
CITY HALL – COMMUNITY ROOM
DECEMBER 21, 2023 – 6:00PM

The special meeting of the Farmington Hills City Council was called to order by Mayor Rich at 6:01pm.

Council Members Present: Aldred, Boleware, Bruce, Dwyer, Knol and Rich

Council Members Absent: Bridges

Others Present: City Manager Mekjian, Interim City Clerk Lindahl, Assistant City Manager Mondora, Director Skrobola, Police Chief King, and City Attorney Joppich

CONSIDERATION OF APPROVAL OF AUTHORIZING THE CITY MANAGER AND CITY CLERK TO SIGN A CONTRACT WITH AXON FOR THE OFFICER SAFETY BUNDLE FOR A 5-YEAR TERM. CMR 12-23-129

Police Chief King thanked Council for their consideration and taking time to hold this special meeting. He explained that the department feels that this is the best way to move forward as future challenges not only increase officer and community safety but also department effectiveness, and this contract will only increase accountability within the officers and agency. He added that the sole objective is to bring professionalism and increase performance levels from officer up to chief of police in how the department polices the community.

Kyle Hites, Axon Regional Manager, provided Council with a presentation which introduced Axon, discussed how they can help solve issues the department is currently experiencing, and reviewed the benefits of the Officer Safety Bundle package. The following was highlighted:

- Axon works with over 19,000 agencies including a number of agencies in the greater Detroit area, noting that Warren, Dearborn and Novi have purchased similar officer safety packages.
- They provide a secure central location to store and manage data and digital evidence, such as body camera footage, in car camera footage as well as associated photos and other CCTV camera footage in a more efficient and effective manner; addressing the issue of sharing data as prosecutors and partner agencies are able to access.
- The built in AI redaction capabilities will assist with FOIA requests for body and in car camera footage, allowing staff to expedite the turnaround time to process these types of requests, promoting the efficiency and transparency of the department.
- All tasers devices will be upgraded to the Taser 10 which is a more effective, less lethal device.
- Axon performance, a software-based tool, will give supervisors and command staff the ability to audit videos to ensure that the department is following policy standards, that their officers are being held accountable and their day-to-day interactions with the public are professional.
- In car cameras will be upgraded to the Fleet 3 system which has integrated license plate reader technology; a technology currently used within the city that has assisted in solving crimes quickly; which promotes efficiency, officer safety and transparency.
- All cloud security and digital data is designed to meet FBI requirements.

Mr. Hites discussed the costs and terms associated with the proposal, noting the following:

- The total five-year cost of the proposal is \$2,721,297.20 with an effective start date of March 1, 2024.
- The first year is split into two payments; \$49,006.43 invoiced in February 2024 when the hardware is expected to ship and program set to go, live with the remaining amount of \$496,253.01 invoiced on July 1, 2024.
- The remaining years an invoice of \$544,259.44 will come in July with the last one being July 2028.
- This bundle receives many discounts, and the department is set to receive \$931,370.89 in savings over the five-year contract
- After January 1st the prices are set to increase, as well as incentives that will be lost, increasing the contract to \$3,121,297.00, therefore, executing the contract before the end of the year, the city is set to receive savings of \$400,000, which is equivalent to nearly nine months of service in contract value.

Council questioned the urgency to have the agreement signed, why there is such an increase from 2023 to 2024 and the timeline if the agreement was approved this evening.

In response to the questions, Michael Tagliaferri, Vice President of Sales, stated that their fiscal year matches the calendar year so at the end of 2023 they close the books and begin a new fiscal year. He added that Axon recently moved into the S&P 500 and is one of the 500 largest companies in the in the US so the decision to not offer 2023 pricing in 2024 is a financial and legal decision, not a sales decision.

Mr. Hites mentioned that they have locked in shipping timelines and all hardware is expected to ship in February with installation likely taking place between March and April, including in car cameras, body cameras and tasers.

Discussion was held on the need for all officers to have their own body cameras and tasers instead of checking out from an equipment pool in the armory, as it eliminates delayed officer response and issues with mis-tagging because videos from the pool cameras are not assigned to a specific officer.

Council expressed concerns regarding the performance of existing vendors and issues with current contracts and requested that going forward they be provide with updates on issues with any vendors or contracts, in which they approved, not meeting their contractual obligation.

Discussion was held on the timeline provided by Chief King detailing staff's evaluation of the proposal and the issues with current vendors and impact on the efficiency and safety of the department.

Mayor Rich questioned the terms and conditions of the flock system and if there is assurance it will continue to be used responsibly with integration to the new system.

Chief King responded stating that the department's policy is a model policy, and the system is used to look for violent felonies, domestic violence, stolen cars and missing persons, observing only the plate, vehicle make, model, color, not the interior occupancy of the vehicle. He added that the new system will mirror this policy and they will follow it very strictly.

Council expressed concerns if the installation was not complete within the timeframe discussed and how the city would handle a breach of contract.

Mr. Tagliaferri addressed Council's concerns stating that the completion expectation is April 2024, although it is somewhat of a moving target due to the coordination of vehicles to be taken out of service for installation and the weeklong training for officers, but they are confident with the timeline as they have the inventory ready.

Attorney Joppich added that if there were a breach of contract, the city would provide written notice and the opportunity to the vendor to fix the issue; it would all be documented, and the city would consult with his office to formulate those notices and how to proceed going forward.

City Manager Mekjian stated that this is a critical program for our Police Department and the Chief along with staff will be overseeing the schedule and the implementation, so if there are issues staff will be on top of it and prepared to take legal action if necessary.

Council requested that going forward they be provide with updates on issues with any vendors or contracts, in which they approved, not meeting their contractual obligation.

There were no public comments.

MOTION by Dwyer, support by Knol, that the City Council of Farmington Hills hereby authorizes the City Manager and City Clerk to sign and enter into an agreement with Axon for the Officer Safety Bundle, 5-year agreement for \$2,721,297.20, or \$544,259.44 per year.

MOTION CARRIED 6-0.

ADJOURNMENT

MOTION by Aldred, support by Boleware, to adjourn the special City Council meeting at 7:03pm.

MOTION CARRIED 6-0.

Respectfully submitted,

Carly Lindahl, Interim City Clerk